

# Planning Committee (Major Applications) B

Wednesday 13 September 2023

6.30 pm

Ground Floor Meeting Room G01A - 160 Tooley Street, London SE1 2QH

## Membership

Councillor Richard Livingstone (Chair)  
Councillor Kath Whittam (Vice-Chair)  
Councillor Ellie Cumbo  
Councillor Sam Foster  
Councillor Jon Hartley  
Councillor Portia Mwangangye  
Councillor Emily Tester

## Reserves

Councillor Cassandra Brown  
Councillor Sam Dalton  
Councillor Barrie Hargrove  
Councillor Nick Johnson  
Councillor Sarah King  
Councillor Reginald Popoola  
Councillor Cleo Soanes

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## INFORMATION FOR MEMBERS OF THE PUBLIC

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### Access to information

You have the right to request to inspect copies of minutes and reports on this agenda as well as the background documents used in the preparation of these reports.

### Babysitting/Carers allowances

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### Access

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### Contact

Gerald Gohler on 020 7525 7420 or email: [Gerald.gohler@southwark.gov.uk](mailto:Gerald.gohler@southwark.gov.uk)

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Members of the committee are summoned to attend this meeting

**Althea Loderick**

Chief Executive

Date: 5 September 2023



# Planning Committee (Major Applications) B

Wednesday 13 September 2023

6.30 pm

Ground Floor Meeting Room G01A - 160 Tooley Street, London SE1 2QH

## Order of Business

Item No.	Title	Page No.
<b>PART A - OPEN BUSINESS</b>		
<b>1.</b>	<b>APOLOGIES</b>	
	To receive any apologies for absence.	
<b>2.</b>	<b>CONFIRMATION OF VOTING MEMBERS</b>	
	A representative of each political group will confirm the voting members of the committee.	
<b>3.</b>	<b>NOTIFICATION OF ANY ITEMS OF BUSINESS WHICH THE CHAIR DEEMS URGENT</b>	
	In special circumstances, an item of business may be added to an agenda within five clear days of the meeting.	
<b>4.</b>	<b>DISCLOSURE OF INTERESTS AND DISPENSATIONS</b>	
	Members to declare any personal interests and dispensation in respect of any item of business to be considered at this meeting.	
<b>5.</b>	<b>DEVELOPMENT MANAGEMENT</b>	1 - 5
<b>5.1.</b>	<b>CANADA WATER RETAIL PARK</b>	6 - 215
<b>5.2.</b>	<b>EASTERN EDGE OF CANADA DOCK ADJACENT TO CANADA WATER RETAIL PARK</b>	216 - 320

**ANY OTHER OPEN BUSINESS AS NOTIFIED AT THE START OF THE MEETING AND ACCEPTED BY THE CHAIR AS URGENT**

**EXCLUSION OF PRESS AND PUBLIC**

The following motion should be moved, seconded and approved if the committee wishes to exclude the press and public to deal with reports revealing exempt information:

“That the public be excluded from the meeting for the following items of business on the grounds that they involve the likely disclosure of exempt information as defined in paragraphs 1-7, Access to Information Procedure rules of the Constitution.”

**PART B - CLOSED BUSINESS**

**ANY OTHER CLOSED BUSINESS AS NOTIFIED AT THE START OF THE MEETING AND ACCEPTED BY THE CHAIR AS URGENT**

Date: 5 September 2023

## Planning Committee (major Applications)

### Guidance on conduct of business for planning applications, enforcement cases and other planning proposals

1. The reports are taken in the order of business on the agenda.
2. The officers present the report and recommendations and answer points raised by members of the committee.
3. The role of members of the planning committee (major applications) is to make planning decisions openly, impartially, with sound judgement and for justifiable reasons in accordance with the statutory planning framework.
4. The following may address the committee (if they are present and wish to speak) for **not more than 3 minutes each**.

- (a) One representative (spokesperson) for any objectors. If there is more than one objector wishing to speak, the time is then divided within the 3-minute time slot.
- (b) The applicant or applicant's agent.
- (c) One representative for any supporters (who live within 100 metres of the development site).
- (d) Ward councillor (spokesperson) from where the proposal is located.
- (e) The members of the committee will then debate the application and consider the recommendation.

**Note:** Members of the committee may question those who speak only on matters relevant to the roles and functions of the planning committee that are outlined in the constitution and in accordance with the statutory planning framework.

5. If there are a number of people who are objecting to, or are in support of, an application or an enforcement of action, you are requested to identify a representative to address the committee. If more than one person wishes to speak, the 3-minute time allowance must be divided amongst those who wish to speak. Where you are unable to decide who is to speak in advance of the meeting, you are advised to meet with other objectors in the foyer of the council offices prior to the start of the meeting to identify a representative. If this is not possible, the chair will ask which objector(s) would like to speak at the point the actual item is being considered.
6. Speakers should lead the committee to subjects on which they would welcome further questioning.
7. Those people nominated to speak on behalf of objectors, supporters or applicants, as well as ward members, should sit on the front row of the public seating area. This is for ease of communication between the committee and the speaker, in case any



issues need to be clarified later in the proceedings; it is **not** an opportunity to take part in the debate of the committee.

8. Each speaker should restrict their comments to the planning aspects of the proposal and should avoid repeating what is already in the report. The meeting is not a hearing where all participants present evidence to be examined by other participants.
9. This is a council committee meeting which is open to the public and there should be no interruptions from the audience.
10. No smoking is allowed at committee.
11. Members of the public are welcome to film, audio record, photograph, or tweet the public proceedings of the meeting; please be considerate towards other people in the room and take care not to disturb the proceedings.

**Please note:**

Those wishing to speak at the meeting should notify the constitutional team by email at [ConsTeam@southwark.gov.uk](mailto:ConsTeam@southwark.gov.uk) in advance of the meeting by **5pm** on the working day preceding the meeting.

**The arrangements at the meeting may be varied at the discretion of the chair.**

**Contacts:** General Enquiries  
Planning Section  
Environment, Neighbourhoods and Growth  
Tel: 020 7525 5403

Planning Committee Clerk, Constitutional Team  
Governance and Assurance  
Tel: 020 7525 3667

<b>Item No.</b> 5.	<b>Classification:</b> Open	<b>Date:</b> 13 September 2023	<b>Meeting Name:</b> Planning Committee B (Major Applications)
<b>Report title:</b>		Development Management	
<b>Ward(s) or groups affected:</b>		All	
<b>From:</b>		Proper Constitutional Officer	

### RECOMMENDATIONS

1. That the determination of planning applications, or formal observations and comments, the instigation of enforcement action and the receipt of the reports included in the attached items be considered.
2. That the decisions made on the planning applications be subject to the conditions and/or made for the reasons set out in the attached reports unless otherwise stated.
3. That where reasons for decisions or conditions are not included or not as included in the reports relating to an individual item, they be clearly specified.

### BACKGROUND INFORMATION

4. The council's powers to consider planning business are detailed in Part 3F which describes the role and functions of the planning committee and planning sub-committees. The matters reserved to the planning committee and planning sub-committees exercising planning functions are described in part 3F of the Southwark Council constitution.

### KEY ISSUES FOR CONSIDERATION

5. In respect of the attached planning committee items members are asked, where appropriate:
  - a. To determine those applications in respect of site(s) within the borough, subject where applicable, to the consent of the Secretary of State for Levelling Up, Housing and Communities and any directions made by the Mayor of London.
  - b. To give observations on applications in respect of which the council is not the planning authority in planning matters but which relate to site(s) within

the borough, or where the site(s) is outside the borough but may affect the amenity of residents within the borough.

- c. To receive for information any reports on the previous determination of applications, current activities on site, or other information relating to specific planning applications requested by members.
6. Each of the following items are preceded by a map showing the location of the land/property to which the report relates. Following the report, there is a draft decision notice detailing the officer's recommendation indicating approval or refusal. Where a refusal is recommended the draft decision notice will detail the reasons for such refusal.
7. Applicants have the right to appeal to Planning Inspector against a refusal of planning permission and against any condition imposed as part of permission. Costs are incurred in presenting the council's case at appeal which maybe substantial if the matter is dealt with at a public inquiry.
8. The sanctioning of enforcement action can also involve costs such as process serving, court costs and of legal representation.
9. Where either party is felt to have acted unreasonably in an appeal the inspector can make an award of costs against the offending party.
10. All legal/counsel fees and costs as well as awards of costs against the council are borne by the budget of the relevant department.

### **Community impact statement**

11. Community impact considerations are contained within each item.

### **SUPPLEMENTARY ADVICE FROM OTHER OFFICERS**

#### **Assistant Chief Executive (Governance and Assurance)**

12. A resolution to grant planning permission shall mean that the Director of Planning and Growth is authorised to grant planning permission. The resolution does not itself constitute the permission and only the formal document authorised by the committee and issued under the signature of the Director of Planning and Growth shall constitute a planning permission. Any additional conditions required by the committee will be recorded in the minutes and the final planning permission issued will reflect the requirements of the planning committee.
13. A resolution to grant planning permission subject to legal agreement shall mean that the Director of Planning and Growth is authorised to issue a planning

permission subject to the applicant and any other necessary party entering into a written agreement in a form of words prepared by the Director of Law and Governance, and which is satisfactory to the Director of Planning and Growth. Developers meet the council's legal costs of such agreements. Such an agreement shall be entered into under section 106 of the Town and Country Planning Act 1990 or under another appropriate enactment as shall be determined by the Director of Law and Governance. The planning permission will not be issued unless such an agreement is completed.

14. Section 70 of the Town and Country Planning Act 1990 as amended requires the council to have regard to the provisions of the development plan, so far as material to the application, and to any other material considerations when dealing with applications for planning permission.
15. Section 38(6) of the Planning and Compulsory Purchase Act 2004 provides that where, in making any determination under the planning Acts, regard is to be had to the development plan, the determination shall be made in accordance with the plan unless material considerations indicate otherwise. The development plan is currently the Southwark Plan which was adopted by the council in February 2022. The Southwark Plan 2022 was adopted after the London Plan in 2021. For the purpose of decision-making, the policies of the London Plan 2021 should not be considered out of date simply because they were adopted before the Southwark Plan 2022. London Plan policies should be given weight according to the degree of consistency with the Southwark Plan 2022.
16. The National Planning Policy Framework (NPPF), as amended in July 2021, is a relevant material consideration and should be taken into account in any decision-making.
17. Section 143 of the Localism Act 2011 provides that local finance considerations (such as government grants and other financial assistance such as New Homes Bonus) and monies received through CIL (including the Mayoral CIL) are a material consideration to be taken into account in the determination of planning applications in England. However, the weight to be attached to such matters remains a matter for the decision-maker.
18. "Regulation 122 of the Community Infrastructure Levy regulations (CIL) 2010 as amended, provides that "a planning obligation may only constitute a reason for granting planning permission if the obligation is:
  - a. necessary to make the development acceptable in planning terms;
  - b. directly related to the development; and
  - c. fairly and reasonably related to the scale and kind to the development.

A planning obligation may only constitute a reason for granting planning permission if it complies with the above statutory tests."

19. The obligation must also be such as a reasonable planning authority, duly appreciating its statutory duties can properly impose i.e. it must not be so unreasonable that no reasonable authority could have imposed it. Before resolving to grant planning permission subject to a legal agreement members should therefore satisfy themselves that the subject matter of the proposed agreement will meet these tests.

#### BACKGROUND DOCUMENTS

Background Papers	Held At	Contact
Council assembly agenda 23 May 2012	Constitutional Team 160 Tooley Street London SE1 2QH	Virginia Wynn-Jones 020 7525 7055
Each planning committee item has a separate planning case file	Development Management 160 Tooley Street London SE1 2QH	Planning Department 020 7525 5403

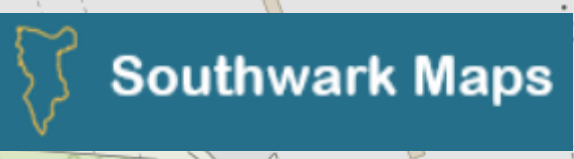
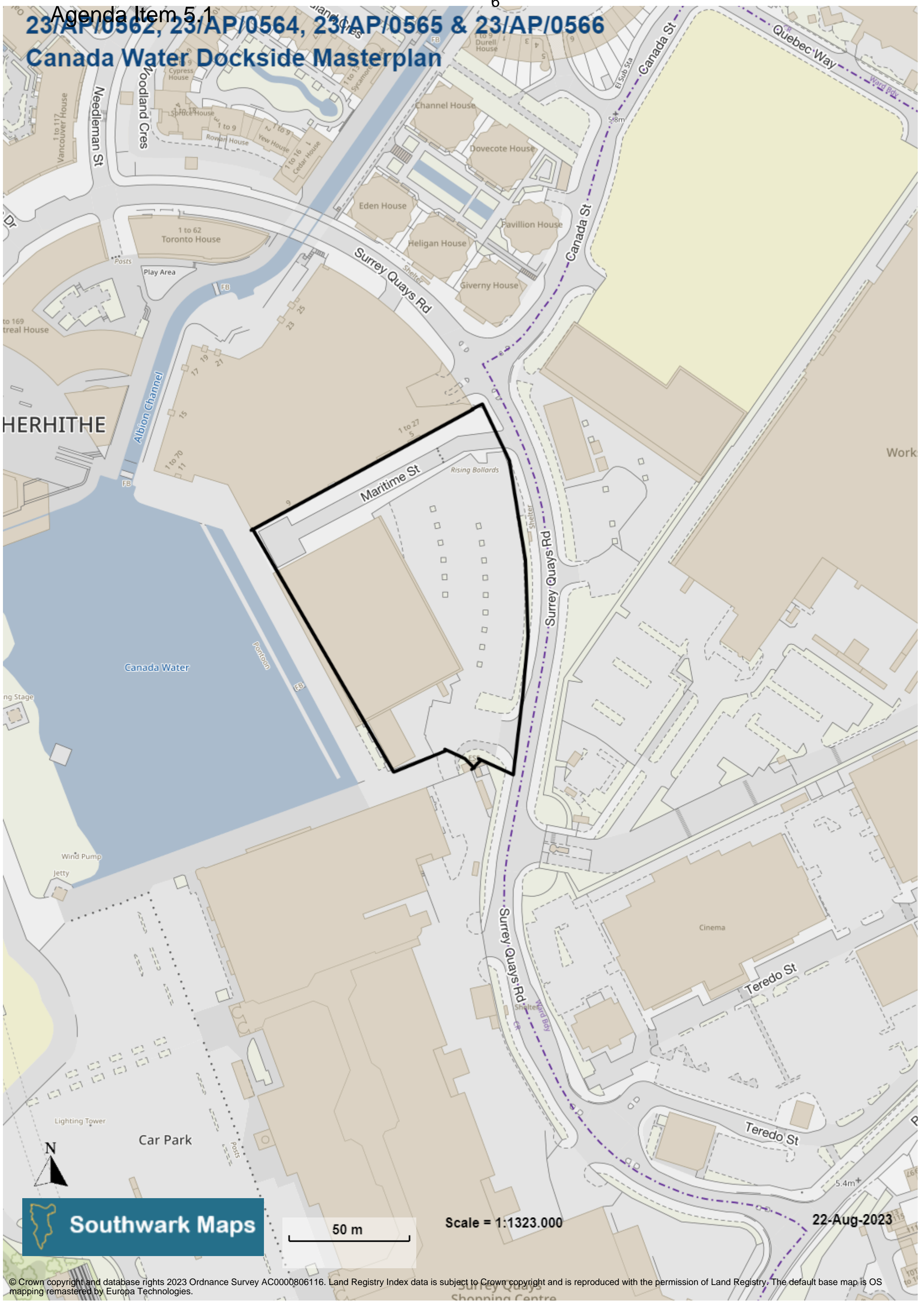
#### APPENDICES

No.	Title
None	

**AUDIT TRAIL**

<b>Lead Officer</b>	Chidilim Agada, Head of Constitutional Services	
<b>Report Author</b>	Gregory Weaver, Constitutional Officer Nagla Stevens, Deputy Head of Law (Planning and Development)	
<b>Version</b>	Final	
<b>Dated</b>	5 September 2023	
<b>Key Decision?</b>	No	
<b>CONSULTATION WITH OTHER OFFICERS / DIRECTORATES / CABINET MEMBER</b>		
<b>Officer Title</b>	<b>Comments sought</b>	<b>Comments included</b>
Assistant Chief Executive – Governance and Assurance	Yes	Yes
Director of Planning and Growth	No	No
<b>Cabinet Member</b>	No	No
<b>Date final report sent to Constitutional Team</b>		5 September 2022

# Agenda Item 5.1 23/AP/0562, 23/AP/0564, 23/AP/0565 & 23/AP/0566 Canada Water Dockside Masterplan



Scale = 1:1323.000

22-Aug-2023

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<b>Item No.</b> 5.1	<b>Classification:</b> Open	<b>Date:</b> 13 September 2023	<b>Meeting Name:</b> Planning Committee (Major Applications) B
<b>Report title:</b>	<b>Development Management planning applications:-</b>  <b>Address:</b> Units 1 And 4 Canada Water Retail Park and Maritime Street, London SE16  <b>Proposal</b> Approval of Reserved Matters (access, appearance, landscaping, layout and scale) pursuant to Outline Planning Permission 21/AP/2655 for the comprehensive commercial development of the Canada Water Dockside Masterplan site together with approval of Reserved Matters (access, appearance, landscaping and layout) pursuant to the Outline Planning Permission 21/AP/2610 which proposed hard and soft landscaping to create a shared public realm space for use by pedestrians and vehicles on Maritime Street.  <b>Reserved Matters Approval sought for the following:-</b>  <b>23/AP/0562</b> - Reserved Matters Approval sought for construction of Building A1 (maximum height 110m AOD) comprising the following uses; offices (Class E), retail (E) and food and beverage (E), with landscaping, plant and associated works.  <b>23/AP/0564</b> - Reserved Matters Approval sought for construction of Building A2 (maximum height 55.2m AOD) comprising the following uses; offices (Class E), retail (E) and food and beverage (E), with cycle parking, landscaping, loading bay, plant and associated works.  <b>23/AP/0565</b> - Reserved Matters Approval sought for construction of a basement spanning Plots A1 and A2 comprising cycle parking, plant space and ancillary floorspace and associated works (all ancillary Class E) to support office and retail/food and beverage uses in Plots A1 and A2.  <b>23/AP/0566</b> - Approval of Reserved Matters (access, appearance, landscaping, layout and scale) in respect of hard and soft landscaping to create a shared public realm space for use by pedestrians and vehicles on Maritime Street.		
<b>Ward(s) or groups affected:</b>	Rotherhithe		
<b>From:</b>	Director of Planning and Growth		
<b>Application Start Date</b>	01.03.2023	<b>PPA Expiry Date</b> n/a	
<b>Earliest Decision Date</b>	13.04.2023		

## RECOMMENDATION

1. That the applications for the following Reserved Matters be granted

- 23/AP/0562
- 23/AP/0564
- 23/AP/0565
- 23/AP/0566

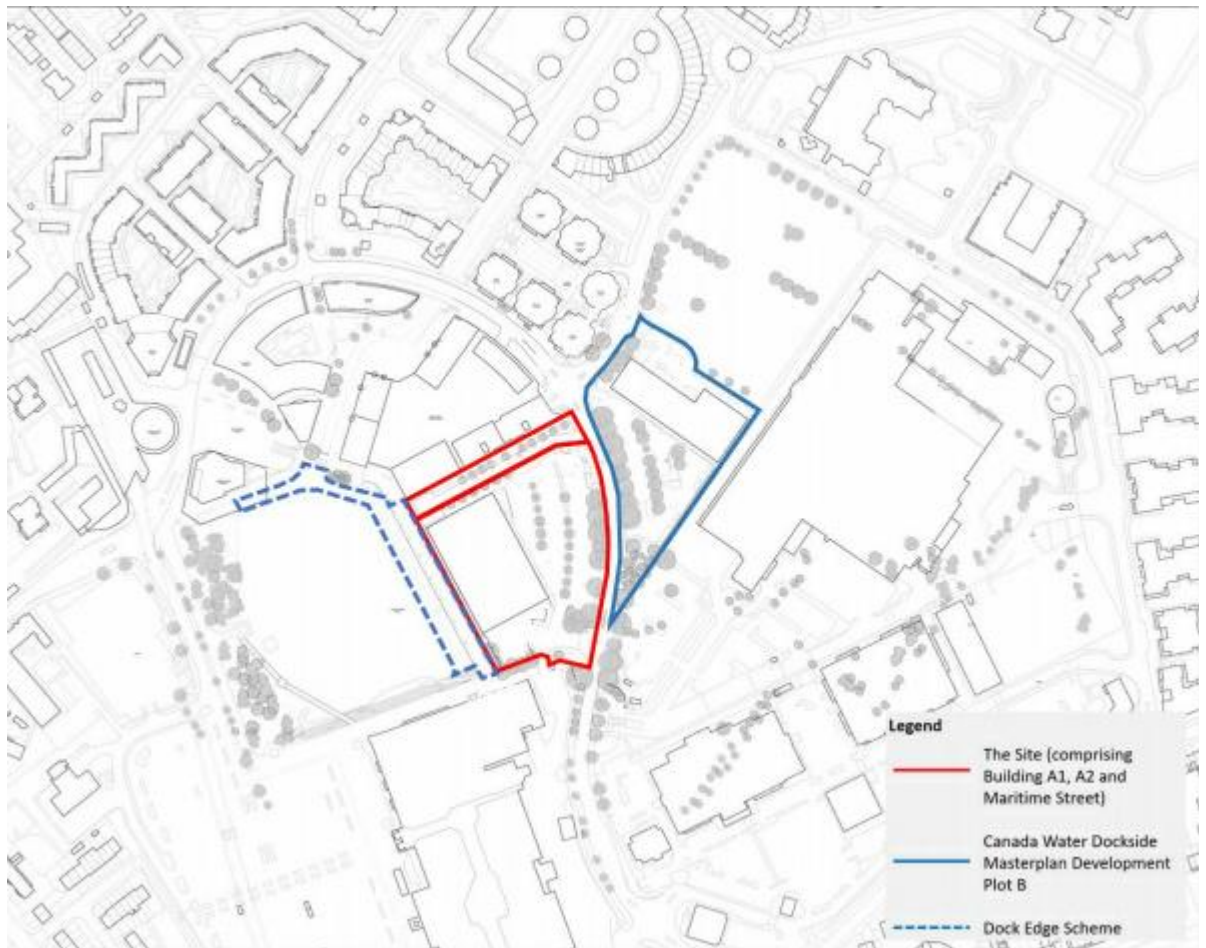
subject to the additional conditions and informative as set out in the draft recommendations at Appendix 1; and

2. That environmental information be taken into account as required by Regulation 26(1) of the Town and Country Planning (Environmental Impact Assessment) Regulations 2017 (as amended); and
3. That following the issue of planning permission, the Director of Planning and Growth write to the Secretary of State notifying them of the Decision, pursuant to Regulation 30(1)(a) of the Town and Country Planning (Environmental Impact Assessment) Regulations 2017; and
4. That following issue of planning permission, the Director of Planning and Growth place a statement on the Statutory Register pursuant to Regulation 28(1) of the TCP (EIA) Regulations 2017, which contains the information required by Regulation 28 and, for the purposes of Regulation 28(1)(h) being the main reasons and considerations on which the Planning Committee's decision was based shall be set out in the report; and
5. That the Planning Committee in making their decision has due regard to the potential Equalities impacts that are outlined in this report.

## EXECUTIVE SUMMARY

6. This report deals with four separate Reserved Matters Applications (RMA) which will be delivered as one phase of the Canada Water Dockside (CWD) Masterplan following the grant of Outline Planning Permissions (OPP) 21/AP/2655 and 21/AP/2610. The applications are being reported to the Planning Committee as requested by Members when Outline Consent was granted.
7. It should be noted that the Reserved Matters Applications are bound by the s106 legal agreement and conditions attached to the Outline Planning Permission 21/AP/2655; and 21/AP/2610.
8. A separate report is on the agenda for this committee in respect of a full planning application for enhancement to the adjacent Dock Edge (23/AP/0798). The proposals for the Dock Edge will deliver a high quality piece of public realm directly related to the development of the CWD Masterplan.
9. The RMAs subject of this report relate to what is known as Phase one of the Masterplan (identified as Plot A). Within Plot A there are two buildings proposed (Buildings A1 and A2 with a shared basement beneath). The public realm

enhancements to Maritime Street will be delivered as part of the Building A2 works.



**Image above:** Plan to identify the location of RMAs subject of this committee report as well as the Dock proposals.

10. The proposed phasing for the development is as follows

- Phase 0 = Demolition on Plot A
- Phase 1 = Construction of basements of Buildings A1 and A2
- Phase 2 = Construction of Building A2 above ground
- Phase 3 = Construction of Building A1 above ground
- Phase 4 = Demolition on Development Plot B and construction of Building B (note that a separate/future submission is required for RM for Plot B).

11. For Building A1 Reserved Matters Approval is sought to construct a building of ground, mezzanine plus 24 storeys (to a height of 110.0m AOD) to accommodate ground floor active retail/food and beverage uses on all corners of the building and an office lobby space that gives prominence to cyclists through a Mobility Hub on the Boulevard. The upper levels of the building provide workplace in five 'stacked boxes', each representing a standard building type with its own west-facing terrace overlooking the Dock. The workplace floors have been designed to be flexible and adapt to different tenancies, as well as building in soft spots to allow structural alterations without material waste. Specifically, the application proposes:

- 59,273 sq m GEA of workplace;
- 777 sq m GEA of retail/food and beverage;

- 822 sq m GEA of roof plant
- 3,661 sqm GEA basement

The proposals include the provision of 1,210 long stay and 236 short stay cycle parking spaces.

12. For Building A2 Reserved Matters Approval is sought to construct a building of ground, mezzanine plus 11 storeys above (to a height of 55.40 m AOD), to accommodate workplace, food and beverage and retail uses, with supporting cycle parking, plant and other ancillary spaces. Specifically, the application proposes:

- 33,853 sq m GEA of workplace;
- 808 sq m GEA of retail/food and beverage;
- 582 sq m GEA of roof plant.
- 2,131 sqm GEA basement

The proposals include the provision of 672 long stay and 136 short stay cycle parking spaces.

13. The ground floor of A2 will accommodate active retail/food and beverage uses on all sides of the building, animating and activating the public realm, and an office lobby space that gives prominence to cyclists through the creation of a Mobility Hub on the corner of the Boulevard and Surrey Quays Road, between Building A2 and adjacent Building A1. The upper levels of the building provide office floorspace in the format of large open plan layouts with the upper levels benefitting from external terraces. Some of the terraces are large enough to accommodate 'sitting out' whilst others will be used to accommodate soft landscaping with large openings facilitating natural ventilation and views out from the adjacent office space. The internal layout has been designed to be flexible and adapt to multiple tenancies.
14. A combined basement will span the full extent of A1 and A2. The basement will accommodate plant for both buildings and a significant quantum of cycle storage for Building A1.
15. Condition 36 attached the OPP secures the implementation of a detailed strategy for ventilation of the basement.
16. The proposals to enhance Maritime Street include works of hard and soft landscaping designed to improve the public realm in this location, whilst continuing to provide access for pedestrians and cyclists. The development would rationalise the vehicular movements along Maritime Street to allow for a centralised green spine and planting areas. The proposed planting areas will also act as rain gardens providing an important element of the Sustainable Urban Drainage strategy (SUDs) for the CWDM site. Essential vehicle access will be retained, this includes Porters Edge servicing and emergency access (as per existing), together with emergency fire tender access for the proposed Building A2.
17. The submission of each of the applications described in this report follows a series of pre application discussions as a result of which improvements were secured in respect of the detailed design of the buildings and surrounding public realm.

18. The development would deliver a significant quantum of commercial floorspace including affordable workspace which would bring positive economic benefits to the borough in accordance with the OPP and development plan policies.
19. The design of each building is unique. Both buildings have a different contemporary design in terms of mass and architectural approach, offering a modern, flexible and unique office environments intended to become a distinctive employment destination within the town centre. Together, the proposals will deliver two high quality buildings which will make a positive contribution to the townscape.
20. High quality public realm will be delivered by the RMAs subject of this application. The public realm proposals comprise on-site spaces such as The Boulevard, Waterfront Square and Dock Edge Walk. These spaces have been designed to deliver important routes for pedestrians and cyclists as well as high quality landscaped spaces for people to dwell; together with opportunities for spill out for the commercial units. The on-site spaces will connect to existing and planned future public realm to be delivered as part of the town centre redevelopment.
21. In addition, detailed enhancements are proposed for Maritime Street and the eastern edge of the Dock. It is recognised that the proposals for the Dock and Maritime Street fall within land not owned by the applicant and therefore if the applicant is unable to secure all the necessary consents to undertake the proposed enhancements, the s106 attached to the Outline Permission secures a fall back payment in lieu for these elements. The s106 obligates the Applicant to use all reasonable endeavours to obtain necessary consents but in the event that it is not possible they will be required to submit a full Scope of Works including a detailed Cost Plan to the Council to demonstrate the cost of delivering the Dock Edge and Maritime Street enhancements. Once the full costs have been agreed by the Council the Applicant is obligated to make a payment in lieu for the Council to deliver public realm improvements in the vicinity of the site.
22. The proposal responds positively to transport and sustainability policies delivering upon the principles established as part of the OPP. Furthermore, the impact upon neighbouring amenity was assessed and deemed to be acceptable as part of the OPP and there is no change to those impacts arising from the detailed RMA proposals. Subject to the appropriate mitigation secured by the conditions and s106 obligations attached to the Outline Permission, these proposals are in line with the objectives of the Masterplan and compliant with development plan policies.
23. The cumulative impact of the development, together with adjacent committed schemes was assessed in terms of the environmental impacts, including construction impacts and operational impact in respect of land use, transport, design and amenity and deemed to be acceptable when OPP was granted. Furthermore the OPP considered the impact on the British Land Masterplan permission and it was clearly concluded that redevelopment of this site would not fetter the ability of the adjacent BL Outline Permission to be built to its maximum parameters and for the range of uses identified within the adjacent blocks. That conclusion stands following assessment of the RMAs subject of this report.
24. In all other respects, subject to the appropriate mitigation secured by the conditions and obligations attached to the OPP as well as those additional recommended



conditions set out below, the proposal is considered to be acceptable for the reasons discussed in this report.



**Image above:** CGI to show Building A2 (centre) and the lower sections of A1 (right) together with the surrounding public realm of the Dock Edge, Waterfront Square and the Boulevard (NB: image does not show the full height of A1)

#### Non-residential

Use Class	Existing sqm (GEA)	Proposed sqm (GEA)	Change +/-
Temporary Flexible Class E / F2 / Sui Generis  (Events and community space)	3,580 sqm (GIA) 3,978 sqm (GEA)  Based on GIA:GEA = 90%	0	- 3,978 sqm (GEA)
Temporary Sui Generis (Immersive experience hospitality and ancillary uses)	2,548 sqm (GIA) 2,831 sqm (GEA)  Based on GIA:GEA = 90%	0	- 2,831 sqm (GEA)
Retail / Food and Drinks (Class E/a/b)		1,585 sqm (GEA)	+ 1,585 sqm (GEA)
Workplace		93,126 sqm (GEA)	+ 93,126



E(g)(i)			sqm (GEA)
Plant		1,404 sqm (GEA)	+ 1,404 sqm (GEA)
Basement		5,792 sqm (GEA)	+ 5,792 sqm (GEA)
Jobs	Unknown due to transient nature of meanwhile uses	FTE at operational phases - • A1 FTE – 3,450 – 4,485 • A2 FTE – 1,985 – 2,580	N/A

## Environmental

CO2 Savings beyond Part L Bldg. Regs (2013 and 2021 baseline reported).	Plot A1 53% reduction in regulated carbon emissions over Part L 2013 13.1% reduction against Part L 2021  Plot A2 51% reduction in regulated carbon emissions over Part L 2013 16.9% reduction against Part L 2021
Trees lost and gained	The whole Masterplan site (including Plot B) contains 92 trees of varying qualities, of which 30 were secured to be retained at OPP stage. The masterplan will deliver 94 new trees (including the trees to be planted in Maritime Street).

	Existing	Proposed	Change +/-
Urban Greening Factor Score	N/A	Plot A1 – 0.43 Plot A2 – 0.35 Maritime Street – 0.42	
BNG		Plot A1: Total net change in habitat units for Plot A1 equates to 0.08, (using the Defra Biodiversity Metric Calculator 3.1), resulting in a BNG increase of 8.87%.	

		<p>Plot A2: Total net change in habitat units for Plot A2 equates to 0.89, (using the Defra Biodiversity Metric Calculator 3.1), resulting in a BNG increase of 210.10%.</p> <p>The Outline Planning Permission was designed to meet the target of 10%, and indicated a potential net gain of 136.78%. Since this calculation was undertaken, the methodology for calculating BNG has been updated by DEFRA. Using the new calculator and with no material change in the proposed tree and greening across Plots A1 and A2 (and using the illustrative scheme for Plot B), the masterplan overall now achieves a net gain of 41.97%.</p>	
Surface Water Run Off Rate	135.7 l/s/ha	67.85 l/s/ha	67.85 l/s/ha – 50% less than existing run off
Green/Brown Roofs	0sqm	<p>For Plot A1 Extensive green roof 545.5 sqm Intensive green roof – 463 sqm</p> <p>For Plot A2 Extensive green roof – 470.5 sqm Intensive green roof – 696 sqm</p>	+ 2,175 sqm
EVCPS	0	The servicing area	+6

		provides a total of 4no. vehicle servicing bays (comprising one 10m vehicle bay and three 8m vehicle bays), together with 2no. accessible Blue Badge parking bays. All of the parking and servicing bays will be fully equipped with electric charging facilities.	
Cycle parking spaces	20	For Plot A1 - 1,210 long stay and 236 short stay spaces with 3% of these provided as accessible stands and 8% as Sheffield stands.  For Plot A2 - 672 long stay and 136 short stay spaces with 6% of these provided as accessible stands and 20% as Sheffield stands.	+2,234

CIL and S106 contributions (NB: CIL estimates do not take account of phasing or any offset from existing floorspace)

SCIL (estimated)	£266,618.42
MCIL (estimated)	£6,244,579.08
S106	N/A for the RMA as this was secured as part of the OPP

## BACKGROUND INFORMATION

### Site location and description

#### The Site

25. The proposed development relates to part of the 'Canada Water Dockside Masterplan' site. The site comprises two adjoining red line boundaries relating to land under different ownerships: The Canada Water Dockside Site (CWD) and the smaller Maritime Street Site. Within the CWD site there are two Plots known as Plot A and Plot B. Within Plot A there are 3 separate planning applications which together will result in the delivery of two buildings (Building A1 and A2) with a combined basement

and surrounding areas of public realm. It is those buildings which are subject to the RMAs discussed in this report.

26. Plot A lies to the west of Surrey Quays Road. The land is currently occupied by Unit 1 of the Canada Water Retail Park, occupied by Dock X on a temporary basis, along with surface level parking.
27. The Maritime Street Site is 0.15 hectares and includes a section of Maritime Street, located between former Unit of the Canada Water Retail Park and the Porters Edge development.
28. The image below shows the location of Plot A in the surrounding context with the adjacent British Land illustrative scheme. Plot B is not yet subject to Reserved Matters so will not be discussed in this report.

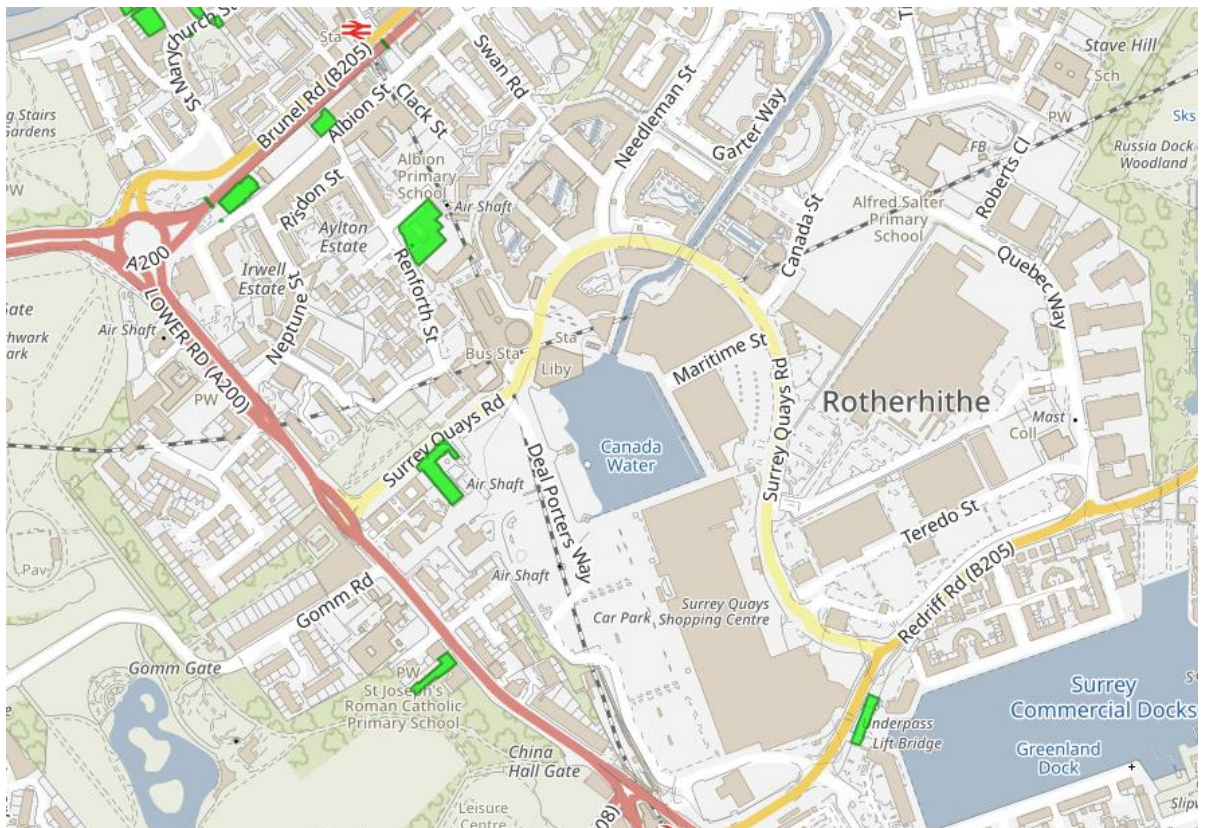






**Image above:** Location plan and aerial view with Plot A and B identified in red.

29. Plot A is bound to the north by Maritime Street (beyond which is the Porters Edge building), to the south by Surrey Quays Shopping Centre (existing but to be developed as part of BL Zone D), to the east by Surrey Quays Road and to the west by Canada Water Dock.
30. The site is not located within a Conservation Area nor within the curtilage of a Listed Building. There are listed buildings in the wider vicinity as shown annotated in green on the image below.



31. The following area designations apply:
- Canada Water Major Town Centre
  - Canada Water Opportunity Area

- Canada Water Action Area
  - Canada Water Strategic Heating Area
  - Air Quality Management Area
  - Flood Zone 2/3 (parts of the site)
  - Site Allocation NSP80
  - Identified Tall Building Location
  - Strategic Cultural Area
32. The site is located within the Canada Water Town Centre and is a designated Opportunity Area. The site lies away from the Transport for London Road Network (TLRN) and Strategic Road Network (SRN), the roads surrounding the site being borough roads, although the Rotherhithe Roundabout (TLRN) and Lower Road (SRN) are within around 600m of the site. Canada Water Underground and bus stations are within 300-400 metres providing access to Jubilee line services and London Overground services on the extended East London Line. A wide range of buses operate in the area which provides a public transport accessibility level (PTAL) of 6a, on a scale of 1 to 6b where 6b is the most accessible, indicating excellent accessibility to public transport.

### Surrounding Sites

33. Porters Edge

The Porters Edge development was completed in 2019 as Phase 1 of planning permission ref. 12/AP/4126 (subsequently varied by permission refs 15/AP/2821, 16/AP/0200 and 17/AP/3694). It comprises 235 residential units, a retail store and offices (occupied by Decathlon) and other retail units.

34. Canada Water Masterplan

The Canada Water Masterplan covers an area of 21.27 hectares. Hybrid Planning Permission was granted in May 2020 under reference 18/AP/1604 for redevelopment of the town centre to create a diverse mix of retail, residential, leisure and cultural facilities. The Masterplan will deliver a series of buildings on plots focussed around three new public spaces. Zone F and Zone D are the closest plots that would have a direct relationship with the application site.





EXISTING BUILDING FROM DOCK EDGE



EXISTING SERVICING ACCESS



VEHICLE ACCESS POINT FROM SURREY QUAYS ROAD



VIEW FROM THE DOCK EDGE WALK



VIEW TO PORTERS EDGE FROM SOUTHWEST CORNER



VIEW TO SURREY QUAYS ROAD FROM SOUTHWEST CORNER

*Image above: Existing site conditions.*

## Details of proposal

35. Permission was granted under 21/AP/2655 for:

Outline planning permission (all matters reserved) for a commercial masterplan containing offices and other complementary town centre uses with no housing. The proposals involve the demolition of all buildings and structures and the comprehensive redevelopment of the site to provide three Development Plots (A1, A2 and B) which will contain three Buildings (A1, A2 and B) above ground with

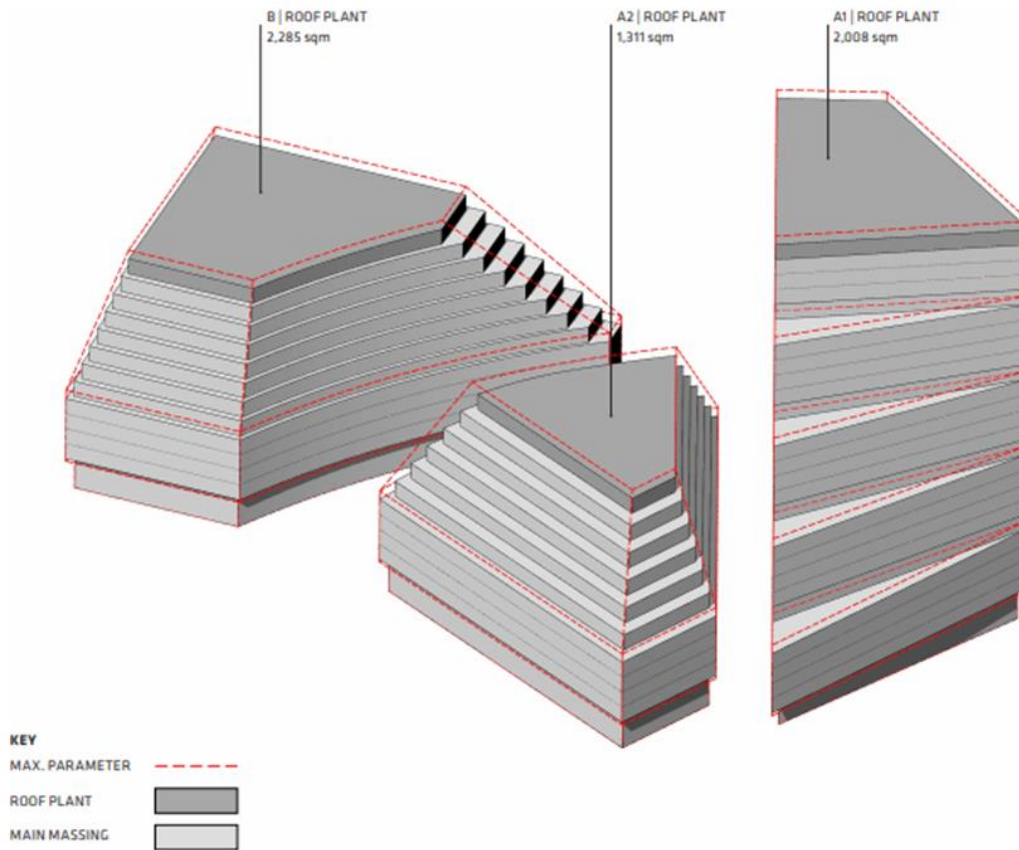
basements. The proposed land uses are: offices (Class E), retail/professional services/food and drink (E), learning and non-residential institutions/local community (F1/F2), medical or health (E) and indoor sport, recreation or fitness (E). - Works of hard and soft landscaping are proposed to create a series of new and improved public realm spaces within the site. This will include the replacement of some trees and the planting of additional new trees. - New vehicular access points are proposed to be created from Surrey Quays Road and Canada Street, along with other incidental works.

And under 21/AP/2610 for:

Outline planning permission (all matters reserved) for works of hard and soft landscaping to create a shared public realm space for use by pedestrians and vehicles. The proposals will retain and re-provide access for servicing vehicles to Porters Edge, and will improve the street through the provision of new trees and other planting, alongside new street furniture and surface finishes to enable the use of the space for play and recreation.

36. The outline permission was granted subject to various parameter plans which established an overall block structure and a maximum 3D building envelope within which each building would sit. Specific plans dictate the maximum extent of individual buildings which have been designed with set backs on the ground floor as well as set backs and overhangs on the upper levels. The plans dictate the minimum amount of public realm, extent of basement and maximum building heights. They also indicate locations for access points and appropriate land uses within each building. In addition to the parameter plans a set of design codes were approved ensure design quality in terms of the form, function and architectural appearance of the development. The Design Code documents set out the detailed design principles against which any subsequent Reserved Matters application should be assessed.





**Image:** Building envelopes created by approved Outline parameter plans

37. A non-material amendment application was granted under reference 23/AP/0571 in July 2023 for amendments to the Parameter Plans and Development Specification in respect of updating the basement split between Plot A1 and A2 (no change to overall basement size), clarification over plant space GEA to ensure that the Development Specification is in full accordance with the approved Parameter Plans for Building A1, A2 and B, amended split between Buildings A1 and A2, alterations to the Maximum Parameter Building Line on the southern façade of Plot A1 and soffit spot heights on A1 and finally amendments to the tree retention plan.
38. The Reserved Matters Applications (RMAs) subject of this report cover the matters of access, appearance, landscaping, layout and scale.
39. For clarity these comprise:
- ‘Access’ – the accessibility to and within the site, for vehicles, cycles and pedestrians in terms of the positioning and treatment of access and circulation routes and how these fit into the surrounding access network.
- ‘Appearance’ – the aspects of a building or place within the Development which determine the visual impression the building or place makes, including the external built form of the development, its architecture, materials, decoration, lighting, colour and texture.
- ‘Landscaping’ – the treatment of land (other than buildings) for the purpose of enhancing or protecting the amenities of the site and the area in which it is situated;

‘Layout’ – the way in which buildings, routes and open spaces within the Development are provided, situated and orientated in relation to each other and to buildings and spaces outside the Development.

‘Scale’ – the height, width and length of each building proposed within the Development in relation to its surroundings

40. The OPP defines a range of potential uses and maximum floorspace caps which can be delivered within the development (including Plot B) as well as an overall cap for the Masterplan site. The proposed quantum of development and range of land uses proposed within the RMAs accords with the details approved at Outline stage (with minor changes regularised by the subsequent NMA). The table below also demonstrates that sufficient space is available for appropriate development to come forward in Plot B.

Land Use	Consented floorspace (sqm GEA)	Plot A1 RMA	Plot A2 RMA	Remaining floorspace (sqm GEA)
Retail/professional services/food and drink (E(a/b/c))	5,000	777	808	3,415
Learning and non-residential institutions/local community (F1/F2)	300 - 750	0	0	300 - 750
Medical or health (E(e))	3,000	0	0	3,000
Indoor sport, recreation or fitness (E(d))	1,500	0	0	1,500
<b>NON-WORKPLACE SUB-TOTAL</b>	<b>2,000 – 7,000</b>	<b>777</b>	<b>808</b>	<b>415 – 5,415</b>
Workplace (E(g)(i))	143,780	59,273	33,853	50,654
<b>ABOVE GROUND SUB-TOTAL</b>	<b>145,780</b>	<b>60,050</b>	<b>34,661</b>	<b>51,069</b>
Roof Plant	5,604	822	582	4,200
Basements	13,006	3,661	2,131	7,214
<b>TOTAL</b>	<b>164,390</b>	<b>64,533</b>	<b>37,374</b>	<b>62,898</b>

*Image: Approved range and quantum (sqm GEA) of development for the Masterplan site (this includes Plot B) together with confirmed proposals for Building A1 and A2*

## **Buildings**

41. Building A1 would be located on the southern section of Plot A, with a frontage to the Canada Water Basin, Dock Edge Walk and Surrey Quays Road. This would be the tallest building within the proposed development with maximum height of up to 110 metres AOD. The principal land use will be workspace (E(g)(i)) with complementary town centre uses at the ground floor.
42. Building A2 would be located on the northern section of Plot A, adjacent to the Porters Edge development. A2 is the lowest building within the CWD Masterplan with a

maximum height of up to 55.4 metres AOD. The principal land use will be workspace (E(g)(i)) with complementary town centre uses (retail and food & beverage) at the ground floor.

43. A linked basement will be provided for both Buildings A1 and A2 accessed from Surrey Quays Road.

### **Maritime Street and New Public Realm on Plot A**

44. A scheme of soft and hard landscaping is proposed for Maritime Street with retained provision for access for servicing vehicles to Porters Edge. Access to Maritime Street is intended to be limited to refuse collection, emergency vehicles, and occasional deliveries associated with the Porters Edge development (including residential move ins/outs and the dockside retail unit). It is noted that Decathlon has its own dedicated servicing yard accessed from Surrey Quays Road to the north of the Canada Street junction, with egress then via the signalised junction itself. The quality of the street would be enhanced through the provision of new trees, planting, street furniture, lighting and opportunities for play.
45. A range of external spaces were approved as part of the OPP. The 'extent of public realm' parameter plan identifies the minimum areas of public realm to be delivered as part of the masterplan which includes the minimum size of Waterfront Square. In order to meet the minimum specification for Waterfront Square it is necessary for Building A2 to be designed with the 9m limit of deviation setback on the western façade. The detailed design put forward in the current RMA accords with this 9m setback.
46. As part of the current RMA proposals external spaces to come forward would comprise Waterfront Square as the main area of public realm (to be delivered with Building A2). The square is intended to be a multi-functional space that will provide a place for informal gathering, events, spill out for adjacent commercial units and an opportunity to link the development to the Canada Water Dock. The Boulevard, which is a diagonal pedestrian route between Buildings A1 and A2 linking Deal Porters Square and the dockside to Surrey Quays Road, enhancements to Surrey Quays Road and the eastern Dock Edge.

### **Consultation responses from members of the public and local groups**

47. Letters were sent to 725 local residents within 100m boundary of the site, the application was advertised as EIA development in the local press and numerous site notices were erected within the vicinity of the site.
48. A total of 12 representations have been received split across the various RMAs as follows
- 23/AP/0562 – 3 Support and 4 Objections
  - 23/AP/0564– 2 Support and 1 Objection
  - 23/AP/0565 – 1 Support and 0 Objections
  - 23/AP/0566– 1 Support and 0 Objections

The comments have been summarised in the table below.

Objections	Officer Response
<p>Design</p> <ul style="list-style-type: none"> <li>• The development is too high</li> <li>• The development is too dense</li> <li>• Out of keeping with the character of the area</li> <li>• Overdevelopment</li> </ul>	<p>The detailed proposals accord with the parameters established by the Outline permission in terms of height and density. At the time of determining the Outline permission it was considered that the redevelopment of this site as currently proposed would make an efficient and effective use of the site delivering a significant quantum of commercial development appropriate for this town centre location. The proposals accord with relevant development plan policies, they respond well to the urban character of the area and do not amount to overdevelopment of the site.</p>
<p>Amenity</p> <ul style="list-style-type: none"> <li>• Loss of light – a study should be undertaken</li> <li>• Too close to adjoining properties</li> <li>• Loss of privacy</li> <li>• Noise nuisance</li> <li>• Harm during construction</li> </ul>	<p>A full daylight/sunlight assessment was undertaken as part of the Outline application. The impacts in this respect were deemed to be acceptable, the detailed RMA proposals do not give rise to any additional impacts beyond those previously found to be acceptable.</p> <p>The position of the buildings and relationship to existing and future neighbours was also duly considered and deemed to be acceptable at Outline stage. The detailed proposals accord with the Outline proposals in this respect.</p> <p>Noise impacts during construction and operation were duly considered as part of the Outline permission and relevant conditions have been attached to limit harm by way of noise as well as a requirement for detailed Construction and Environmental Management Plans to be submitted to minimise harm during construction.</p>
<p>Community impacts</p> <ul style="list-style-type: none"> <li>• Strain on existing facilities</li> <li>• More open space needed on the development</li> </ul>	<p>Within Plot B of the development it is proposed to provide a health hub and community uses.</p> <p>The impact of the development on existing community infrastructure was considered</p>

	<p>as part of the Outline permission and appropriate obligations were secured to mitigate the impact of developing the site. The mitigation secured as part of the Outline permission includes onsite provision of public realm/open space, affordable retail and workspace units within the buildings, use of local employment during construction and end use employment opportunities.</p> <p>Offsite enhancements are proposed to the Eastern Dock Edge and Albion footbridge.</p> <p>A package of financial contributions was also secured to mitigate the impact in terms of public transport.</p>
General dislike of the proposal	Noted.
<p>Objection from British Land</p> <p>British Land is interested in the AIRE proposals to ensure that they make a positive contribution to the new Town Centre to be created by the consented CWM proposals and to protect the future CWM development from being adversely impacted by the AIRE proposals.</p> <p>British Land's comments relate to the applications regarding Building A1 (ref. 23/AP/0562) and the Eastern Edge of Canada Water Dock (ref. 23/AP/0798) only.</p> <p>The following matters are of concern/require further clarification</p> <p>Lack of articulation on the southern façade of A1 and the impact this will have on the important public route between A1 and Zone D</p> <p>Cycle crossing onto Printworks Place creates conflict for pedestrians/cyclists</p>	<p>The applicant responded to each of the points raised providing additional clarity.</p> <p>Officers response:</p> <p>As with any building it is necessary to incorporate 'back of house' functional requirements such as ventilation, plant, emergency exits and service areas. These necessary elements have mostly been located on the southern façade of the building which is appropriate given the need to maximise activation onto the Boulevard, Dock Edge and Surrey Quays Road as the main thoroughfares and areas of public realm and accords with the OPP parameter plans. The design of the southern façade successfully accommodates these necessary elements whilst still maximising active frontage with the inclusion of commercial uses on both ends of the façade. The quality of the louvres, glazed and non-glazed elements will be controlled via the conditions already attached to the OPP in respect of material samples, large scale drawings and mock-ups.</p> <p>The suggested cycle crossing onto Printworks Place has been discussed with</p>

<p>CEMPs would need to take account of surrounding developments taking place at the time</p>	<p>Highways officers who have confirmed there are no immediate concerns with this but the exact location of any crossing would be reviewed as part of s278 works</p>
<p>The temporary wind mitigation proposals (required in the event that Zone D of the BL scheme is not built out before A1 is ready for occupation) need to be fully understood (what is proposed/where will it be located and is it fit for purpose)</p>	<p>CEMPs are controlled via the s106 agreement attached to the OPP. It will be a requirement for to coordinate with adjacent developments under construction. This is managed by the Network Management Team.</p>
<p>It is proposed to drain into the Dock but this will require land owner approval from BL. The Council must be satisfied that there is a drainage solution that can be achieved if consent is not given to drain into the dock</p>	<p>Additional details were submitted in respect of the proposed temporary wind mitigation (discussed in detail in the report below). The proposed strategy is considered to be acceptable subject to detailed wind modelling which is controlled via the recommended pre commencement condition</p>
<p>If permission is granted to discharge into the dock it should be subject to a condition that ensure that surface water is treated in line with CIRCA SuDs guidance</p>	<p>There is a condition attached to the OPP to secure detailed drainage proposals (17). At this stage the applicant is proposing to drain into the dock which is acceptable to the Council but a fall back position of connection to a mains sewer (after onsite attenuation and reduction) is adequately secured by the existing condition.</p>
<p>Surprised that the proposals for enlarging the Albion Footbridge are not included in this submission</p>	<p>As part of discharging Condition 17 the applicant will be required to demonstrate that any eater discharged into the dock meets CIRCA SuDs guidance</p> <p>The Outline application included an assessment of Pedestrian Comfort Levels (PCL) in accordance with TfL Guidance. The assessment demonstrated that it is necessary to widen the existing Albion footbridge to provide comfort levels in line with TfL recommended PCL guidance in order to accommodate the additional capacity which will be generated by the proposal. These improvements have been secured in the s106 agreement and details will be submitted to the local planning authority in due course.</p>

	The details are required to be submitted for approval ‘prior to above ground works on Building A1 commencing’. It is understood that the applicant is about to embark on a design competition to commence work on the detailed proposals. This would enable sufficient time for the obligation to be met.
<b>Comments in support</b>	
<ul style="list-style-type: none"> <li>• Supported by The Friends of Southwark Park Charity</li> <li>• General support for the proposals</li> <li>• High quality design</li> <li>• High quality materials / detailed design</li> <li>• Improves surrounding public realm</li> <li>• Improves the quality of the area</li> <li>• Provides amenity space</li> <li>• Provides commercial uses</li> <li>• Provides new landscape, planting &amp; trees</li> <li>• Provides new public spaces</li> <li>• Welcome regeneration of Canada Water</li> <li>• High quality landscaping will be hugely important to the success of the scheme</li> <li>• New public spaces for the community, including new places to shop, eat and drink and activities up and down Maritime Street</li> <li>• The development will create more opportunities for local people</li> <li>• This area is been derelict for too long.</li> <li>• New landscaping and greenery supporting the biodiversity in the area.</li> <li>• Somewhere to sit and relax and enjoy the area, this area needs to be opened up for local people to enjoy to relax and sit and bring their families out to enjoy this part of the dock, this part of the Dock has been neglected for too long and this design will bring it back, so people can use it to its full potential</li> <li>• This area really does need tidying and these new creative designed buildings will create a better outlook for local residents in the area</li> <li>• New Waterfront Square, which will provide a space for the community to gather and socialise.</li> </ul>	

## Planning history of the site, and adjoining or nearby sites

49. The site benefits from Outline Planning Permission for a commercial led redevelopment comprising 3 buildings with associated public realm under references 21/AP/2655 and 21/AP/2610 – full details are available on the planning register.

23/AP/0571 - Non material amendment to planning permission Ref 21/AP/2655 dated 29/01/2021 for Outline planning permission (all matters reserved) for a commercial masterplan containing offices and other complementary town centre uses with no housing. The proposals involve the demolition of all buildings and structures and the comprehensive redevelopment of the site to provide three Development Plots (A1, A2 and B) which will contain three Buildings (A1, A2 and B) above ground with

basements. The amendment sought the following

- Updated basement split between Plot A1 and A2 (no change to overall basement size)
- Clarification over plant space GEA to ensure that the Development Specification is in full accordance with the approved Parameter Plans for Building A1, A2 and B
- Amendment to the Above Ground GEA split between A1 and A2
- Maximum Parameter Building Line proposed to extend 250mm into the Wind Mitigation Zone for the full extent on the southern façade of Plot A1 (outer edge of Wind Mitigation Zone remains as originally approved) and
- Amended soffit spot heights on A1
- Two Additional tree removals

50. Other relevant applications include:-

18/AP/1604 – Hybrid planning permission for the comprehensive redevelopment of the Canada Water Masterplan (British Land).

## **KEY ISSUES FOR CONSIDERATION**

### **Summary of main issues**

51. The main issues to be considered in respect of this application are:

- Principle of the proposed development in terms of land use including affordable workspace;
- Conformity with Outline Permission
- Environmental impact assessment
- Design, including layout, scale and appearance
- Heritage considerations
- Landscaping and ecology
- Archaeology
- Impact of proposed development on amenity of adjoining occupiers and surrounding area, including privacy, daylight and sunlight
- Transport and highways, including servicing, car parking and cycle parking
- Environmental matters, including construction management, flooding and air quality
- Energy and sustainability, including carbon emission reduction
- Planning obligations (S.106 undertaking or agreement)
- Mayoral and borough community infrastructure levy (CIL)
- Consultation responses and community engagement
- Community impact, equalities assessment and human rights

52. These matters are discussed in detail in the ‘Assessment’ section of this report.

### **Legal context**

53. Section 38(6) of the Planning and Compulsory Purchase Act (2004) requires planning applications to be determined in accordance with the development plan, unless material considerations indicate otherwise. In this instance the development plan



comprises the London Plan 2021 and the Southwark Plan 2022. Section 72 of the Planning (Listed Buildings and Conservation Areas) Act 1990 requires decision-makers determining planning applications for development within Conservation Areas to pay special attention to the desirability of preserving or enhancing the character or appearance of that area. Section 66 of the Act also requires the Authority to pay special regard to the desirability of preserving listed buildings and their setting or any features of special architectural or historic interest which they possess.

54. There are also specific statutory duties in respect of the Public Sector Equalities Duty which are highlighted in the relevant sections below and in the overall assessment at the end of the report.

### **Planning policy**

55. The statutory development plans for the Borough comprise the London Plan 2021 and the Southwark Plan 2022. The National Planning Policy Framework (2021) constitutes a material consideration but is not part of the statutory development plan. A list of policies which are relevant to this application is provided at Appendix 2. Any policies which are particularly relevant to the consideration of this application are highlighted in the report.

## **ASSESSMENT**

### **Principle of the proposed development in terms of land use**

#### Relevant policy designations

56. The site is within the Canada Water Opportunity Area, which the London Plan describes as aiming to deliver 20,000 jobs and the Canada Water Major Town Centre will provide at least 40,000sqm (net) new retail uses. Site allocations in Canada Water and Rotherhithe have enormous potential to provide new homes and commercial space, particularly in and around the Canada Water town centre.
57. The site is located within AV.15 Rotherhithe Area Vision of the Southwark Plan 2022. This states that development in Rotherhithe should:
- Create a new destination around the Canada Water Dock which combines shopping, civic, education, and leisure, business and residential uses.
  - Provide as many homes as possible of a range of tenures including social housing while respecting the local character. There will be opportunities for taller buildings on key development sites;
  - Transform Canada Water into a new heart for Rotherhithe with a new leisure centre, shops and daytime and evening events and activities around the Dock and in the Harmsworth Quays Printworks.
  - New retail space will be provided including a new department store and independent shops, offices and places to eat and drink;
  - Provide new education opportunities and health services which will include new school places and a health centre with GPs and could include colleges and universities;
  - Complement and improve the historic character, including the docks, and the unique network of open spaces, water and riverside;
  - Prioritise walking and cycling and improve public transport, including improved links to Southwark Park, the river, boat services and docks, completion of the

Thames Path, a new river crossing to Canary Wharf, better circulation of buses, enhanced cycle routes to support expansion of cycle hire to the area and creating 'healthy streets';

- Improve traffic flow on the road network, particularly on Jamaica Road and Lower Road;
- Provide a range of flexible employment spaces, including premises suitable for smaller businesses;
- Improve roads, pavements and cycleways, particularly the local environment around Albion Street and Lower Road.

58. The site lies within Southwark Plan Allocation 80: Decathlon site and Mulberry Business Park. The site allocation states

"Development of the site must:

- Provide retail uses; and
- Provide community uses; and
- Provide enhanced public realm and civic space; and
- Provide employment floorspace (E(g), B class).

Development of the site should:

- Provide new homes (C3).

Development of the site may:

- Provide leisure uses; and
- Provide student accommodation (sui generis).

59. Canada Water is a major town centre destination which combines shopping, civic, education, leisure, business and residential uses. Much of the current environment is designed to accommodate trips made by cars. The aspiration is to create high quality streets and spaces that are not dominated by car use or by car parking.

60. Development on these sites will be expected to maximise the amount of employment space and its contribution to the regeneration of the town centre. The site should accommodate improved walking routes to Canada Water Station and to public open spaces. The scheme should provide links to existing cycle routes and proposed Cycle Super Highway (if the scheme is provided).

61. Comprehensive mixed-use redevelopment of the site could include taller buildings subject to consideration of impacts on existing character, heritage and townscape".

62. The proposed commercial uses in Building A1 and A2 fully accords with the site allocation and approved OPP.

### Existing Land Use

63. The existing building on Plot A is currently used as for events and entertainment purposes, this is a meanwhile use of the site.

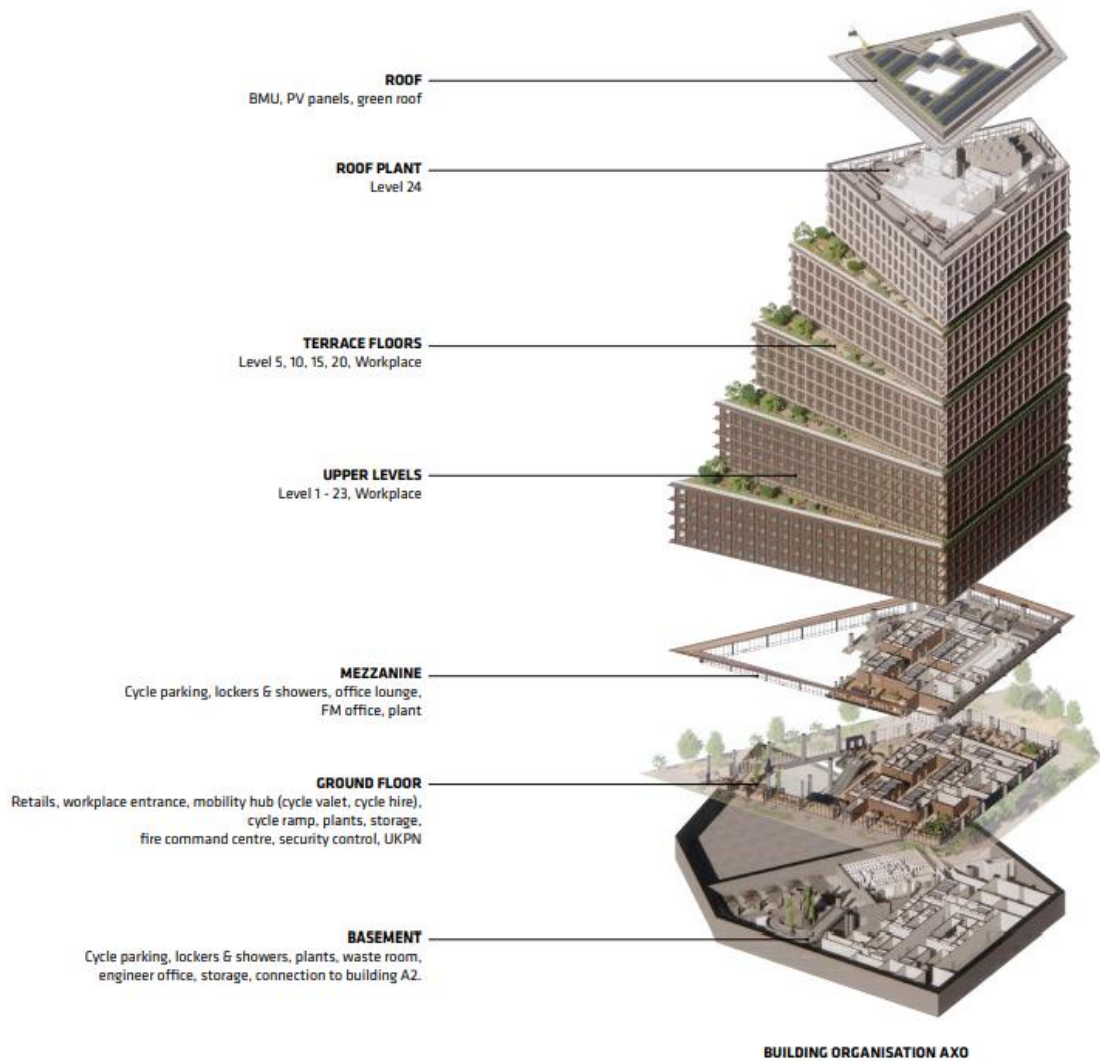
64. Development plan policies recognise the benefits of providing temporary/meanwhile uses to make the most of vacant land/buildings. Cultural and entertainment uses can be particularly successful meanwhile uses within phased redevelopment projects.

Whilst such uses have an important role to play, a successful temporary use should not prevent permanent redevelopment.

65. The existing tenants are aware that the site is proposed for redevelopment and it is understood that their lease arrangements reflect this.
66. The OPP allows for the demolition and redevelopment of this site. As such the loss of the existing uses was accepted at the time of granting the OPP.

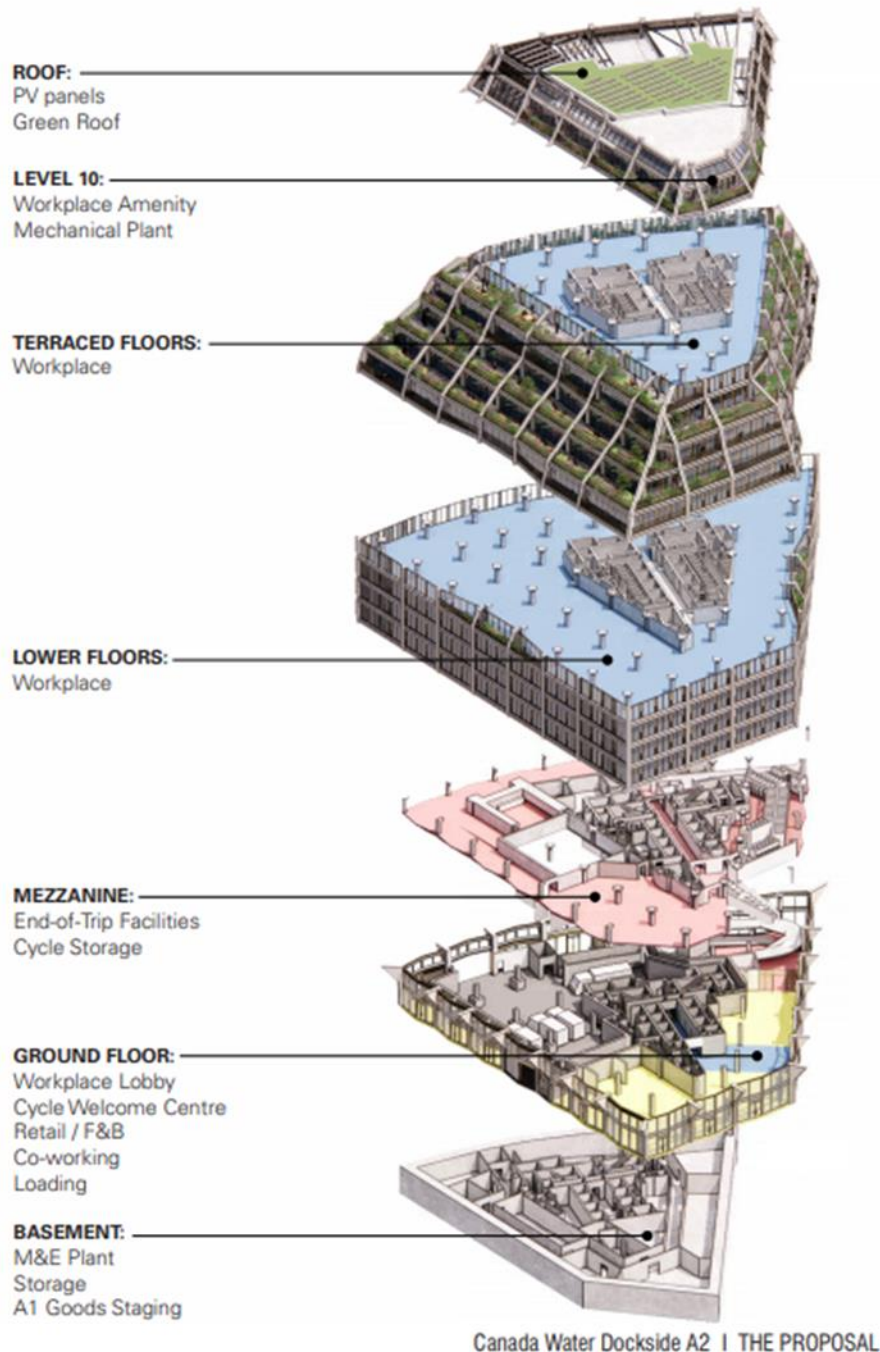
### Employment uses

67. Promoting the economy and creating employment opportunities is key priority for the planning system. The site lies within a London Plan Opportunity area (Policy SD1) and within a defined Major Town Centre (Policy SD6). London Plan Policy GG5 requires local planning authorities to plan for sufficient employment and industrial spaces to support economic growth whilst Policies E1 and E2 deal specifically with the provision of B Use Class (now called Class E(g) since the change to the Use Classes order in 2021) space. London Plan Policy E11 requires development proposals to support employment, skills development, apprenticeships, and other education and training opportunities in both the construction and end-use phases.
68. Southwark Plan Policy SP4 seeks to ensure that Southwark can develop a strong, green and inclusive economy. To achieve this the development plan aims to deliver at least 460,000sqm of new office space between 2019 and 2036 (equating to around 35,500 jobs). The policy states that around 80% of new offices will be delivered in the Central Activities Zone. Additional offices will be delivered in the Canada Water and Old Kent Road Opportunity Areas and in town centres, the policy sets a target of 20,000 jobs to be delivered in Canada Water. Policy SP4 further requires 10% of all new employment floorspace to be affordable workspace for start-ups and existing and new small and independent businesses in Southwark. Finally, the policy identifies Canada Water as appropriate for delivering 40,000sqm of retail floorspace.
69. Building A1 is intended to accommodate a large office lobby, mobility hub and retail/food and beverage uses on the ground floor with office space above.



**Image above:** Proposed layout of spaces within Building A1

70. Building A2 is intended to accommodate retail uses on the ground floor with office space on the upper levels.



**Image above:** Proposed layout of spaces within Building A2

71. The proposal to deliver a significant quantum of workspace is entirely consistent with the OPP.
72. This proposal has the potential to deliver 3,450 – 4,485 FTE jobs in Building A1 and 1,985 – 2,580 in Building A2. This level of employment would make a valuable contribution to the Borough and should be regarded as a significant positive benefit of the scheme.
73. A policy compliant provision of construction and end use employment and training opportunities for local people has been secured as part of the OPP (Schedule 7). This equates to 202 jobs, 202 short courses, and 49 apprentices during construction and 592 end of use jobs.

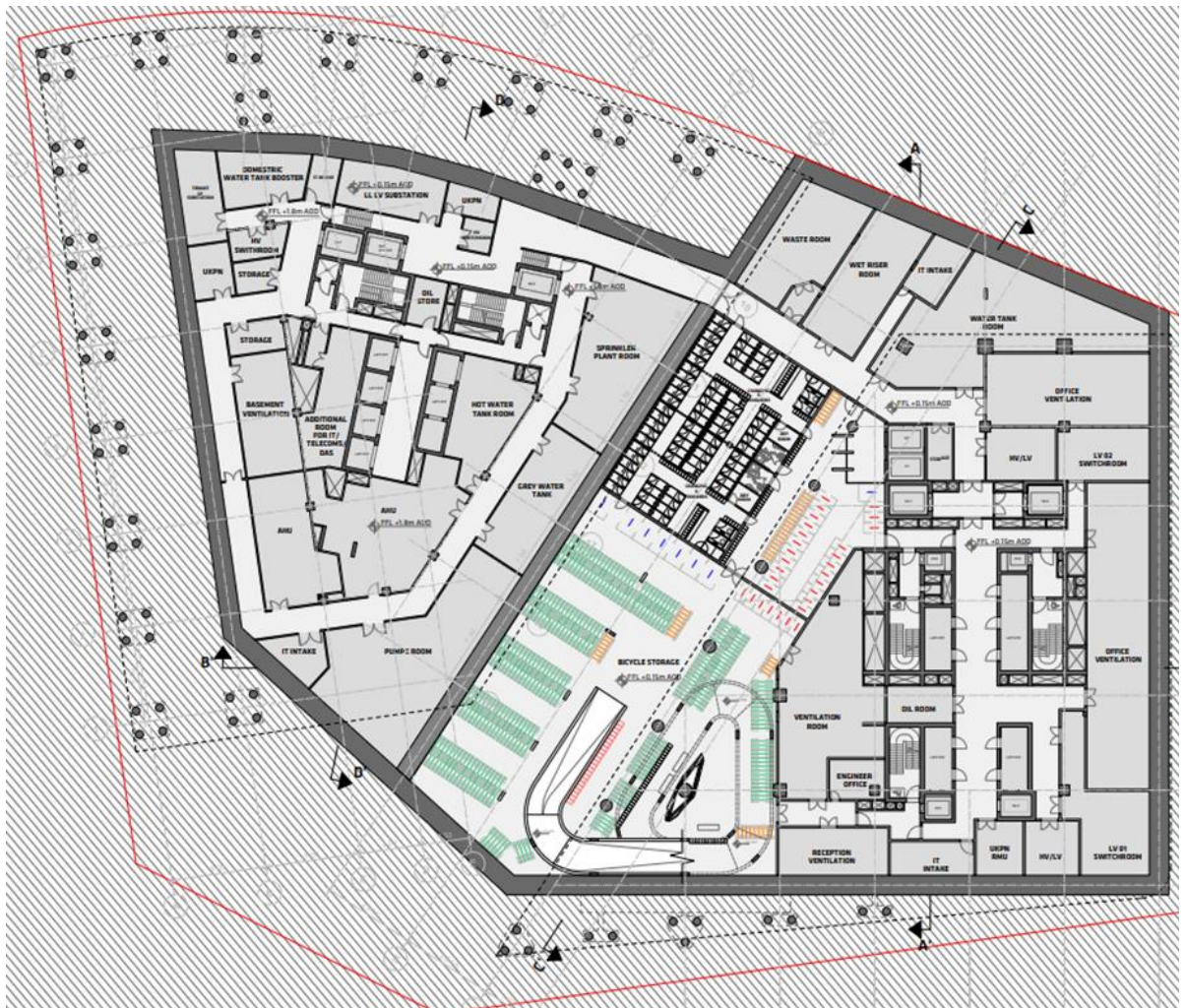
## Retail, professional services and food & drink

74. The Outline Planning Permissions approved a range of non-workplace uses, with a combined minimum area of 2,000 sqm Gross External Area (GEA) and a combined maximum area of 7,000sqm GEA. This included a maximum amount of retail/professional services / food and drink (Use Classes E(a/b/c)) of 5,000 sqm GEA distributed across Development Plots A1, A2 and B. These land uses would be supported at ground or mezzanine floor levels.
75. Plot A1 proposes to provide 777sqm (GEA) of retail/food and drink floorspace at ground levels. Whilst Plot A2 proposes to provide 808 sqm (GEA) of retail/ food and drink floorspace at ground levels. This provides a total of 1,585sqm (GEA) which would leave a minimum of 415sqm and maximum of 5,415 sqm (GEA) of non-workspace uses to be provided within Plot B.
76. This application proposes a series of smaller units located along the ground floor facades of Building A1 and A2 which are intended to be occupied for retail uses. As a town centre location it is entirely appropriate for this development to include a range of retail/café uses. The proposed location will activate the edges of the buildings and the adjacent public realm creating a positive relationship with the Boulevard, Waterfront Square and Dock Edge.
77. Southwark Plan Policy P35 sets out the requirements for new retail development within town centres. For a development of this scale it is necessary for the proposal to include toilets, public drinking fountains and public seating. These features have already been secured within the s106 legal agreement attached to the OPP to which this RMA will be bound. The plans submitted with the RMAs identify public toilet provision within Building A2 and drinking fountains in Maritime Street and the Boulevard.

## Basement

78. The OPP allowed for a maximum amount of basement space below Buildings A1 and A2. The maximum basement finished floor level depth for all Development Plots was set as 0.15m AOD with the maximum excavation level expected to be 2m below the floor level, with the exception of areas in each plot for lift pits and access stairs which can extend up to 5m below the finished floor level. It is proposed to provide a combined basement that comfortably sits within the approved parameters. The basement will be accessed via a single point of access/egress from Surrey Quays Road adjacent to Maritime Street and will accommodate uses that are ancillary to the above ground uses, including plant, servicing and storage.





**Image above:** Basement layout

79. The two basements are single storey but have different depths to address different roles and requirements for building services. This difference in levels is accommodated by a ramped connection between the buildings.
80. The Basement RM application included a Basement Impact Assessment (BIA). The assessment covers the following:
- Impact of the proposals on surface water flow and flooding;
  - Impact of the proposals on groundwater flow, levels and quality;
  - Impact of the proposals on structural stability, including potential impact on adjacent and/or nearby properties;
  - Impact on archaeology;
  - The identification of suitable construction methods and mitigation measures for development;
  - A method for monitoring local ground conditions, water movement, subsidence and drainage; and
  - The cumulative impact of basement development (built or proposed) in the surrounding area.
81. Matters relating to surface water, drainage and contamination have been assessed by relevant statutory consultees and such impacts are discussed in detail in the relevant sections of this report below.

82. The report identifies relevant nearby assets such as the underground tunnel and Thames Water assets and confirms that the applicant has engaged with relevant organisations in this respect.
83. The report concludes that subject to proposed construction techniques and further site investigations and construction monitoring no adverse impacts will arise from construction of the basement, there are already conditions attached to the OPP to secure the following:
- Land contamination assessments
  - Detailed SUDs proposals and drainage verification
  - Thames water connections
  - Piling designs
  - Archaeological evaluation
  - Mechanical ventilation for the basement
84. The detailed design for the basement in terms of size and use accords with the OPP and sufficient technical details have been provided at this stage to demonstrate that subject to control and mitigation no harm will arise during construction. The proposal is therefore acceptable in this respect.

### Affordable workspace

85. London Plan Policy E2 requires the provision of a range of low-cost Class B1 business space to be supported to meet the needs of micro, small and medium sized enterprises and to support firms wishing to start up and expand.
86. Policy E3 of the London Plan deals specifically with affordable workspace and identifies the circumstances in which it would be appropriate to secure affordable space.
87. Southwark Plan Policy P31 deals with affordable workspace. Criterion 2 of the policy requires Major 'B Use Class' development proposals to deliver at least 10% of the floorspace as affordable workspace on site at a discounted market rent for a period of at least 30 years.
88. The OPP proposed a policy compliant level of affordable workspace being 10% of the overall floorspace (GIA) as affordable space. However, due to the large quantum of space to be delivered within the Masterplan the Outline Permission allows for the provision of affordable retail as well as affordable office space.

The s106 secured the following:-

- **Quantum of space and split across the Proposed Development** – 10% of total GIA for each building to be provided as affordable workspace. The wording of the s106 allows for Buildings A1 and A2 to be treated as one plot for the purposes of delivering this space in the event that these buildings are substantially implemented around the same time; provided that the overall aggregate space equates to 10% of the commercial floorspace within the two buildings.
- **Level of discount applied** – for a minimum of 30 years at a minimum of 25% reduction on market rent for office space and a greater discount for affordable



retail/food and drink or medical hub

- **Cap on non-office affordable workspace** – no more than 50% of the affordable Gross Internal Area (GIA) to be used for alternative affordable space
- **Specification of space delivered** - Fitting out specified
- **Service charge capped** – £4.50 per sq ft
- **Rent free periods** – 6 months
- **Occupation restrictions for market space** - no more than 50% of the market rate floorspace can be occupied within each building until the affordable workspace within that building has been fitted-out ready for occupation
- **A Management Plan** - detailed strategy required to secure the appointment of a Workspace Provider and a methodology for that Provider to support the occupiers and appropriate marketing of the affordable workspace.

89. The RMA proposes to deliver the following workspace/retail elements.

	GEA	GIA	10% of GIA	NIA (75% of GEA)	10% of NIA
A1	59,273	56,763	5,676	44,455	4,445
A2	33,853	32,619	2,362	25,390	2,539
<b>TOTAL</b>	<b>93,126</b>	<b>89,382</b>	<b>8,938</b>	<b>69,845</b>	<b>6,984</b>

90. As such, a total of 8,938sqm GIA of Affordable Workspace is required to be provided between Buildings A1 and A2. For ease given the identification of internal areas within the buildings, NIA is used as the principal measure, but this will equate to GIA through the leasing of ancillary space required to support these net areas. the total NIA required is 6,984 sqm

91. The following amount of affordable workspace is proposed within the RMA applications:

- 6,025sqm NIA of Affordable Workspace in Building A1, comprising 5,539sqm NIA of workspace and 486sqm NIA of Alternative Affordable Workspace in the form affordable retail space.
- 959sqm NIA of Affordable Workspace in Building A2, comprising 223sqm NIA of workspace and 736sqm NIA of Alternative Affordable Workspace in the form of affordable retail space

92. This represents a combined provision of 10% across Buildings A1 and A2. Whilst the provision of Alternative Affordable Workspace in Building A2 exceeds the 50% allowance for this building in isolation, the s106 agreement allows for A1 and A2 to be treated as one plot for the purposes of affordable workspace provisions provided the buildings are being substantially delivered at the same time. As the proposed phasing plan demonstrates Buildings A1 and A2 will be under construction at the same time and consequently it is acceptable to consider the affordable workspace as one phase. The provision of Alternative Affordable Workspace across both A1 and A2 would amount to 18% of the total Affordable Workspace with 82% proposed as traditional workspace. This accords with the S106 obligation.

93. The affordable workspace within Building A1 will comprise offices, integrated with the open market office accommodation within the building. It is proposed to reserve confirmation of which floors the space would be located on until the submission of the Affordable Workspace Strategy, so as to avoid impacting upon the commercial pre-letting process. An operator would also be identified at this stage. This is an acceptable approach as all of the office space has been designed to be of the same quality and specification regardless of tenure.
94. In addition to the office space on the upper floors, Plot A1 also proposes 486sqm NIA of affordable retail floorspace at ground level – one fronting onto Printworks Place and one within the active lobby space on the Dock Edge. Due to the large size of the unit on Printworks Place, the demand from eligible tenants will need to be tested and therefore a fallback of providing the equivalent space as affordable office accommodation is sought. The unit on the Dock Edge is proposed as a potential affordable space, but this depends on whether an operator can be found to meet the commercial brief for this area. Again, if a suitable affordable tenant cannot be found, affordable office space would be provided instead. There are sufficient controls already in place within the s106 to secure appropriate occupiers.
95. The affordable workspace within Building A2 will comprise affordable retail within the following parts of the building:
- 215sq m NIA of affordable retail or Food & Beverage (F&B) uses within five self-contained units of between 39 and 55sq m each, along Maritime Street and Surrey Quays Road. These units have been sized specifically to attract retail start-ups and small local operators, reducing fit-out costs and commitments;
  - Approx. 466sq m NIA of affordable F&B space within a food hall or separate restaurant/café units fronting Waterfront Square. This is more likely to attract affordable tenants if a food hall concept can be delivered within the space;
  - The potential for an affordable F&B operator for the 'cycle cafe', comprising 30approx. 55sq m NIA fronting the Boulevard and Surrey Quays Road within the entrance to the Cycle Hub.

Alongside the Alternative Affordable Workspace, it is proposed to provide 223sq m NIA of affordable co-working space fronting the Boulevard, which is likely to be low-cost co-working space with subsidised memberships for affordable businesses.

96. As discussed above the principle of allowing affordable retail provision has been established by the OPP. The units within Plots A1 and A2 have been designed with affordable retail tenants in mind, and are thought to be likely to appeal to the following types of businesses in particular (but not exclusively):
- Coffee shop
  - Hairdresser
  - Barber
  - Beautician – nails, threading etc.
  - Pilates
  - Yoga
  - Chiropractor

- Physiotherapy

Many of these uses also offer amenity essential for office occupiers which will further underwrite the deliverability of office use in this new business location as well as serving the local community.

97. For the reasons set out above the proposal accords with the OPP in respect of affordable workspace provision.

### Land Use Summary

98. As discussed above the proposal is to deliver a commercial scheme comprising mainly office floor space but with other appropriate town centre uses. This is consistent with the approved OPP and would meet the requirements of the development plan policies discussed above.

### **Conformity with Outline Permission**

99. Following the determination of the Outline permission, it has been necessary to amend the Parameter Plans approved under 21/AP/2655 to facilitate the following amendments
- Updated basement split between Plot A1 and A2 (no change to overall basement size)
  - Clarification over plant space GEA to ensure that the Development Specification is in full accordance with the approved Parameter Plans for Building A1, A2 and B
  - Amendment to the Above Ground GEA split between A1 and A2
  - Maximum Parameter Building Line proposed to extend 250mm into the Wind Mitigation Zone for the full extent on the southern façade of Plot A1 (outer edge of Wind Mitigation Zone remains as originally approved) and
  - Amended soffit spot heights on A1
  - Two Additional tree removals
100. These minor amendments to the originally approved Parameter Plans have been regularised by way of a Non-material Amendment 23/AP/0671.
101. As discussed in detail in the land use section of this report, the information submitted with the RMAs demonstrates that the proposed floorspace and land uses for Buildings A1 and A2 are within the allowances established within the Development Specification. Furthermore, it has been demonstrated that whilst Plot B has not yet been developed in detail and is not subject to the current tranche of RMAs, if Plot A is built out as proposed within the submitted RMAs the Plot B illustrative scheme would remain achievable within the floorspace and land use allowances that were permitted within the CWD OPP.
102. The proposal accords with the OPP in terms of affordable workspace provision and quantum of public realm (discussed in more detailed in the relevant sections of this report).
103. The detailed design of the buildings in terms of the footprint and, layout of spaces, entry and exit points, façade treatment, architectural style and materials pallet and routes around and through the site accord with the principles established by the

Design Codes approved as part of the OPP.

104. Condition 2 (parts ii and iii) of the OPP requires a very specific minimum setback for Building A2 on the western façade adjacent to Waterfront Square. A 9m setback is required to ensure that an appropriate quantum of public realm can be delivered within the site boundary. Building A2 has been designed with a 9m setback from the maximum parameter on the west façade (at ground and dominant face), and Waterfront Square has been correspondingly designed to meet the minimum dimensions shown on the approved 'extent of public realm' parameter plan.

## **Environmental impact assessment**

### Regulatory framework

105. Environmental Impact Assessment is a process reserved for the types of development that by virtue of their scale or nature have the potential to generate significant environmental effects. The categories of development to which this applies, the size thresholds and selection criteria, are set out in the Town and Country Planning (Environmental Impact Assessment) (EIA) Regulations 2017.
106. The OPP was considered to be EIA development. An assessment of the likely significant environmental effects of the Canada Water Dockside Masterplan was reported in an Environmental Statement (ES) co-ordinated by Truim which accompanied the OPP, submitted in July 2021. The environmental topic areas that were assessed within the July 2021 ES were as follows:
- Socio-Economics;
  - Transport and Accessibility;
  - Noise and Vibration;
  - Air Quality;
  - Daylight, Sunlight, Overshadowing, Light Spillage and Solar Glare;
  - Wind Microclimate;
  - Archaeology;
  - Townscape, Heritage and Visual; and
  - Greenhouse Gases / Climate Change.
107. Condition 3 of the OPP requires each application for Reserved Matters to contain the information set out in the Reserved Matters Compliance Statement Checklist which includes the requirement for an Environmental Statement (ES) Statement of Conformity (SoC).
108. An ES SoC is a document that considers the details of the relevant RMA and explains the conformity of those details with the conclusions of the environmental impact assessments reported in the Canada Water Masterplan ES.
109. The SoC considers relevant legislation and relevant national, regional and local planning policy, and guidance and considers any updates to planning policy since the July 2021 ES and planning consent for topics where the requirement for further assessment work has been identified as required. Revisions to policy, legislation and any guidance effecting impact assessment methodology have been considered. There have been the following overarching changes to legislation and planning policy since the July 2021 ES:

- Environment Act 2021
- National Planning Policy Framework; and
- Southwark Local Plan

The introduction of this new legislation and policies do not materially alter the assessments undertaken or conclusions of the July 2021 ES which were in line with the EIA Regulations and all relevant legislation, policy and guidance at the time.

110. The RMA details for Buildings A1 and A2 including the basement and surrounding public realm have been reviewed against the Canada Water Dockside Masterplan ES by Truim and all technical specialists who contributed, to confirm that the details conform with the assessment of effects previously undertaken and the mitigation proposed remains proportionate and relevant. The review has identified that the RMA details would not alter the likely significant residual effects previously identified within the approved Canada Water Dockside Masterplan ES.
111. A non-material amendment application (NMA) was submitted under section 96A of the Town and Country Planning Act 1990 (as amended) in connection with the RMA for Buildings A1, A2 and the basement because the RMA details fall slightly outside the approved parameters of the Canada Water Dockside Masterplan (discussed further in the 'conformity' section of this report). The ES SoC therefore also considers the potential for these non-material amendments to result in any new or changed likely significant residual environmental effects to those identified within the Canada Water Dockside Masterplan ES.
112. The review has identified that the RMA, as amended by the NMA, would not alter the likely significant residual effects within the approved Canada Water Dockside Masterplan ES. However, since submission of the ES additional assessments have been undertaken in relation to wind and light pollution to inform this ES SoC. This additional assessment work, which is relevant to the RMA details was submitted as 'further environmental information' to supplement the existing Canada Water Dockside Masterplan ES.
113. The ES SoC confirms that there is no change to the basis of the demolition and construction assessments within the July 2021 ES, e.g., duration, sequencing, methodology or estimates of demolition and construction related material volumes, waste quantities or road traffic and traffic routing set out in the July 2021 ES.

### Socio economics

114. No further assessment is required as the July 2021 ES assessed the reasonable worst-case scenario for this topic, and the details being sought for approval fall within the parameters previously assessed. No information submitted with the RMAs will affect the conclusions of this assessment, nor introduce new environmental effects.

### Transport and Accessibility

115. No further assessment is required as the July 2021 ES assessed the reasonable worst-case scenario for this topic, and the details being sought for approval fall within the parameters previously assessed. No information submitted with the RMAs will affect the conclusions of this assessment, nor introduce new environmental effects.

## Noise and Vibration

116. No further assessment is required as the July 2021 ES assessed the reasonable worst-case scenario for this topic, and the details being sought for approval fall within the parameters previously assessed. No information submitted with the RMAs will affect the conclusions of this assessment, nor introduce new environmental effects.

## Archaeology

117. No further assessment is required as the July 2021 ES assessed the reasonable worst-case scenario for this topic, and the details being sought for approval fall within the parameters previously assessed. No information submitted with the RMAs will affect the conclusions of this assessment, nor introduce new environmental effects.

## Solar glare

118. The ES SoC provides a quantitative solar glare and light spill assessment based on the façade information included in the RMAs. The implications of the RMAs on the solar glare assessment considers impact on the following locations:
- Approaching junction between Surrey Quays Road and proposed Printworks Street from south, facing north – View 1;
  - Approaching junction between Surrey Quays Road and proposed Printworks Street from northeast, facing south-west – View 2;
  - Approaching junction between Surrey Quays Road and proposed Canada Street from south, facing north – View 3; and
  - Approaching junction between Canada Street and Surrey Quays Road from north-east, facing south-west – View 4.
119. The light spill and solar glare analysis has been undertaken for Buildings A1 and A2 cumulatively with the Consented British Land Canada Water Masterplan in the surrounding context to provide a likely worst-case scenario for each analysis and results that are most representative of the future position.

### 120. Viewpoint 1

The July 2021 ES concluded that the effect on this receptor would be minor adverse (not significant). The location represents a northward travelling road user on Surrey Quays Road approaching the junction of the future Printworks Place and Surrey Quays Road. The analysis shows that there is no solar glare arising from the Proposed Development and so the potential for additional solar glare at this location is Negligible and not significant. This effect is reduced from that identified in the July 2021 ES.

### 121. Viewpoint 2

The July 2021 ES concluded that the effect on this receptor would be minor adverse (not significant). The location represents a south-westward travelling road user on Printworks Place approaching the junction of the future Printworks Place and Surrey Quays Road. The analysis shows that there is the potential for some minor solar glare to occur between April and June. However, this is at the edge of the 30° circle and so not likely to cause any significant disabling glare. Additionally, this is a narrow façade with rhythmic breaks in the glazing and so any potential solar glare will likely be

fleeting and not sustained. Any additional glare arising from the Proposed Development at this viewpoint is likely to be Minor Adverse and Not Significant. This aligns with the effect identified in the July 2021 ES.

122. Viewpoint 3

The July 2021 ES concluded that the effect on this receptor would be negligible (not significant). The location represents a northward travelling road user on Surrey Quays Road approaching the junction of Canada Street and Surrey Quays Road. The analysis shows that there is no solar glare arising from the RMAs for Buildings A1 and A2 and so the potential for solar glare at this location is Negligible and not significant. This aligns with the effect identified in the July 2021 ES.

123. Viewpoint 4

The July 2021 ES concluded that the effect on this receptor would be minor adverse (not significant). The location represents a south-westward travelling road user on Canada Street approaching the junction of Canada Street and Surrey Quays Road. The analysis shows that there is no solar glare arising from the RMAs for Buildings A1 and A2 and so the potential for solar glare at this location is Negligible and Not Significant. This effect is reduced from that identified in the July 2021 ES.

124. The expected solar glare effects are not significant and as such no mitigation is considered to be necessary.

### Light Spill

125. As part of the ES light spill analysis an assessment of exterior and internal lighting was undertaken. The exterior lighting arrangement is based on the December 2022 Preliminary Lighting Specification for Exterior Lighting by Studio Fractal. The lighting arrangements within Building A1 and A2 have not undergone detailed design at this stage and so an indicative lighting arrangement was modelled to achieve 300 lux at desk height through the office space as per CIBSE recommendations with luminaires position at least 3m from any glazing.

126. The assessment considers the impact on the Dock. The July 2021 ES concluded that the effect on this receptor would be minor adverse (not significant). The updated analysis shows additional light spill of 250-300 lux reaches the current eastern Dock Edge. However, this area is part of the thoroughfares around the Proposed Development and so is expected to be well lit after dark given the expected levels of footfall. Furthermore, it must be noted that the modelling cannot factor the dappling effect of the dockside trees. It is likely, therefore, that the actual lux levels reaching the dock basin will be below the levels indicated in the analysis. The results show that no significant light spill to this receptor originates from the office spaces on the upper floors of the Proposed Development but from the exterior lighting only. Furthermore, internal lighting control measures such as Passive Infrared Sensor (PIR) sensors to turn down lighting on unoccupied floors will be put in place to reduce the potential light spill arising from the Proposed Development to an acceptable level. Therefore, when applying professional judgement, the likely light spill effect arising from the Proposed Development would be Minor Adverse and not significant. This aligns with the effect identified in the July 2021 ES.

The Council's Ecologist was satisfied at Outline Stage that the development would

not have an unacceptable impact on the Dock in this regard.

127. It should be noted that additional impact of lighting onto the Dock will arise as a result of the proposed enhancements to the dock which form part of a separate planning application. That application also includes a lighting assessment to demonstrate any impacts arising from that specific proposal.
128. In respect of Porters Edge the July 2021 ES concluded that the effect on this receptor would be minor adverse (not significant). The results of the light spill analysis to Porters Edge show additional light spill of approximately 40 lux reaches the lower floors of Porters Edge. However, the results show that this arises from the exterior lighting and not the office space and so this level potential spill is limited to the lowest, non-residential floors. Therefore, when applying professional judgement, the likely light spill effect arising from the Proposed Development to residential properties will be Negligible and not significant. This effect is reduced from that identified in the July 2021 ES.
129. In respect of Water Gardens the July 2021 ES concluded that the effect on this receptor would be minor adverse (not significant). The results of the light spill analysis to Water Gardens show no additional light spill reaches the lowest parts of the facades of the Water Gardens. Therefore, the likely light spill effect arising from the Proposed Development will be Negligible and Not Significant. This effect is reduced from that identified in the July 2021 ES.
130. In respect of the British Land Masterplan The July 2021 ES concluded that the effect on this receptor would be Minor Adverse (Not Significant). The results of the light spill analysis to Zone D of the British Land Canada Water Masterplan are considered further in the SoC. Zone H of the British Land Canada Water Masterplan is adjacent to Building B and so will not be materially impacted in light spill terms by Building A1 and A2. The results show additional light spill of approximately 3 lux reaches the lower floors of Zone D. Therefore, the likely light spill effect arising from the Proposed Development will be at worst Minor Adverse and Not Significant. This aligns with the effect identified in the July 2021 ES.
131. The potential for light pollution is not considered to be significant. The OPP is already subject to a condition to control all external lighting. No further mitigation is necessary.

### Wind Microclimate

132. The July 2021 ES assessed the wind microclimate effects on the site and surrounding area for both the maximum outline parameters, and a secondary supplementary assessment of the Illustrative Scheme.
133. The July 2021 Wind Microclimate Chapter committed to more detailed assessments with the RMAs when more detailed information would be available on the layout, massing, façade details and landscaping to confirm the effects and required mitigation measures.
134. The SoC provides a quantitative wind microclimate assessment based on wind tunnel testing of the detailed design proposals for Building A1 and A2, and Maritime Street, and is supported by a Pedestrian Level Wind Microclimate Report.



135. The assessment classifies expected pedestrian uses at ground level as shown in the image below. The assessment also considers wind conditions on the amenity terraces.



136. The following configurations were tested for the RMAs:
- Configuration 1: Proposed Development with Existing Surrounding Buildings and Proposed Landscaping;
  - Configuration 2: Proposed Development with Existing Surrounding Buildings, Proposed Landscaping and Mitigation Measures; and
  - Configuration 3: Proposed Development with Cumulative Surrounding Buildings, Proposed Landscaping and Mitigation Measures.
137. Configuration 2 has been assessed with the Proposed Dock Edge Scheme and Dock Edge landscaping as proposed in application reference 23/AP/0798.
138. As the detail design of Plot B will come forward through a later RMA, this was considered within the context of the cumulative surrounding buildings (Configuration 3) at its maximum parameter massing, as this represents the worst-case scenario.

Configuration 1: Proposed Development with Existing Surrounding Buildings and Proposed Landscaping

139. The assessment shows the following:
- On-site and off-site thoroughfares would range from suitable for sitting to walking use during the windiest season. There are isolated locations that would require mitigation.
  - On-site entrances would be suitable for sitting and standing use during the windiest season
  - Off-site entrances would be suitable for standing and strolling use during the windiest season.

- The on-site bus stop would be suitable for standing use during the windiest season
- On-site ground level mixed-use amenity spaces would range from suitable for sitting to strolling use during the summer season with one isolated location requiring mitigation
- On-site ground level seating spaces would be suitable for sitting and standing use during the summer season – it should be noted that conditions will improve with the proposed mitigation in place to make the seating areas more comfortable
- Terraces would range from suitable for sitting to strolling use during the summer season with mitigation being required
- Annual strong wind exceedances would occur on ground level, at both on-site and off-site thoroughfare locations. Strong winds at five locations would pose a safety concern to cyclists and more vulnerable pedestrians and would be considered significant. Wind mitigation measures have therefore been developed to improve wind conditions, as set out in Configuration 2.

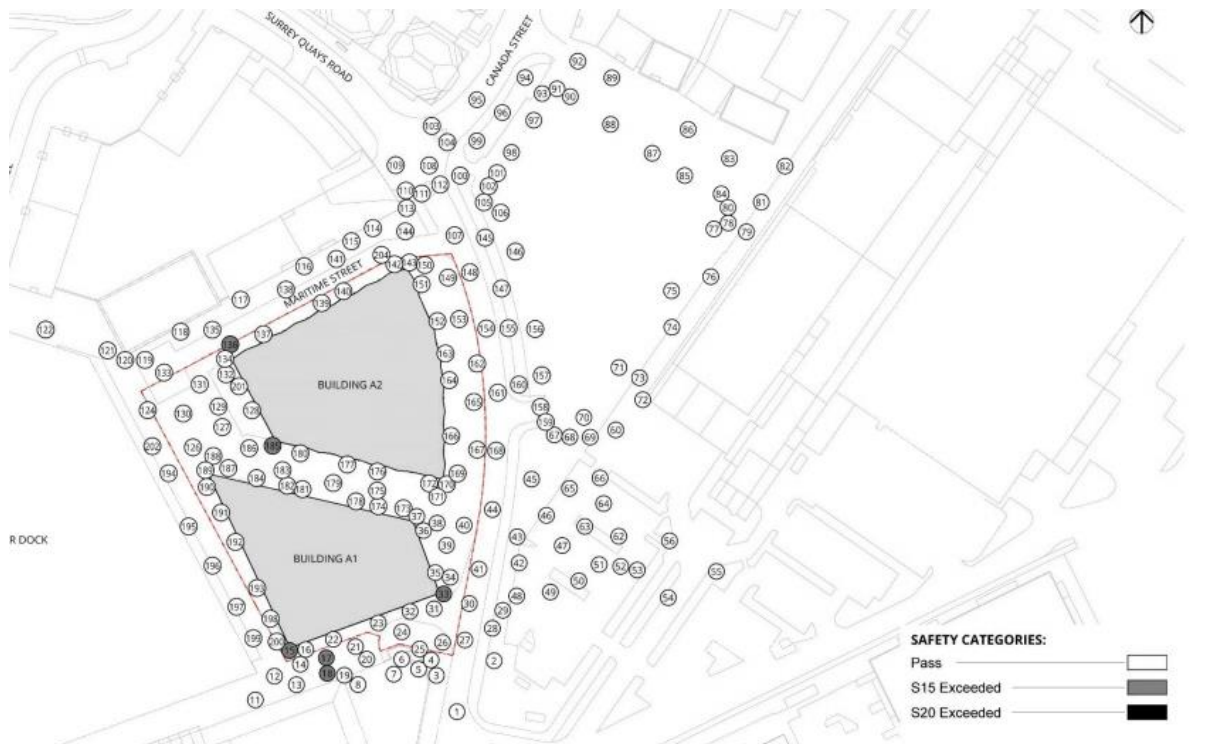
140. There are no uncomfortable conditions in Configuration 1 in either the summer or windiest season.



**Image above:** Configuration 1: Proposed Development with existing surrounding buildings and proposed landscaping, ground level, windiest season.



**Image above:** Configuration 1: Proposed Development with existing surrounding buildings and proposed landscaping, ground level, summer season.



141. **Image above:** Configuration 1: Proposed Development with existing surrounding buildings and proposed landscaping, ground level, annual safety.

142. Wind mitigation has been developed and assessed with the intention to improve wind conditions around the Proposed Development such that they would be suitable for

the intended pedestrian uses. Wind mitigation measures were developed in the context of the existing surrounding context, which would represent a worse-case scenario than the cumulative surrounding context. The outcomes of this assessment are presented in Configuration 2. In line with the Canada Water Dockside Masterplan S106 agreement, the Dock Edge Scheme would come forward alongside the Proposed Development, and as such the Proposed Dock Edge was included within Configuration 2. The Proposed Development with the implementation of wind mitigation measures was also assessed in the context of the cumulative surrounding buildings to confirm the suitability of wind conditions, as presented in Configuration 3.

143. Wind conditions at the Proposed Development have been optimised through development of the proposed landscaping and wind mitigation strategy using extensive iterative wind tunnel testing balanced against the practical constraints of a useable public realm. With this strategy implemented, wind conditions across the majority of the Proposed Development site would be suitable for the intended pedestrian uses (discussed further in Configuration 2, below). The following mitigation measures have

been identified, and incorporated into the landscaping and architectural plans:

- Porous screen at the western corner of A2 (4.3m tall, 2m wide and 50% open);
- Northern and western sides of spill-out seating west of A2 screened by 1.5m high 'hit and miss' wind shelter butting up to north-western corner of A2 (solid screens or dense planting, area of 'hit' equal to or greater than 'miss');
- Four equally spaced porous elements on the southern façade of A1 (floor to soffit, 0.75m wide, 50% open);
- Six equally spaced porous elements on the western façade of A1 (floor to soffit, 0.75m wide, 50% open);
- Porous element at the south-east corner of A1 (1.2m tall, 50% open, "L" shaped);
- Three multi-stem trees to the south of A1 (3m tall);
- Planting underneath trees along the site boundary west of the northern corner of A1 (1.5m tall); and
- Solid balustrades (1.5m tall) on the terraces of A1 and porous seat backs should spill-out space be specified in tenants fit out (1.5m tall, 50% porous).

This mitigation would be required in the context of both the existing and the cumulative surrounding context scenarios.

144. Wind tunnel tests have also identified the need for temporary mitigation in a scenario where development is not in place on the adjacent Zone D of the British Land Canada Water Masterplan. Additional enhancement to the landscaping scheme has been developed in the wind tunnel, which comprises the introduction of additional planting in large pots. The planting location and sizes have been shown on 'CWD-ASD-DE-XX-DR-L-0701-XX-P01\_LANDSCAPE PLAN FOR TEMPORARY WIND MITIGATION' submitted as part of the RMA for Building A1. It is necessary to secure detailed planting proposals for these elements and a full technical wind assessment to demonstrate that the measures are adequate by way of the recommended condition. The condition should also require the applicant to confirm that all necessary consents have been obtained to install the planters which are proposed to be located on land which forms a public highway between Building A1 and the dock edge. At this stage the Councils Highways Officer has stated that any landscaping required on the public highway would be subject to s278 approval and this is already covered in

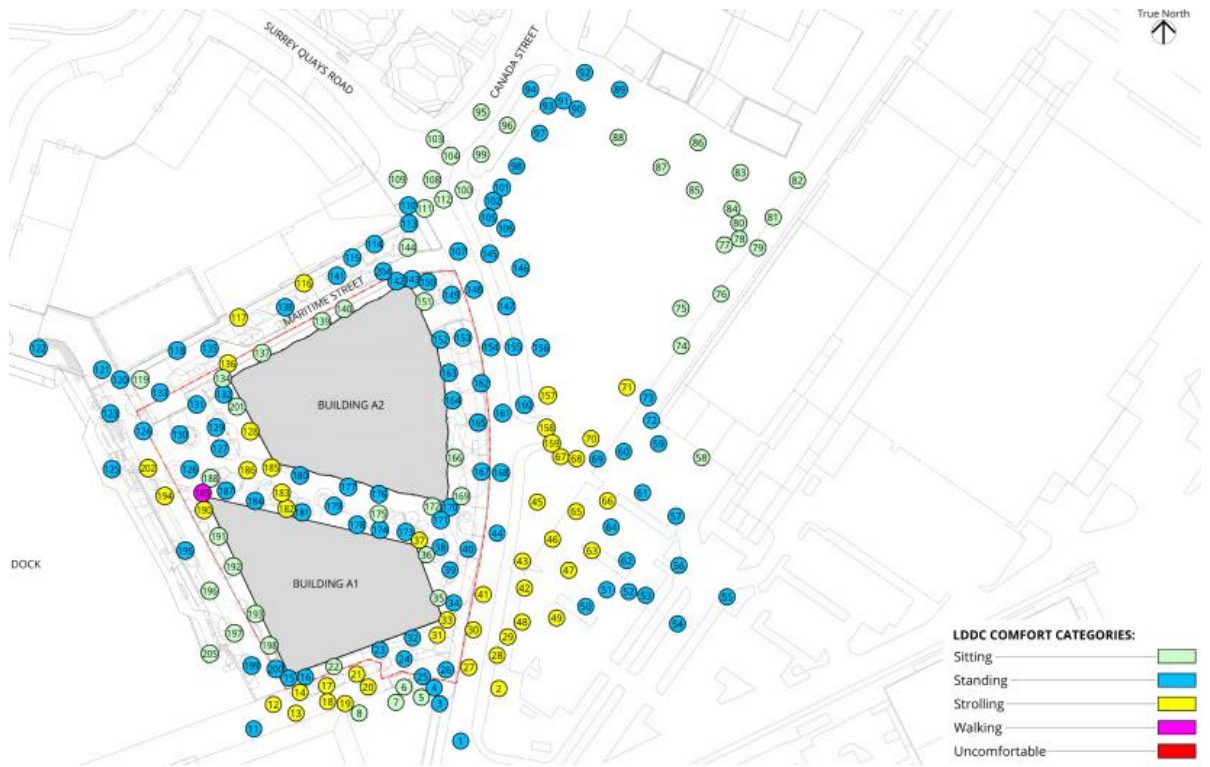
the s106 Agreement attached to the Outline Permission.

Configuration 2: Proposed Development with Existing Surrounding Buildings, Proposed Landscaping and Mitigation Measures

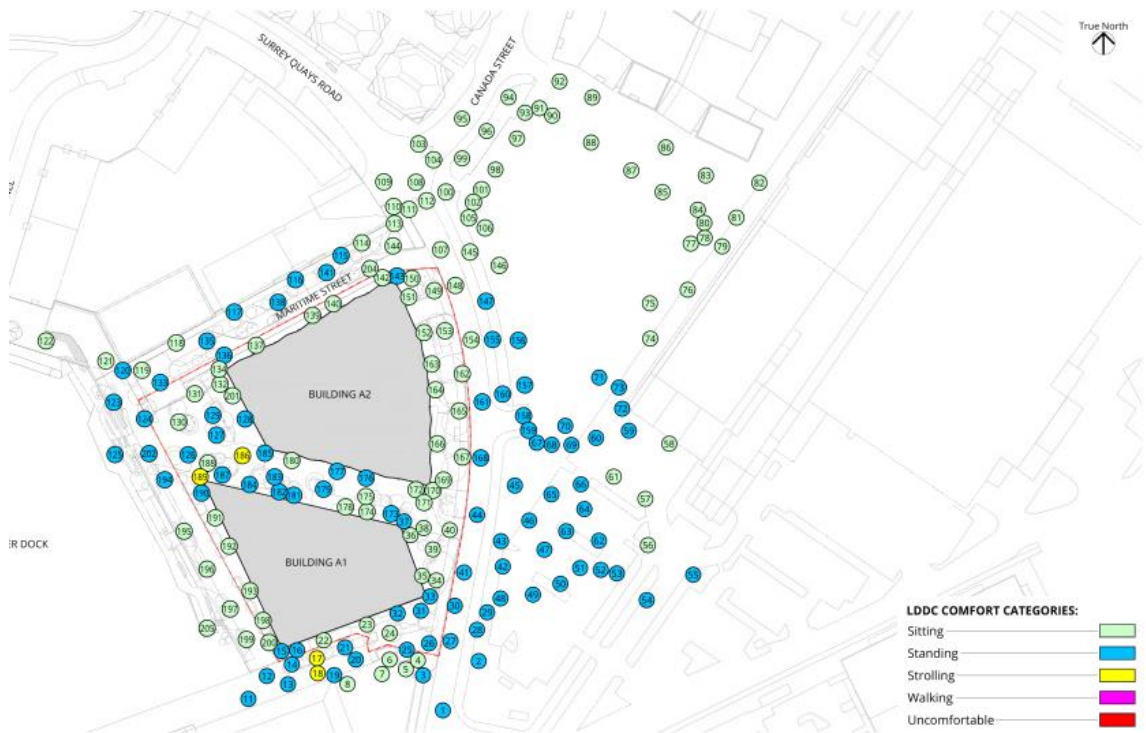
145. The mitigation measures listed above were incorporated in Configuration 2.
146. With the inclusion of mitigation measures the assessment shows the following:
- Wind conditions at on-site thoroughfares would range from suitable for sitting to strolling use during the windiest season. Notably, adverse effects in Configuration 1 would improve. In one isolated location walking conditions would remain. Whilst this would be one category windier than suitable, conditions would remain suitable for strolling use 94% of the time during the windiest season. This is considered to be acceptable.
  - Wind conditions at off-site thoroughfares, including those locations that would be developed into amenity spaces in the cumulative context (the area between A1 and Zone D of the BL Masterplan), would range from suitable for sitting to strolling use during the windiest season. Notably adverse effects in Configuration 1 at thoroughfare location 18 would improve.
  - On-site entrances would be suitable for sitting and standing use during the windiest season
  - Off-site entrances would be suitable for standing and strolling use during the windiest season. There would be strolling conditions at entrance location 116, one category windier than suitable. However, the entrance is recessed which provides a gradual transition for users from the calm interior to windy exterior. Wind conditions around the entrance would therefore be suitable.
  - The on-site bus stop would be suitable for standing use during the windiest season.
  - On-site ground level mixed-use amenity spaces would be suitable for sitting and standing use during the summer season with conditions being slightly windier during winter where the spaces become suitable for standing and strolling. Notably, adverse effects in Configuration 1 would improve
  - Off-site ground level mixed-use amenity would be suitable for sitting and standing use during the summer season.
  - On-site ground level seating spaces would be suitable for sitting use during the summer season.
  - Terraces would be suitable for sitting and standing use during the summer season. Notably, adverse effects in Configuration 1 would improve.
  - No strong winds would occur on or off-site in accessible spaces in the context of Configuration 2. Notably, significant effects in Configuration 1 would improve
147. There are no uncomfortable conditions in Configuration 2 in either the summer or windiest season.
148. In this configuration the public realm areas in Waterfront Square and Eastern Dock Edge would be suitable for sitting and standing use during the summer season. These areas will be used for a variety of purposes including seating. As such the applicant was asked to undertake design improvements to the seating areas within the Dock Edge red line boundary to improve comfort conditions. As a result of this the amphitheatre seating was redesigned to incorporate more extensive soft



landscaping. As discussed in the Dock Edge application the seating areas will now achieve appropriate comfort conditions. A qualitative assessment has been provided to show that the seating areas will befit for purpose. Full modelling will be secured by way of a pre commencement condition attached to the dock edge application.

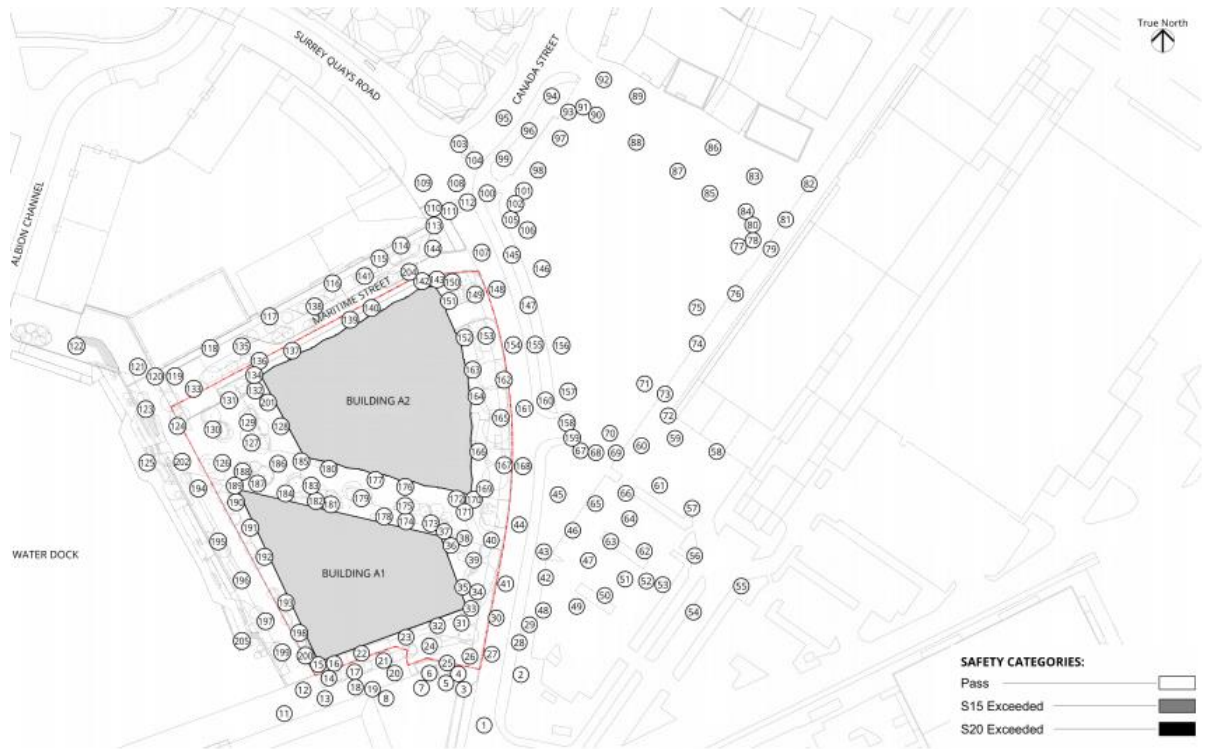


**Image above:** Configuration 2: Proposed Development with existing surrounding buildings, proposed landscaping and mitigation measures, ground level, windiest season.



**Image above:** Configuration 2: Proposed Development with existing surrounding

buildings, proposed landscaping and mitigation measures, ground level, summer season (note this assessment has been updated by way of the amendments to the dock edge works).



**Image above:** Configuration 2: Proposed Development with existing surrounding buildings, proposed landscaping and mitigation measures, annual safety, summer season.

149. Wind conditions in the context of the cumulative schemes detailed in the July 2021 ES, included the British Land Canada Water Masterplan. RMAs have recently been approved for the following zones of the Canada Water Masterplan and have been included within the cumulative impact assessment (Configuration 3) discussed below:

- Zone F – Ref. 21/AP/4712;
- Zone G – Ref. 22/AP/2439;
- Zone H – Ref. 21/AP/3338; and
- Zone L – Ref. 20/AP/2495.

Two further RMAs have been approved that pertain to the landscaping surrounding the masterplan site. In line with best practice, landscaping has been limited to the Proposed Development site and the immediate surroundings to present a worst-case impact on the surrounding pedestrian realm. As such, RMAs approved for public realm within the adjacent BL masterplan site have not been included within the cumulative impact assessment but in reality once the landscaping is implemented on adjacent sites wind conditions would improve.

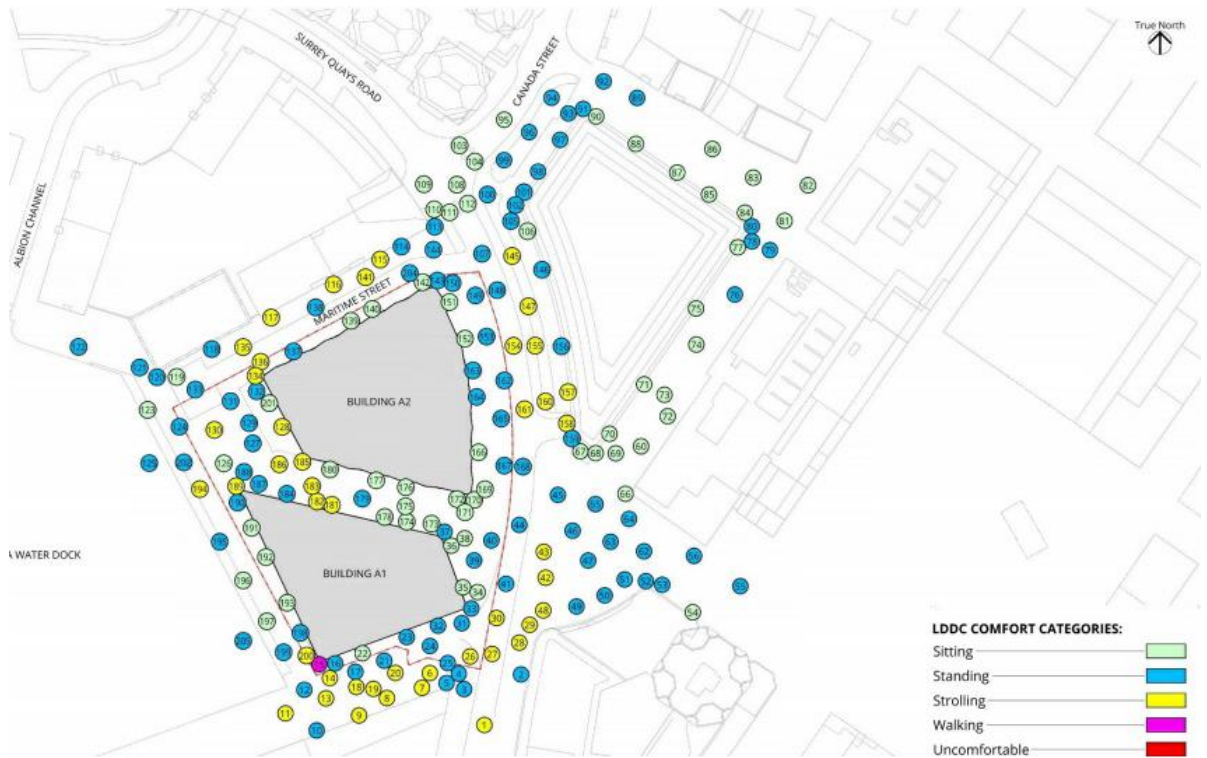
### Configuration 3: Proposed Development with Cumulative Surrounding Buildings, Proposed Landscaping and Mitigation Measures

150. This scenario includes the proposed landscaping scheme and the permanent wind mitigation discussed for Configuration 2. This configuration is the most likely long



term scenario to arise as the adjacent British Land Masterplan has commenced and there is a clear commitment to redeveloping this part of the Masterplan (Zone D). However, it will be a number of years before Zone D is complete and therefore the development subject of this report could be complete and ready for occupation for a number of years before the cumulative surrounding buildings are erected. It is for this reason a temporary mitigation scheme is required as discussed in the earlier sections of this report.

151. In the cumulative scenarios and with the confirmed permanent mitigation in place the assessment shows:
- On-site thoroughfares would range from suitable for sitting to strolling use during the windiest season.
  - Off-site thoroughfares would range from suitable for sitting to strolling use during the windiest season.
  - On-site entrances would be suitable for sitting and standing use during the windiest season
  - Off-site entrances would range from suitable for sitting to strolling use during the windiest season. Off-site entrance locations 115 and 116 would have strolling conditions, one category windier than suitable. However, the entrances would be recessed which provides a gradual transition for users from the calm interior to the windier exterior. Wind conditions around these entrances would therefore be suitable.
  - The on-site bus stop at location 162 would be suitable for standing use during the windiest season.
  - On-site ground level mixed-use amenity spaces would be suitable for sitting and standing use during the summer season.
  - Off-site ground level mixed-use amenity would range from suitable for sitting to strolling use during the summer season. Locations 147 and 160 would have strolling conditions, one category windier than suitable; however, it is expected that local shelter will be developed as necessary as Plot B comes forward through future RMAs, which would improve wind conditions.
  - On-site ground level seating spaces would be suitable for sitting use during the summer season
  - Off-site ground level amenity seating would be suitable for sitting and standing user during the summer season. Locations 91 and 157-159 would be suitable for standing use, however, it is expected that local shelter will be developed as necessary as this comes forward through future RMA for Plot B which would improve wind conditions
  - Terraces would range from suitable for sitting to standing use during the summer season. Additional mitigation in the form of 1.5m tall 50% porous screens have been assessed in the wind tunnel should outdoor dining specified by the end tenant on the A1 terrace levels. With these screens in place around outdoor dining, wind conditions at seating on the A1 terrace would be suitable for sitting use during the summer season
  - No strong wind exceedances would occur on or off-site. Notably, adverse effects in Configuration 1 south of A1 in the absence of the developed temporary mitigation (measurement location 18) and west of A2 (measurement location 132) would improve in the cumulative scenario
152. There are no uncomfortable conditions in configuration 3 during winter or summer.



**Image above:** Configuration 3: Proposed Development with cumulative surrounding buildings, proposed landscaping and mitigation measures, ground level, windiest season



**Image above:** Configuration 3: Proposed Development with cumulative surrounding buildings, proposed landscaping and mitigation measures, ground level, summer season



**Image above:** Configuration 3: Proposed Development with cumulative surrounding buildings, proposed landscaping and mitigation measures, ground level, annual safety.

153. The above wind analysis demonstrates that mitigation is necessary to ensure that wind conditions are appropriate for the intended use of public realm areas surrounding the site as well as on the terraces proposed for A1 and A2. With the above stated mitigation in place (which forms part of the detailed design for the RMAs) appropriate and comfortable microclimate conditions can be achieved. These conditions will improve as the surrounding cumulative schemes are built out. At this stage, given how the surrounding development is progressing there is nothing to indicate that the cumulative scenario will not be achieved. Nevertheless, a temporary wind mitigation strategy has been proposed in the event that the adjacent developments do not come to fruition. The temporary strategy is considered to be acceptable in principle but it is necessary for the applicant to provide full details in terms of design as well as a technical wind assessment and as such a pre commencement condition has been recommended for Building A1 RMA.
154. With the implementation of mitigation, there would be no strong wind exceedances that would pose safety concerns on or off-site. There would be no strong wind exceedances that would pose safety concerns on or off-site in the cumulative scenario.
155. The plans submitted with the RMAs identify in detail all necessary permanent wind mitigation measures required for the terraces and ground floor locations as well as a proposal for the temporary mitigation. Condition 1 of each RMA will specify the requirement for the development to be built in accordance with the relevant plans. Furthermore Condition 9 of the OPP requires all approved wind mitigation features to be installed prior to first occupation of the relevant building and maintained thereafter in perpetuity. As discussed in this report an additional condition is recommended for the temporary wind mitigation. The temporary wind mitigation is proposed within the red site boundary for the Dock Edge Application rather than the RMA boundary for

Plot A1. However, the Dock Edge Works are intrinsically linked to the CWD Masterplan by virtue of the obligations set out in Schedule 6 of the s106 Agreement.

### Air Quality

156. The July 2021 ES Air Quality Chapter scoped out the assessment of the potential impacts of emergency generators. This was because no significant effects were anticipated – whilst the specific location of any emergency generators were unknown, they were proposed to be located well away from any air quality sensitive uses, to ensure a very good dispersion environment for discharge of emissions. The generators would also be tested infrequently for short periods and would not regularly operate.
157. The July 2021 ES Air Quality Chapter committed to providing the technical details of the generators, including their locations, as part of any RMA(s); to confirm that there are no significant effects. The technical details of the generators, including their locations, are now included as part of the RMAs.
158. The Environment Act 2021 has been introduced since submission of the July 2021 ES and updates to the NPPF have been introduced in respect of air quality. The updated and new legislation/guidance does not affect the air quality assessment undertaken within the July 2021 ES Chapter; the effects therefore remain valid.
159. The Proposed Development will be provided with emergency power using two diesel generators located on the roofs of Buildings A1 and A2. Technical information for the generators has been provided as part of the ES SoS and assessed by the Council's Environmental Protection Team (EPT). The Air Quality Assessment finds that proposed emergency generators will not lead to exceedance of annual average NO<sub>2</sub> objectives. In respect of 1 hour NO<sub>2</sub> objectives, the generators may lead to exceedance of the 200µg limit value however the generators are limited to full power testing for a maximum of 5 hours per year which makes exceedance of the air quality objective (200µg not to be exceeded more than 18 times) very unlikely. The affected receptors are not in locations where there is likely to be any significant cumulative impact with other sources, such as from a busy road. Overall the assessment finds the impact is negligible and EPT agree that the impact is not significant. As such there is no objection to the proposed location of emergency generators.

### Daylight, Sunlight and Overshadowing

160. At the time of granting OPP the impact on daylight, sunlight and overshadowing arising from the development on nearby receptors was assessed using the maximum building envelopes created by the Parameter Plans. On this basis the impact deemed to be acceptable was the 'worst case scenario' and any refinement of the development proposals within the maximum envelopes would have the same or a lesser impact.
161. The changes to the Maximum Parameter envelope approved under the NMA in respect of the wind mitigation zone for Building A1 are very minor. As such, this will not affect the assessment of effects of Daylight, Sunlight and Overshadowing on neighbouring properties compared with the Canada Water Dockside Masterplan ES.
162. In respect of the adjacent BL schemes the OPP assessed the impact based on the

maximum parameters for the BL Plots and potential for residential uses within each of the plots. As such the worst case scenario was assessed and possible effects deemed to be acceptable. The recently approved RMAs for Plots H, F and G of the BL Masterplan sit within the Outline maximum parameters and as such no additional effects are anticipated.

163. The July 2021 ES reported on the likely effects with reference to the 2011 BRE Guideline. Since submission of the July 2021 ES, the BRE have published an update to BR209 in June 2022 (“the BRE Guidelines 2022”). The advice provided within the BRE Guidelines 2022 has not changed in relation to the assessment methodology from the now withdrawn 2011 edition which was referenced within the July 2021 ES with one exception relating to the overshadowing of solar panels. As the overshadowing of solar panels was not considered in the July 2021 ES the impact has therefore been considered as part of the SoC.
164. The BRE guidance for the overshadowing of solar panels has changed in the 2022 update. It advises that the Annual Probable Sun Hours (APSH) is measured to neighbouring solar panels and that if a Proposed Development causes a reduction in APSH of less than 10%, then no further consideration is required.
165. Following the update to the BRE guidance regarding overshadowing of solar panels, the surrounding vicinity was reviewed for the presence of solar panels. This showed that the tallest element of Porters Edge was the only location to the north of the Proposed Development to include solar panels. The baseline level of APSH to these panels was calculated to be 97%. The assessment undertaken shows that the photovoltaic array will not experience a change in their expected APSH with the Proposed Development in place. Any effect is therefore considered Negligible and Not Significant.

### Townscape, Visual and Built Heritage

166. The National Planning Policy Framework (NPPF) was updated in July 2021. The 2021 NPPF includes some updates to the wording and paragraph reference numbers of the 2019 NPPF which is cited in the July 2021 Townscape Heritage Visual Impact Assessment (THVIA). The 2021 updates to the relevant parts of the NPPF are very minor and do not alter the principles and approaches to conservation and enhancement of the historic environment set out in chapter 16 of the NPPF or the key aspects of well-designed places set out in chapter 12 of the NPPF. The 2021 NPPF does not alter the assessment and conclusions presented in the July 2021 THVIA.
167. Historic England (HE) published an updated Advice Note 4: Tall Buildings in 2022 which supersedes the 2015 Advice Note 4 on Tall Buildings which was referenced in the July 2021 THVIA. The updated HE guidance does not introduce any new criteria or approaches which would alter the assessment and conclusions presented in the July 2021 THVIA.
168. The proposed massing for Buildings A1 and A2 conforms with the consented parameters which were assessed in the July 2021 THVIA and align with the Design Code. This assessment considers any additional or different effects, to those found in the July 2021 THVIA, which might arise from the detailed profile, architectural expression and materiality proposed for Buildings A1 and A2.

169. The Maritime Street proposals accord with the consented parameters spatially and with the Design Codes for the layout, appearance and landscaping for the future development, as set out in Landscape Design and Access Statement.
170. For Building A1, the consented outline parameters were drawn so that there would be very little flexibility in terms of height, mass and profile of the building. Its tiered profile and the recesses between the tiers were mandatory in the consented Design Codes and are evident in the detailed design proposals in the RMA. The Design Codes stipulate a 30:70 wall to window ratio and outer grid of natural materials (masonry, stone, timber), locally sourced where possible. Colours are required to complement those selected for the tall buildings on the neighbouring British Land Canada Water Masterplan and are required to be recessive in key views from London Bridge which look through Tower Bridge Materials and colours should also be chosen to both complement and contrast with other tall buildings locally, so as to form a coherent group identity whilst avoiding coalescence in views.
171. The proposed height and mass of Building A1 is entirely within the consented outline parameters. Consequently, Building A1 has a legible shoulder height at c.30m, with the mass above expressed in a series of volumes which are visually separated by terraces and arranged to angle up towards the top. The ground level will include reception, lobby and retail space, so as to activate the adjacent routes and spaces, with a mezzanine level above with cycle facilities. Levels 1-23 will accommodate office space. Level 24 will include roof plant with a Building Maintenance Unit (BMU) and Photo Voltaic (PV) panels and green roof above.
172. The detailed design for A1 conforms with the window ratio and selection of natural materials stipulated in the Design Codes. The brick grid of each tier lightens in colour towards the top, ranging from dark brown brick at the base to light beige brick at the top. This will give the building a distinct identity and complement materials and tones seen in the local context. It will also help the upper elements to appear recessive on the skyline and in key views, as stipulated in the Design Codes. The infill panels are matt finished metal elements which are consistent in tone across the tiers. The depth of the outer grid and width of infill panels varies depending on the orientation of the façade. Metal fins are arranged vertically and horizontally to add shading and interest and will match the colour of the infill panels and window frames. The tapered arrangement of the building form allows for large triangular terraces on the western elevation of each of the intermediate levels. The intermediate levels are recessed and fully glazed, enhancing the legibility of each expressed tier and maximising views out to the planting on the long terraces which extend across each of these levels.
173. The approved non-material amendment to the maximum building extent to A1 was considered to be very slight relative to the scale of Development and the proposed detailed design would not noticeably increase the perceptible scale and bulk of A1 comparison to the maximum parameters of the approved Masterplan assessed within the CWD July 2021 ES.
174. The maximum parameter envelope would not noticeably change the relationship of the proposed detailed design of Building A1 to the streetscape
175. For Building A2, the consented parameters define a much lower maximum height than Building A1, set at 55.4m AOD. The consented envelope includes a shoulder



height at the Dock edge, shared with Building A1 and nearby existing and future development, and the upper levels set back from there. The upper volume is defined by sloping planes which recede on all sides towards the top. There is greater flexibility with the positioning and massing of Building A2 than Building A1, with a 5 metre deviation permitted for the building footprint within the maximum parameters (9m required on the western façade). A 10 metre deviation in height (within the maximum parameters) is also permitted. The Design Codes set out the opportunity for terraces and significant planting within the set-back upper levels.

176. The proposed Building A2 accommodates retail uses at ground level, 'end-of-trip' facilities for cyclists, runners and pedestrians at mezzanine level, offices at Levels 1-9 and a double height tenant amenity space on Level 10. The mass of the proposed Building A2 fits entirely within the consented outline parameters. It has a double height ground level, set slightly back from the office levels above. The five levels above are largely regular in plan, resulting in a strong shoulder datum at Level 5, above which the storeys reduce in size towards the top. The regular grid of the façade is manipulated and sculpted at upper levels, accommodating landscaped terracing and lending a dynamic and organic visual quality to the building. The outer grid is expressed in light coloured horizontal and vertical precast elements, with a horizontal emphasis to the openings and glazing set between. The glazing will have a vertical articulation, alternating with warm bronze coloured metal elements. At upper levels, and where the façade is sculpted, glass balustrades will enclose different sized terraces with opportunities for different types of planting and furnishings. The largest terraces will accommodate trees and integrated benches.
177. The July 2021 THVIA provided an assessment of 24 viewpoints. It included representations of the consented parameters of Buildings A1, A2 and B in the 24 views and detailed representations of an Illustrative Scheme for all three plots in selected views. The Illustrative Scheme showed one way in which it would be possible to design a detailed scheme which was compliant with the outline parameters and Design Codes.
178. The ES SoC is informed by an assessment of 11 views. These 11 views were selected from the 24 views assessed in the July 2021 THVIA. They were chosen as views in which Buildings A1 and/or A2 are visible and which together provide a 360 degree perspective of the proposals and include key views of townscape and heritage sensitivity. The proposed detailed designs for Buildings A1 and A2 are represented in these 11 views in a detailed and accurate rendered format.
179. The proposed height and massing of Buildings A1 and A2 in the RMAs would accord with the consented parameters and Design Code, which were found to cause no harm to heritage and OPP stage. The proposed form, architecture and materials of Buildings A1 and A2 have been conceived in relation to their visual and experiential context and would be of the required high quality and appropriate design character. The Maritime Street proposals accord with the consented parameters spatially and with the Design Codes, The proposed RMAs would therefore not affect the heritage significance of any heritage assets in the vicinity and therefore the effects would remain as defined in the July 2021 THVIA.
180. Noting the above, the detailed RMA proposals would be in conformity with the townscape visual or built heritage effects assessed in the approved ES. Accordingly there would be no change to the townscape, visual and built heritage effects or



mitigation previously identified within the approved ES.

### Cumulative Effects

181. Additional relevant cumulative schemes have been identified since the approval of the OPP. Specifically the Dock Edge Application submitted by the Applicant and the adjacent British Land Canada Water Masterplan RMAs that have subsequently been submitted and approved by the LBS following submission on the 2021 ES for CWD. The BL schemes include:

- Zone F – Ref. 21/AP/4712;
- Zone G – Ref. 22/AP/2439 (yet to be determined);
- Zone H – Ref. 21/AP/3338;
- Zone L – Ref. 20/AP/2495;
- Carriageway along New Brunswick Street + public realm and landscaping to New Brunswick Street and Printworks Place – Ref. 21/AP/4616;
- Public realm and landscaping works (Park Walk) – Ref. 22/AP/2580.

The British Land Canada Water Masterplan RMAs are not considered to alter the conclusions of the July 2021 ES, and the cumulative assessments presented remain valid. However, where this detailed RMA information is relevant to a specific assessment – for example the updated massing-based assessments presented in this SoC the most relevant RMA plans for the British Land Canada Water Masterplan site have been assessed.

182. Three plots of the British Land Canada Water Masterplan (Zone F; Zone H and Zone L) have had RMAs approved since July 2021 and are therefore represented in detailed wirelines Zone G is also included because of its advanced stage in the planning process and with the addition of detailed proposals for the Dock Edge which are being submitted alongside, but separately from, the CWD RMAs. Due to the landscaped nature of the Dock Edge proposals, which would not be easily understood through wireline representation, the submitted proposals for the Dock Edge are rendered into the cumulative views.

183. The detailed RMA proposals for Zones F, G and H of the British Land Canada Water Masterplan conform with the consented parameters and Design Codes for that Masterplan, and consequently do not significantly differ from the outline parameters for those plots represented in the views in the July 2021 THVIA. The detailed designs for Zones F, G and H therefore do not alter the cumulative effects reported in the July 2021 THVIA.

184. The Dock Edge proposals are adjacent to the west side of Buildings A1 and A2 and would not be seen, or have any effect on, the settings of heritage assets, all of which are in the wider area. The Dock Edge proposals would impact on townscape character only in terms of the character and quality of the public realm, and would only affect a small part of the study area, which includes the site. There would be no effects on townscape character resulting from the Dock Edge proposals in addition to those reported in the July 2021 THVIA. In terms of visual impact, the Dock Edge proposals would only be seen in views across the Dock and close to the Site: major and neutral effects have been found to these views in the July 2021 THVIA and these landscape proposals would not result in any additional effect, which mainly arises from the built form. The cumulative effects would therefore remain as defined in the July 2021 THVIA

## ES Statement of Conformity Summary

185. The Proposed Development coming forward under the RMAs would not cause any new significant effects when compared to the assessments and conclusions presented within the July 2021 ES. There would also be no change to the likely significant effects presented within the July 2021 ES. Therefore, the findings of the assessments presented within the July 2021 ES remain valid in light of the proposals coming forward under the RMAs and the review process has been proportionate to the approvals being sought.

## **Design**

### **Site context**

186. London Plan Policy D3 requires developments to make the most efficient use of land to optimise density, using an assessment of site context and a design-led approach to determine site capacity. This design led approach is reflected in Southwark Plan Policy P18.
187. In urban design terms the OPP was defined by three main principles: the response to the Canada Water Dock Edge; the creation of a diagonal route through the site; and the arrangement of tall buildings and their massing.
188. The parameter plans approved under the OPP optimise the commercial capacity of the site in terms of layout and massing, with the aim of attracting major businesses to Canada Water. With the principles established what remains to be demonstrated in design terms as part of the RMA is compliance with the parameter plans and delivery of exemplary design quality and successful public realm strategy.

### **Site layout, access and public realm**

189. The approved parameter plans define the location of buildings, routes through the site, areas of public realm and servicing locations. The plans submitted with each RMA accord with the parameter plans in this respect.
190. Layout Buildings A1
- Building A1 is shaped by the major routes, connecting existing streets and squares to emerging places and destinations. The extent and shape of the footprint for the building accords with the agreed Outline parameters.
191. The building comprises retail and food & beverage with office entrances on the ground floor and offices on the upper levels. Cycle parking and end-of-trip facilities are located in the basement and ground floor mezzanine. Ancillary and plant spaces are located in the basement, and at ground & mezzanine and roof plant floor. The roof will accommodate Building Maintenance Unit, PVs panels and green roof. The bottom three floors: basement, ground floor and mezzanine, work together as one part of the building due to their connected functions. These floors and the roof are the only areas dedicated to functions other than workplace.



**Image above:** Proposed ground floor layout for Building A1 showing access points and layout of spaces

192. The layout of spaces proposed for the ground floor, location of retail areas and entrance and exit points successfully optimises the accessibility and activation of the site, responding well to the existing and emerging context.
193. For the upper levels of Building A1 there are five different floor plate shapes that follow the geometry of the building, offering a variety of workspace arrangements over 23 floors. Each floor has a clear head height of 3.2m



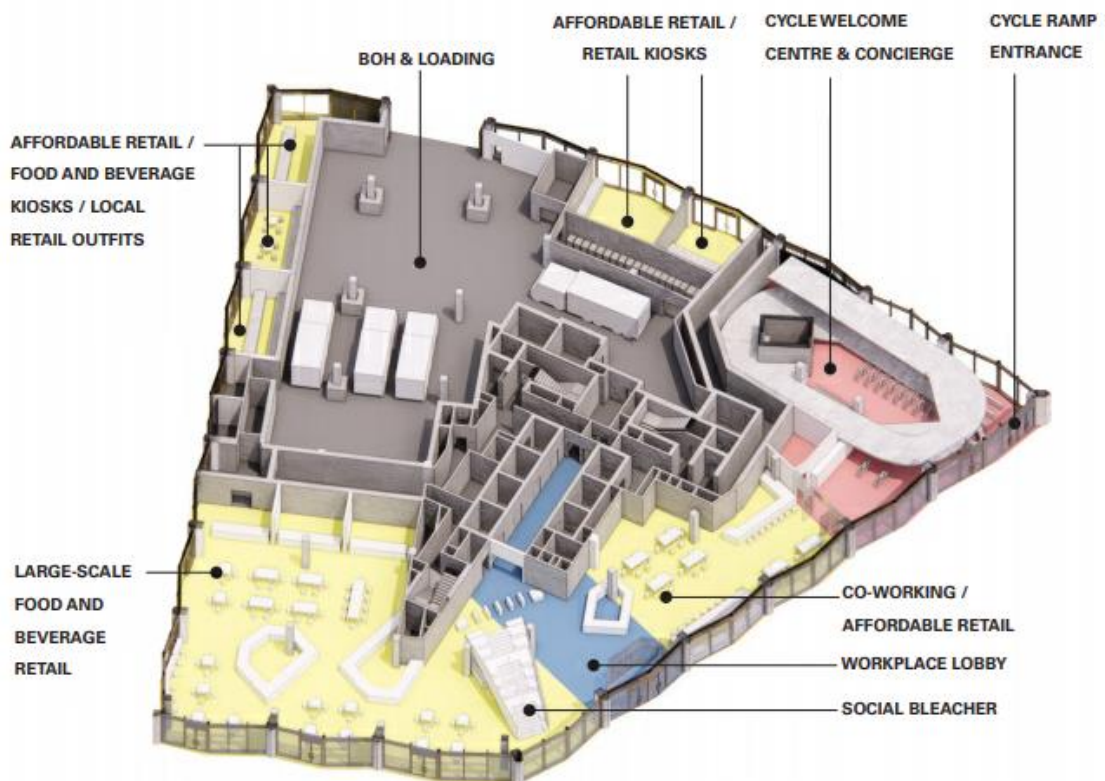
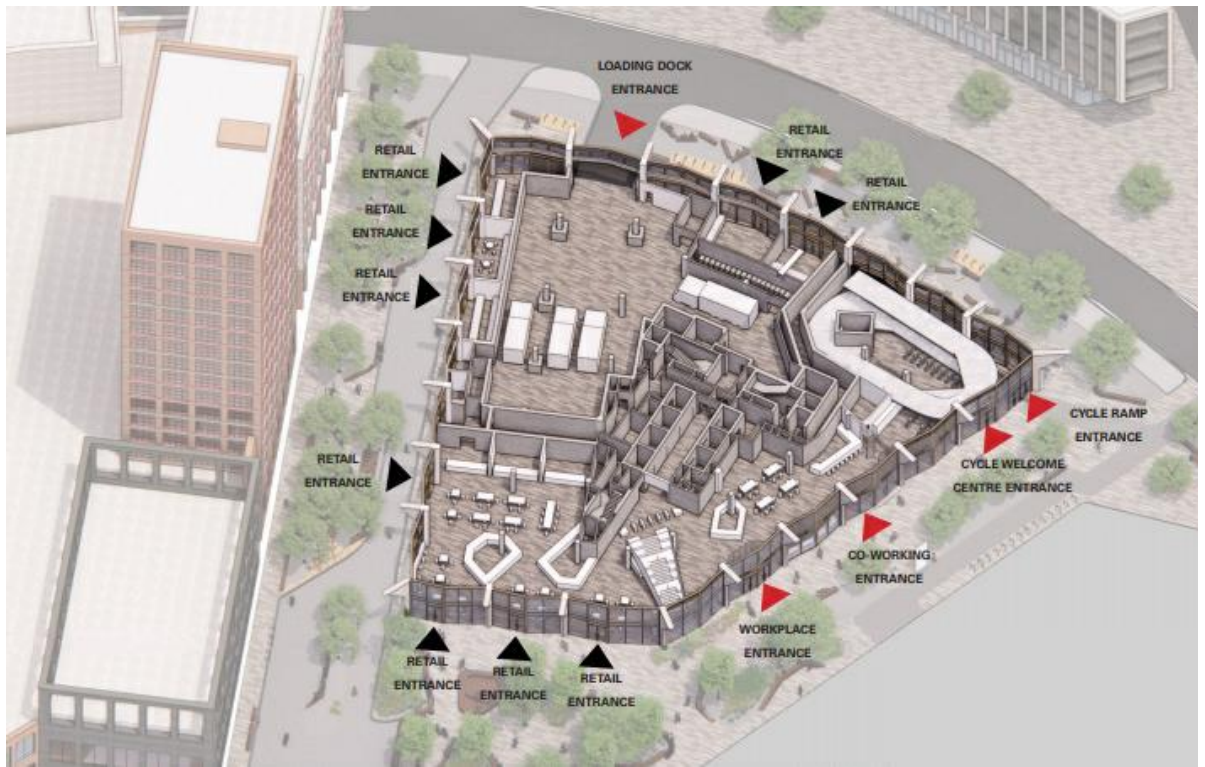
**Image above:** Office floor layouts

#### 194. Layout Building A2

The ground floor layout of Building A2 proposes a single vehicular access/egress to the loading bay and basement parking area via Surrey Quays Road. There are individual retail units proposed to wrap around the ground floor frontage on Surrey Quays Road and Maritime Street. The Waterfront Square and Boulevard frontages have been designed to facilitate a large open plan style retail/market hall with combined access to the office lobby. The design is intended to positively activate the adjacent areas of public realm and to draw the public into this part of the building by blurring the lines between internal and external spaces.

195. Promoting sustainable travel is a key priority for this development, cycle access into the building has been deliberately designed to have a prominent and visible entrance/exit point onto The Boulevard. This has been achieved by designing a large cycle ramp in the building foyer located towards the Surrey Quays Road end of the building. The building has been designed to prioritise travel by bike and has given over a large proportion of space to the access ramp as well as cycle storage facilities and showers occupying prime space on the mezzanine level.
196. The layout of spaces proposed for the ground floor, location of retail areas and entrance and exit points successfully optimises the accessibility and activation of the site responding well to the existing and emerging context.



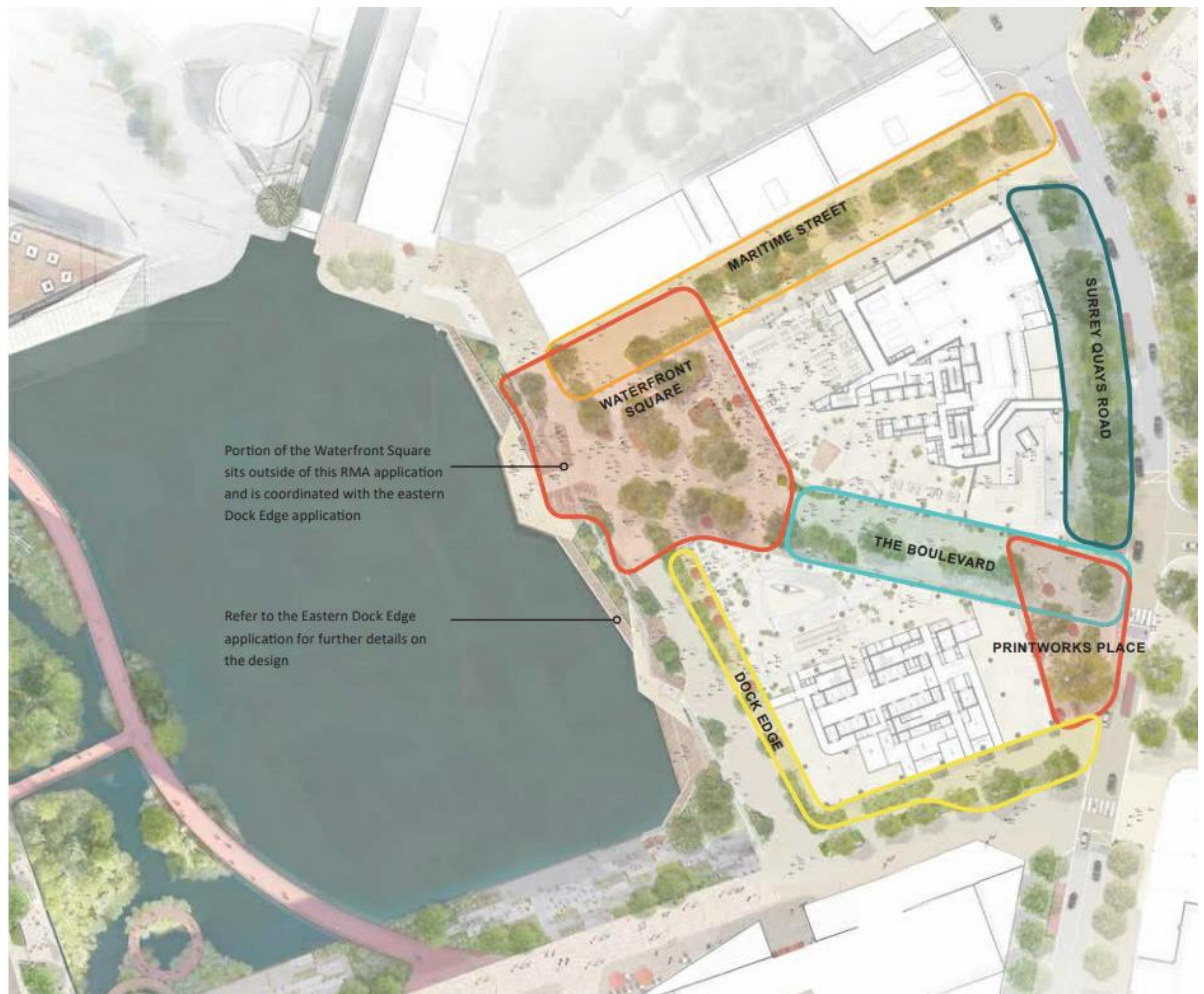


**Images above:** Proposed ground floor layout for Building A2 showing access points and layout of spaces.

197. Given the scale of development coming forward on this site, it is necessary for the scheme to deliver an appropriate amount of high quality public realm. The approved 'Proposed Public Realm Extent' plan clearly defined the location of building plots and the approach to ensuring that the scheme would deliver an appropriate provision of high quality public spaces. The masterplan has been structured to take advantage of

its prominent position on Canada Water Basin. The Dock forms the western edge of the site and has been occupied for some time by a two storey retail pavilion. The British Land Canada Water Masterplan elevates the Dock to the primary civic space of the town centre. The OPP for this site sets Buildings A1 and A2 back from the Dock edge to create a generous route and space for people to sit and enjoy the water. It also establishes a new public space (the 'Waterfront Square' next to the Dock at the junction with Maritime Street. This new public space will form a focus and a destination for people in the town centre. As part of the RMA it is necessary to ensure that the detailed design of this space will result in a high quality environment for existing and future users.

198. The diagram below shows the range of public realm to be delivered as part of the development of Plot A.



199. Waterfront Square

Waterfront Square is envisaged as the heart of the development. Its relationship to the water and orientation to the sun will mean that it is the most popular and attractive external space which will be delivered as part of this proposal. The square is intended to create areas of soft and hard landscape as a gathering space, together with spill out space for the commercial units and accommodating key connections from Deal Porters Square to Printworks Place. Whilst the proposals for the Dock Edge Scheme fall under a separate planning application, the Waterfront Square proposals are designed to coordinate with the Dock Edge proposals.





*Image above: CGI views of Waterfront Square*



**Image above: CGI of Waterfront Square**



200. The above section through Waterfront Square demonstrates that the public realm can adequately accommodate the movement of pedestrians and cyclists, as well as areas for soft landscape, seating and spaces to dwell whilst allowing spill out space for the commercial uses adjacent to the building. A variety of seating options below a canopy of trees together with play elements in the landscape and a platform seating feature will help to create an interesting and attractive public space which will complement the proposals for the dock enhancements both on the eastern edge of the dock but also the wider surroundings including Deal Porters Square and the southern dock edge. The detailed design of the play features and seating will form part of Condition 26 attached to OPP.

201. The Boulevard

The diagonal 'boulevard' route through Plot A leads from Waterfront Square towards the new Park being delivered as part of British Land's Masterplan. This is intended to be the main pedestrian street within the site. The proposal is to create a verdant pedestrian street with planting zones beneath clusters of trees which will create pockets of space for people to dwell. The extent of planting and trees provides a human scale to the linear space, ensuring comfortable environments are provided, these are intended to be quieter spaces which complement the more active uses within the adjacent Waterfront Square. The design extends planting close to the building edge along A1, this directs pedestrians away from edges that don't have entrances and enhances the views from the internal spaces. The principal pedestrian route is provided along the frontage of A2 where there are more entrances, this route meanders along the planting areas and provides a minimum width of 5m.



*Image above: Illustrative view looking along the Boulevard to Waterfront Square*





**Image above:** Illustrative view looking along the Boulevard to Surrey Quays Road

## 202. Surrey Quays Road and Printworks Place

Surrey Quays Road is intended to be characterised by a number of mature retained trees along the road frontage, with a series of flexible spaces beneath them, with areas of fixed seating alongside space for pop-up's and activities for the ground floor of the adjacent buildings to spill out in to.







**Image above:** CGI of Surrey Quays Road and The Boulevard

### 203. Dock Edge

The dock edge forms an important route between A1 and the Dock and forms the interface with the adjacent British land Zone D development. This area will provide a key movement space along the Dock Edge, providing access into the building as well as pedestrian and cycle routes. Whilst the proposals for the Dock Edge works fall under a separate planning application, the public realm proposals as part of Plot A1 extends from areas around the eastern and southern edges of Building A1 and are designed to coordinate closely with the Dock Edge proposals. Along the eastern frontage to Building A1, it is proposed to include further tree planting and areas for outdoor seating and socialising. The Southern edge of the plot forms a continuation of the Dock Walk proposed as part of the adjacent British Land Masterplan. The proposals for this development integrate with the proposed British Land Masterplan space, creating pockets of planting and seating.



**Image above:** CGI of Dock Edge (west of A1)

## 204. Dock Enhancements

The OPP secured enhancements to the eastern edge of the Dock. The Dock sits outside of the applicant's ownership and therefore these enhancements were secured as part of a s106 obligation. The applicant has developed a detailed proposal which is subject to a separate report to be considered to Members at the same Committee Meeting.

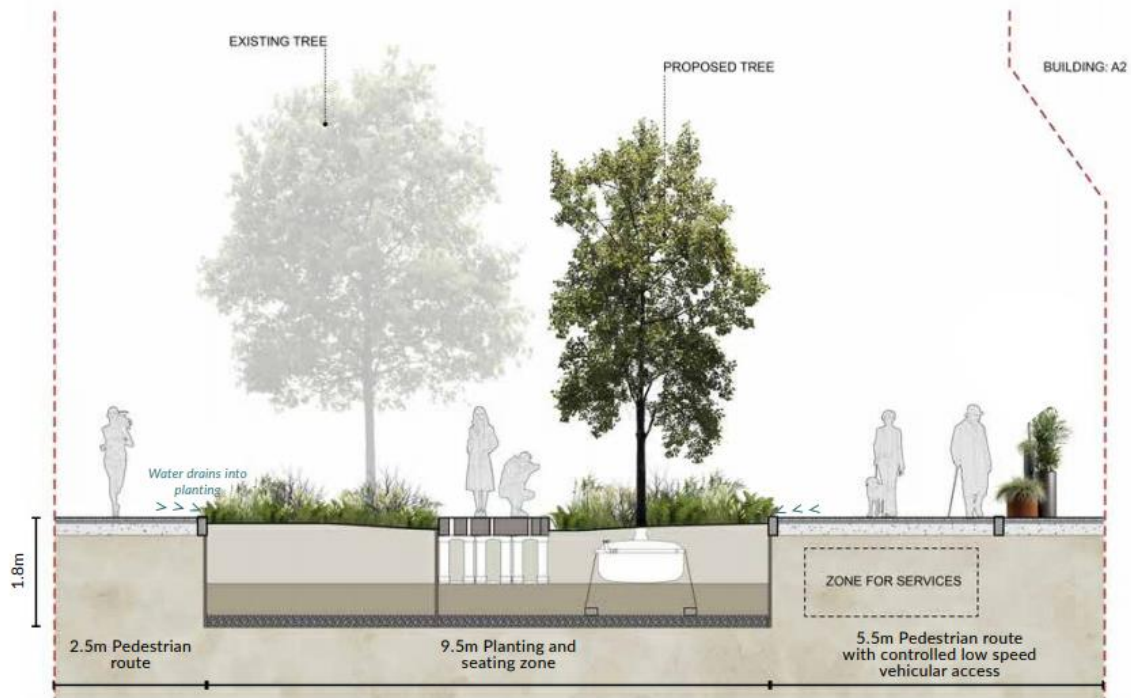
## 205. Maritime Street

The proposed development sees the redevelopment of Maritime Street from a vehicular route to a shared space with a centralised green spine and planting areas. Vehicular access will be controlled through bollards positioned towards Surrey Quays Road and is designed as a low speed environment (5 mph). Whilst this is a controlled environment and vehicle access will be limited to servicing vehicles for the adjacent Porters Edge Development, measures have been taken in the design to ensure safe use of the space by all. Areas of planting are used to control where vehicles can go and enhance the environment for uses, planters at the end of the street provide shelter and protection against the busier street environment. The introduction of activity within the space aims to naturally encourage any vehicles to slow when using the space, the vehicular route is defined by flush kerbs and a change in paving unit size and tone.



**Image above:** proposed layout to show loading bays bollard vehicle and pedestrian routes.





**Image above:** Section through Maritime Street

206. Vehicle tracking diagrams have been provided to demonstrate that safe access can be achieved for the necessary service and emergency vehicles.
207. The proposals provide an uplift in the amount of green space and create an interesting, active and inviting public realm. It is recognised that Maritime Street sits outside of the applicant's ownership and as such there is a fallback position secured in the s106 agreement whereby in the event that the applicant cannot secure all necessary consents to implement the enhanced landscaping proposals, a payment will be made to the Council towards public realm enhancements. The s106 obligation requires the applicant to demonstrate the full costs of the proposal and to make a payment for the full costs of work rather than setting a financial cap.



**Image above:** CGI Maritime Street

208. As part of the OPP an Internal Amenity Overshadowing Report was submitted.

It showed that Waterfront Square and Promenade would receive good levels of sunlight, while the Boulevard would receive less than 0.5 hours of sun (on 21st March). The Waterfront and Waterfront Square are likely to be the areas where most people will gather and as such, the sunlight amenity for the proposed public realm was deemed to be acceptable at outline stage. As part of the detailed design the layout of spaces and proposed planting has been developed to take account of the intended use for each space and climatic conditions in terms of sun light and wind.

209. The site layout is logical responding well to the context and the approved parameters in terms of locations of building and public realm provision. The proposed public spaces within and around the application site create new places to gather and socialise, and improve connectivity and permeability across the Canada Water area. Officers are satisfied that the detailed proposals will deliver the aspirations of the approved Outline parameters.

### **Height scale and massing and appropriateness of a tall building**

210. The OPP allows for the erection of two tall buildings in this location. The overall height of both buildings accords with the OPP parameter plans.
211. In terms of massing, the OPP proposed a height and massing strategy defined within the Design Codes and parameter plans. Maximum building heights, proposed plot extent - dominant face & upper levels, and shoulder heights were approved as follows:
- Building A1 would be the tallest building with heights up to 110metres AOD (around 23 storeys) and a shoulder height of 30metres. The building would have a distinctive tiered form, comprising 2 double storey floors per masonry block, where the upper tiers would splay out to create planted terraces which shift the building orientation. Whilst the tiers step back away from the Dock, it steps forward on the upper levels toward Surrey Quays Road, and rises sheer onto Dock Edge Walk (where it faces British Land's Zone D). This gives the building a rather assertive character on these two faces, less softened by greenery. As the most prominent building within the application site it is intended to serve as a landmark building for the new town centre.
  - Building A2 would include heights up to 55.4metres AOD (around 13 storeys) and a shoulder height of 30metres. The east façade of the building is curved to follow the profile of Surrey Quays Road, and the upper floors step back on each face creating a series of green terraces.
212. The form and massing for both buildings accords with the principles established by the OPP parameter plans and Design Codes. Which includes Building A2 being set back by 9m at ground level from the maximum parameter line on the western elevation to facilitate the delivery of Waterfront Square.
213. Building A1

Building A1 has been designed as a landmark building on the Dock. The building accords with the maximum height agreed at outline stage (110m AOD). It is intended to be read as 5 stacked blocks with terraced setbacks at upper levels on the western façade and staggered overhangs on the eastern façade. Building A1 will have a 1.5m setback on the north, east and south elevations, and the west elevation will have a

deeper setback widening towards the north, creating shelter along the Dock Edge. The first 'block' of Building A1 matches the common shoulder height along with building A2 and other existing and emerging buildings. The building sits within the agreed Outline parameters.



*Image above: West elevation of Building A1 (facing the Dock)*

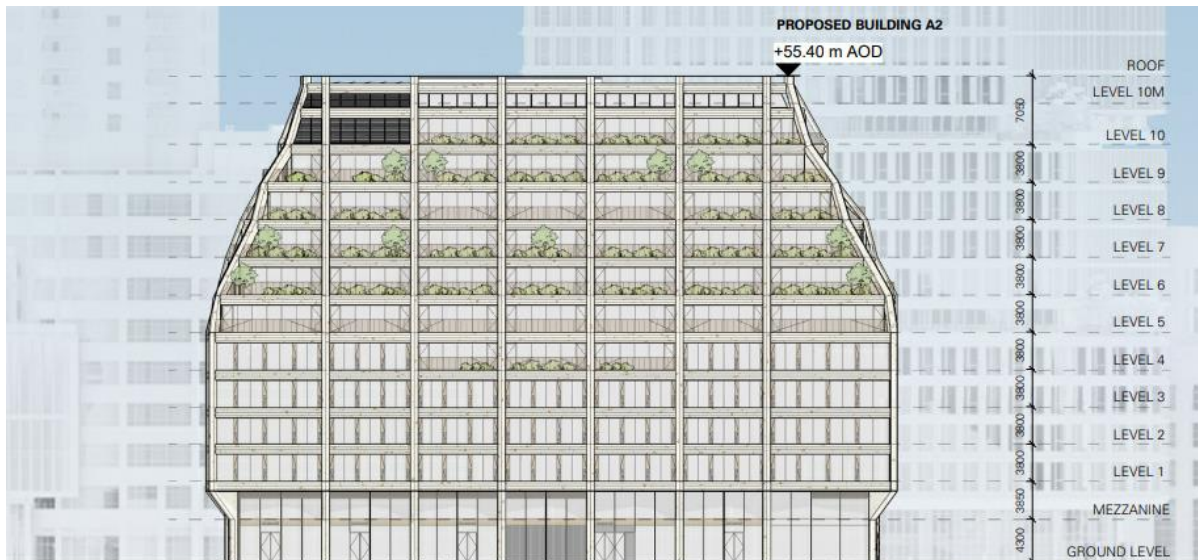




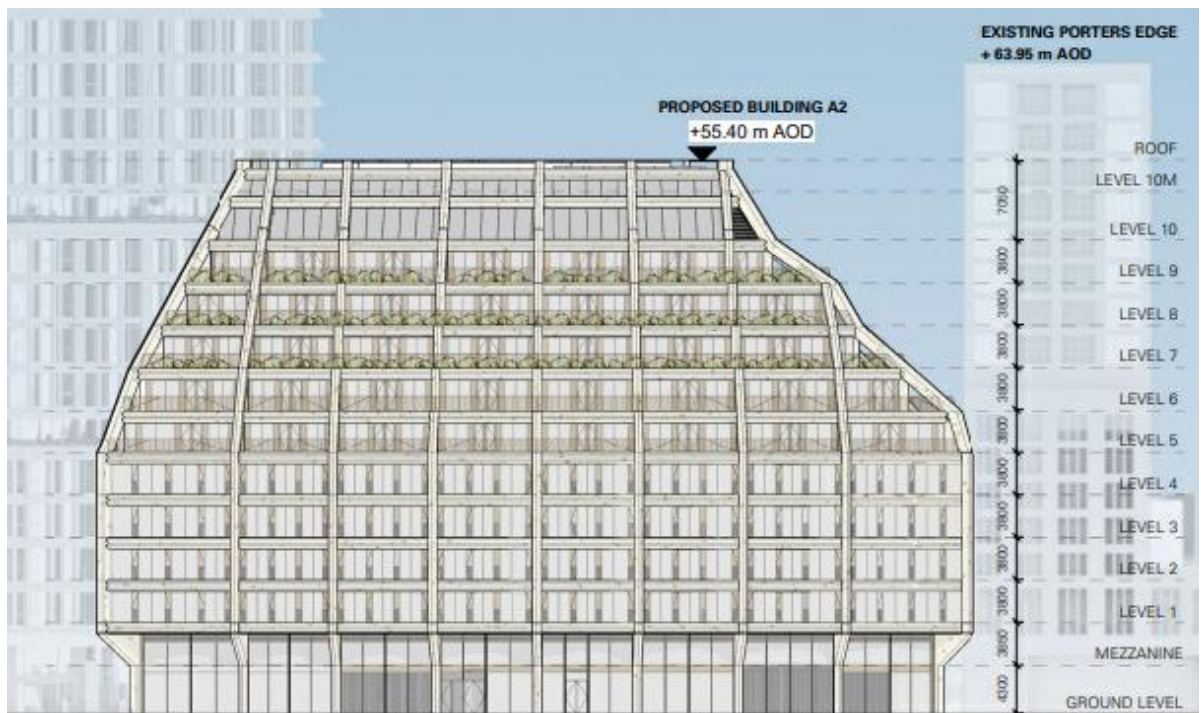
**Image above:** South elevation of Building A1 (facing the British Land Zone D)

#### 214. Building A2

Building A2 has been designed as a highly sculpted building in terms of its form and mass sitting with the Outline approved maximum building parameters as shown in the elevations below.



**Image above:** Proposed north elevation of Building A2 facing onto Maritime Street



**Image above:** Proposed east elevation of Building A2 facing onto Surrey Quays Road

215. The roof level of Building A2 will accommodate an external roof terrace for occupiers of the building (not open to the public) as well as plant, green roof and PVs panels. None of the Rooftop Plant elements will rise above the maximum parameter envelope.
216. Any tall building development must have regard to the impact on the wider London skyline and act as a way to identify important places in the borough and local area. Buildings A1 and A2 will be seen prominently in views from the surrounding area, however it is mainly building A1 which will be seen from long distances, given the additional height. Building A1 will form part of the emerging tall building cluster with those already consented in the Canada Water Masterplan, creating a strong identity for the area as a new town centre. The cluster will have a clear and coherent composition whilst still allowing for each tall building within it to remain well defined.
217. In the local context, buildings A1 and A2 form an enclosure to the dock edge and begin the transition from the lower heights of the Porters Edge development to the taller towers of the Canada Water Masterplan's central cluster. There is sufficient separation between both Building A1 and A2 for both structures to be independently read, helped by A2 being setback further from the dock edge and recessed within the site.
218. Overall the scale and mass of the proposed buildings is appropriate for this location and sits comfortably within the approved maximum height parameters for the OPP.

### **Architectural design and materials**

219. Southwark Plan Policy P14 sets out the criteria for securing high quality design. In respect of architectural design and materials the policy requires all developments to demonstrate high standards of design including building fabric, function and composition; presenting design solutions that are specific to the site's historic context,

topography and constraints; responding positively to the context using durable, quality materials which are constructed and designed sustainably to adapt to the impacts of climate change.

220. The OPP Design Codes contained a detailed set of codes for the buildings and landscape to ensure high quality execution at RM stage. The codes were developed in a way that would enable the buildings at reserved matters stage to be designed by a number of different architectural firms whilst adhering to the common principles and guidelines set out within the control documents.
221. The overall development is considered to be successful in its architectural design and conformity with the design codes set out under the OPP. The development is also considered to respond sensitively to the local and emerging context of Canada Water.
222. The OPP required each building to have its own architectural character which has been achieved effectively. Each building has been designed to deliver a striking architectural form, appropriate to the exceptional high quality expected from this scale of development. The varying heights and contrasting compositions help to establish individual architectural characters for each building. The different architectural characters also provide visual interest to the development whilst common principles such as the shoulder height and material tone help to unify the group.
223. Building A1

Under the OPP, building A1 was conceptualised as a stack of urban scale blocks and was intended to differ from a typical high rise. This concept has been executed successfully as the building form has been designed to be clearly read as five separate blocks. The blocks have been given a domestic scale with the use of a grid facade, breaking the blocks down into small bays. The offsetting of the terrace helps to differentiate each of the stacked blocks and there is a strong angular design to the building. The use of the stacking form will provide visual interest in local views of the building as well as establishing an individual identity when viewed from across London and further distances. The angular form of A1 further contrasts the more slender towers in the Canada Water Masterplan and reinforces the individual architectural character of the building.

224. The ground floor of A1 has been designed as double height with large areas of glazing, each vertical block above contains 4 office floors of accommodation. One of the main drivers for the facade design is to reduce the operational energy use through the employment of passive design elements. In order to articulate the facades, a layered approach has been taken, the outer layers seek to provide interest to the building whilst the inner layers deal with the environmental requirements for light and shading. The outer facade language was developed with a consistently modulated pattern, while the inner layer's solid-to-glazing ratio varies, responding to the solar analysis to reduce energy demand, particularly for cooling.
225. While optimising the facade elements to meet the requirements for energy performance, solar shading fins and partially openable windows have been integrated into the facade system. Vertical fins were added to west and east elevations, and horizontal fins were added to south elevations to maximise the shading effect based upon the prevailing angle of the sun in the sky. Solid infill panels are varied throughout the elevations responding to the solar gain analysis.

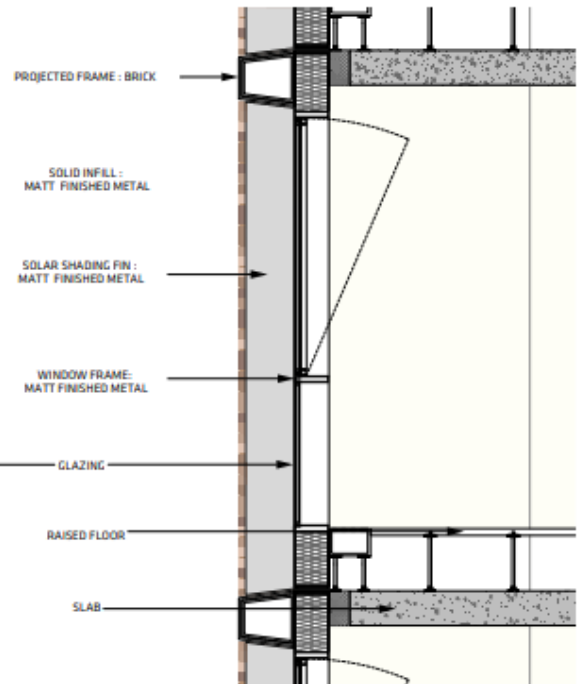
226. The main projecting frame will be constructed of brick which will respond well to the site surroundings and will stand the test of time. Variations between blocks have been achieved with the consistent facade system by varying the material colour and tone of each block. The brick choice will range from a darker brick at the base to anchor the building in its context to a lighter tone brick at the top which will help to reduce its sense of scale in longer views.
227. The ground floor facade has been distinguished from the main facade language, being more transparent and open, presenting an open and accessible ground floor. Functional features, such as louvres, signage and wind mitigation have been integrated with the ground floor facade system. Perforated vertical wind panels are proposed across the ground floor facade to mitigate the potential for strong seasonal winds affecting pedestrian comfort in the vicinity of the building (on the south and west elevations only). The vertical fins integrate into the vertical metal recesses that are part of the facade framing around the ground floor.
228. As with any building it is necessary to incorporate 'back of house' functional requirements such as ventilation, plant, emergency exits and service areas. These necessary elements have mostly been located on the southern façade of the building which is appropriate given the need to maximise activation onto the Boulevard, Dock Edge and Surrey Quays Road as the main thoroughfares and areas of public realm and accords with the OPP parameter plans. The design of the southern façade successfully accommodates these necessary elements whilst still maximising active frontage with the inclusion of commercial uses on both ends of the façade. The quality of the louvres, glazed and non-glazed elements will be controlled via the conditions already attached to the OPP in respect of material samples, large scale drawings and mock-ups.
229. It should also be noted that the Dock Edge Walk between Building A1 and the adjacent British Land Zone D will also be coming forward as a landscaped route. Whilst the designs are still being considered by British Land, it is understood that this will likely include planting, trees and the provision of seating locations in which to encourage pedestrian movement along the route, which will ensure the southern elevation is a destination in its own right, rather than a 'back of house' elevation.

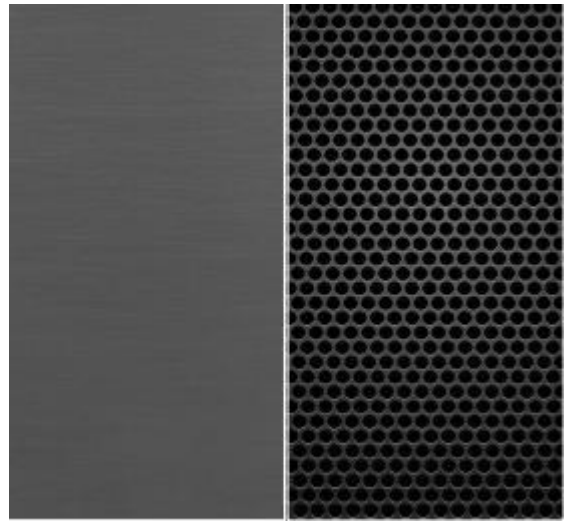
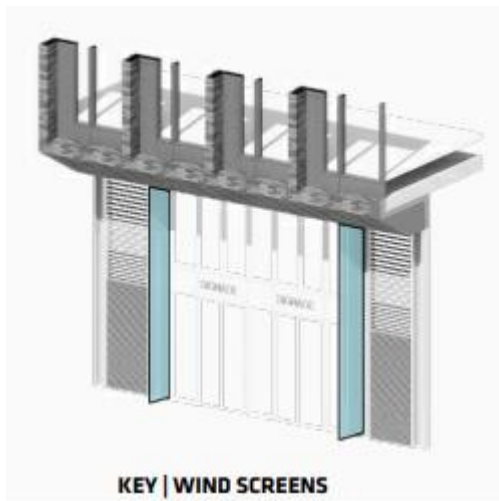




**Approach to materiality**

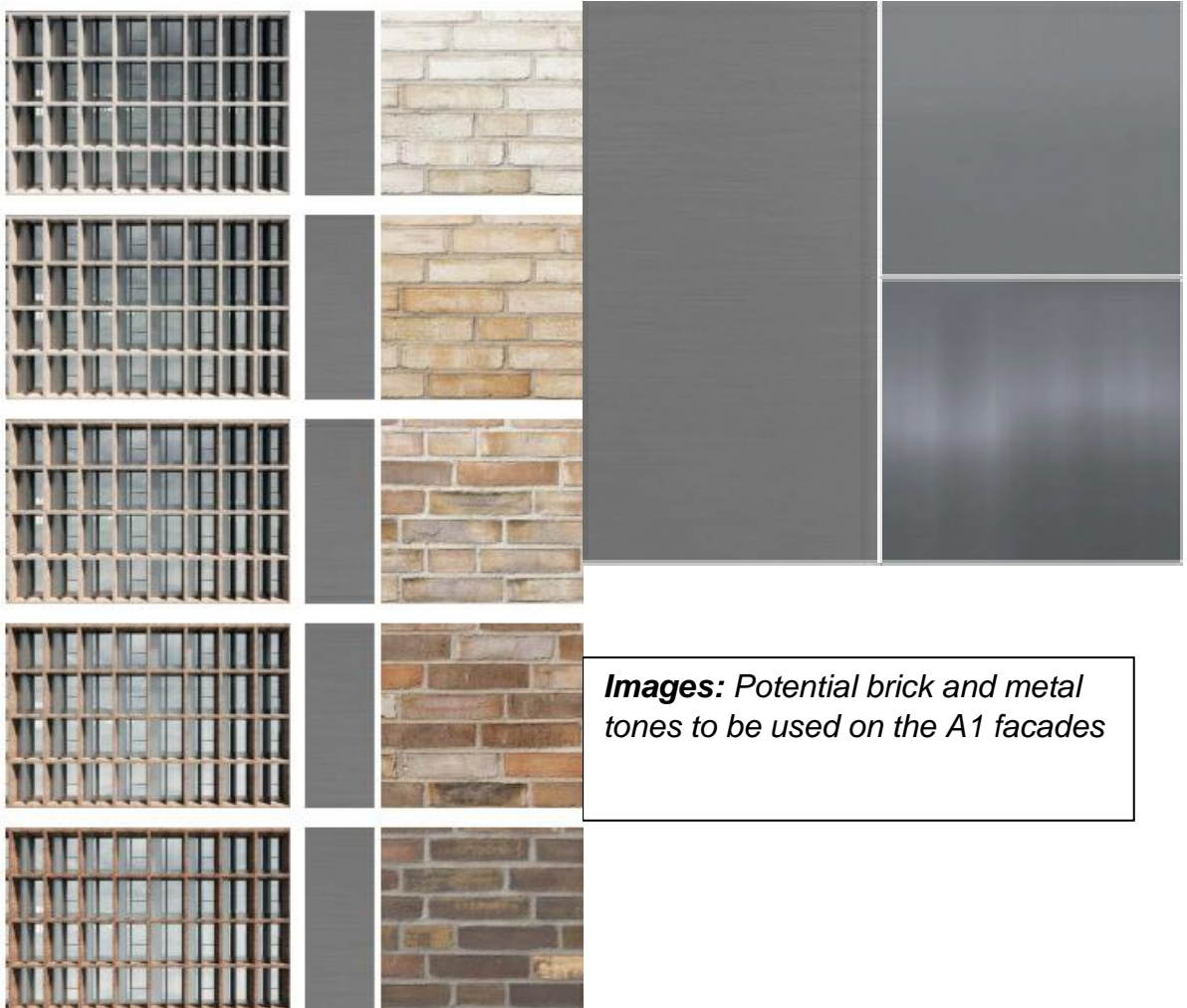
**Approach to ground floor facade & elevation**





*Images above: A1 façade design*

230. The chosen materials pallet for Building A1 is warm tone bricks for the frame with metal infill panels, fenestration and solar fins and metal framed glazed balustrades. Soffits are a key design feature on this building and will be particularly prominent in views from Surrey Quays Road where the upper levels cantilever out. The soffits will match the brick material of the block above adding articulation to the facades. There are conditions attached to the Outline permission to secure full details of materials and mock ups (Conditions 30 and 31).



*Images: Potential brick and metal tones to be used on the A1 facades*

231. The stacked and setback design to the massing has enabled the creation of large

outdoor terraces on the western (dock facing) façade and smaller inset terraces on the other facades. The terraces have been designed to accommodate generous planting as well as amenity areas.



*Image above: CGI to show larger terraces on the western (Dock Edge) façade*



AXONOMETRIC | INSET TERRACES



AXONOMETRIC | TERRACES

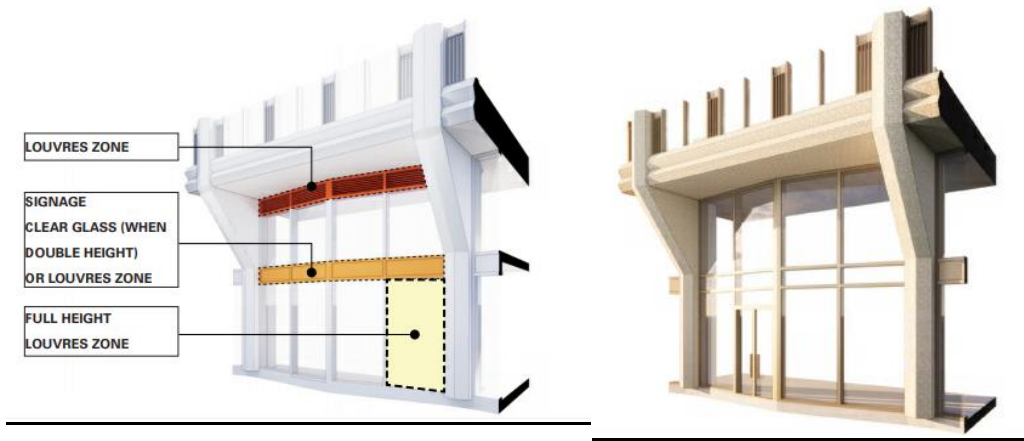
*Image above: Axonometric of larger and smaller terraces*

## 232. Building A2

Building A2 was envisaged as a medium rise building under the OPP, providing a transition in scale from Porter's Edge to the taller heights of A1 and Canada Water Masterplan. This has primarily been achieved through its height, set under the OPP. The building tapers as it rises which provides a visual interest to its form as well as ensuring the transition in heights. Building features such as columns at ground level and use of varying scale terraces bring strong articulation to the elevations which enhance the overall design quality.

233. The building has been clearly designed with 3 horizontal sections: the base which comprises the ground and mezzanine level (inset from the dominant face level above) the dominant face and then the inset terraced upper levels. The architectural treatment for Building A2 has been designed on the basis of a series of urban rooms on the lower levels with verdant terraces on the upper levels. The façade design has been developed following solar gain analysis in order to determine the appropriate ratio of glazing, openable and solid elements. The materials pallet has been chosen to reduce the buildings embodied carbon whilst making a positive contribution design response. .





*Image above: example ground floor façade design*

234. The chosen materials pallet comprises polished pre cast concrete in warm cream/gold tones with bronze metal balustrades, fenestration, solid panels elements and detailing. There are conditions attached to the Outline permission to secure full details of materials and mock ups.



*Image above: example façade to show materials selection and façade composition.*





**Images above:** CGIs of ground floor from various viewpoints

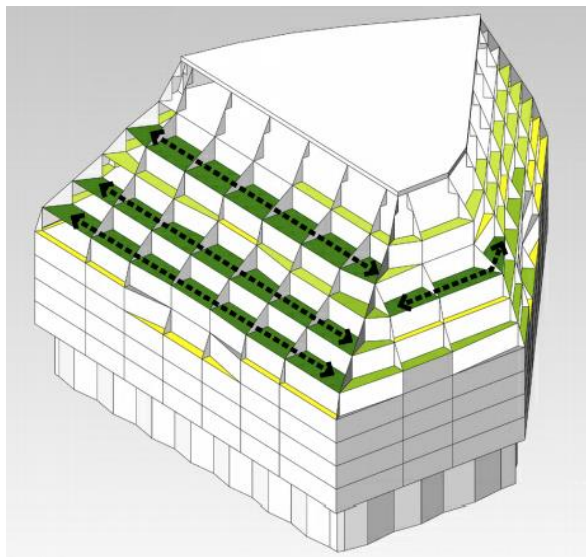
235.



**Image above:** View of A2 from Surrey Quays Road

236. The planted terraces are a fundamental part of the design, making a positive contribution to the building in terms of aesthetics, amenity and environmental sustainability. The terraces are organized into 4 categories. The large inter-connected terraces occur most often on the west and north facades. The depths of the terraces create different opportunities and types of plantings. The largest terraces can accommodate planters with trees and integrated benches. However, even the smallest Juliette style terraces offer future occupiers an opportunity to connect to the outside, offering natural ventilation and views of Canada Water.





- Large terrace (4m-5m): 24
- Medium terrace (3m-4m): 13
- Small terrace (1.5m-3m): 48
- Visual outdoor connections



**Images above:** location and types of terraces which form a fundamental part of the design for Building A2.

237. Details of building maintenance and cleaning strategies have been provided within the submissions to demonstrate that both buildings are robust and will stand the test of time in terms of their high quality design.
238. Officers consider that the detailed proposals for Buildings A1 and A2 as demonstrated in the RMAs will make a positive contribution to the immediate townscape and character of the area. Conditions 30 and 31 attached the OPP secures the submission of detailed samples and mock up bays to ensure that the high quality design is retained during execution.

### **Heritage considerations and impact on protected views**

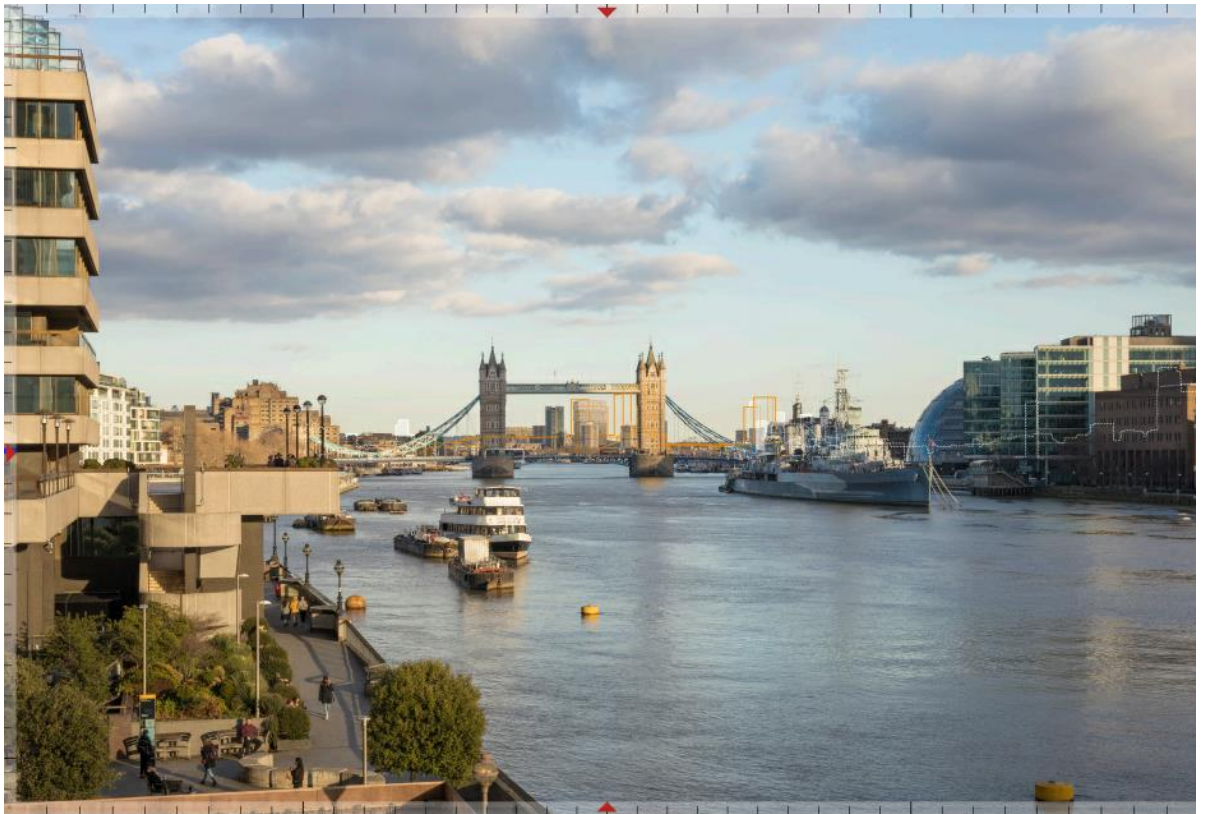
239. Section 66 of the Planning (Listed Buildings and Conservation Areas) Act 1990 requires local planning authorities to consider the impacts of a development on a listed building or its setting and to pay special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses.
240. Chapter 16 of the NPPF contains national policy on the conservation of the historic environment. It explains that great weight should be given to the conservation of

heritage assets. The more important the asset, the greater the weight should be (paragraph 199). Any harm to, or loss of significance of a designated heritage asset should require clear and convincing justification (paragraph 200). Paragraph 202 explains that where a development would give rise to less than substantial harm to a designated heritage asset, the harm should be weighed against the public benefits of the scheme. Paragraph 203 deals with non-designated heritage assets and explains that the effect of development on such assets should be taking into account, and a balanced judgment should be formed having regard to the scale of any harm or loss and the significance of the asset. Working through the relevant paragraphs of the NPPF will ensure that a decision-maker has complied with its statutory duty in relation to Conservation Areas and Listed Buildings.

241. Development plan policies (London Plan Policy HC1 and Southwark Plan Policies P19, P20 and P21) echo the requirements of the NPPF in respect of heritage assets and require all development to conserve or enhance the significance and the settings of all heritage assets and avoid causing harm.
242. The site does not include any listed buildings and is not in a conservation area. However within the vicinity of the site are a number of heritage assets including: The Grade II Listed Dock Manager's Office and 1-14 Dock Offices on Surrey Quays Road and the Turntable and machinery of the former swing road bridge near Redriff Road. Nearby are the Grade II Registered Southwark Park and the Grade II Listed Former Pumping Station on Renforth Street. The area is rich in undesignated heritage assets and structures including the Canada Water Dock and its associated Dock structures and channels, Greenland Dock and Stave Hill. Further afield are a number of Conservation Areas including the St Marys Rotherhithe and the Edward III's Rotherhithe Conservation Areas, both located on the banks of the river, north of the Masterplan. The north bank of the river in Tower Hamlets also includes a number of conservation areas.
243. Due to the scale and massing, Building A1 and A2 will be visible from various viewpoints within the area, as well as in longer views. A1, as the taller building, will be more visible in further views whilst A2 will be seen mostly from local views.
244. The application is accompanied by a Townscape, Heritage and Visual Impact Assessment (THVIA) which identifies the visual impact of the proposed buildings. It consists of a series of accurately prepared photomontage images or Accurate Visual Representations (AVR) which are designed to show the visibility and appearance of the proposed buildings from a range of publicly accessible locations around the site.
245. The views shown in the THVIA were previously agreed upon under the OPP, where 24 views were used to demonstrate the visibility of the masterplan. The views examined in detail how the maximum parameters would appear from several vantage points both in the context of protected views and panoramas as well as incidental and important local views. The OPP established that there would be no significant adverse effect on Built Heritage and no mitigation was required. In terms of Townscape Visual Mitigation, the OPP concluded that this had been designed into the scheme as far as it could for an outline application. The success of the scheme in townscape terms would therefore rely on the detailed design of the final buildings, the quality and choice of materials as well as the type of landscape proposed and that these factors should be carefully assessed at the reserved matters stage. The detailed design of the proposed development has been discussed at length in the relevant sections of

this response however in summary, the buildings are found to be well-designed and well-finished.

246. Of the 24 views shown under the OPP, 11 have been updated with the detailed design of the reserved matters proposals;
- View 1 - London Bridge point A
  - View 2 - London Bridge Point B
  - View 7 - Waterside Gardens, Wapping
  - View 10 - Southwark Park - NE from public footpath over open ground
  - View 12 - North side of Surrey Quays, looking south
  - View 14 - Stave Hill
  - View 16 - Greenland Dock, South side looking NW
  - View 20 - Canada Water, N Corncer, panorama
  - View 21 - Canada Water, SW corner, panorama
  - View 22 - Surrey Quays Road, looking north
  - View 24 - Canada Street
247. Long views such as those from London Bridge show building A1 will be visible when built, however it will form a clear part of the wider Canada Water tall buildings cluster. Even without the future tall buildings, the effect of A1 was seen to be minor, as was established and approved under the OPP.
248. Local views from Southwark Park and Surrey Quays show A1 as a prominent addition to the skyline however the building will again form part of the Canada Water tall buildings cluster. The stacked design and stepped height further helps to mediate the appearance of the A1 whilst also providing the strong architectural identity which will make it a landmark building. The subtle variations in the colour of the five stacks can also start to be seen from these views which provides a visual interest.
249. A2 is mostly visible in views from the dock or immediate surrounding streets, due to its lower height. From these close views, building A1 and A2 provide a framing of the dock and mark the transition of the heights rising towards the Canada Water tall buildings cluster.
250. Overall, the effects presented in the updated views remain the same as established under the OPP. The update has shown the high level of design quality of the proposed buildings and how well the development will sit within the local townscape as well as the emerging identity of Canada Water as a new town centre.



**Image above:** Verified view from London Bridge (Point A ) with BL Proposals shown in wire line

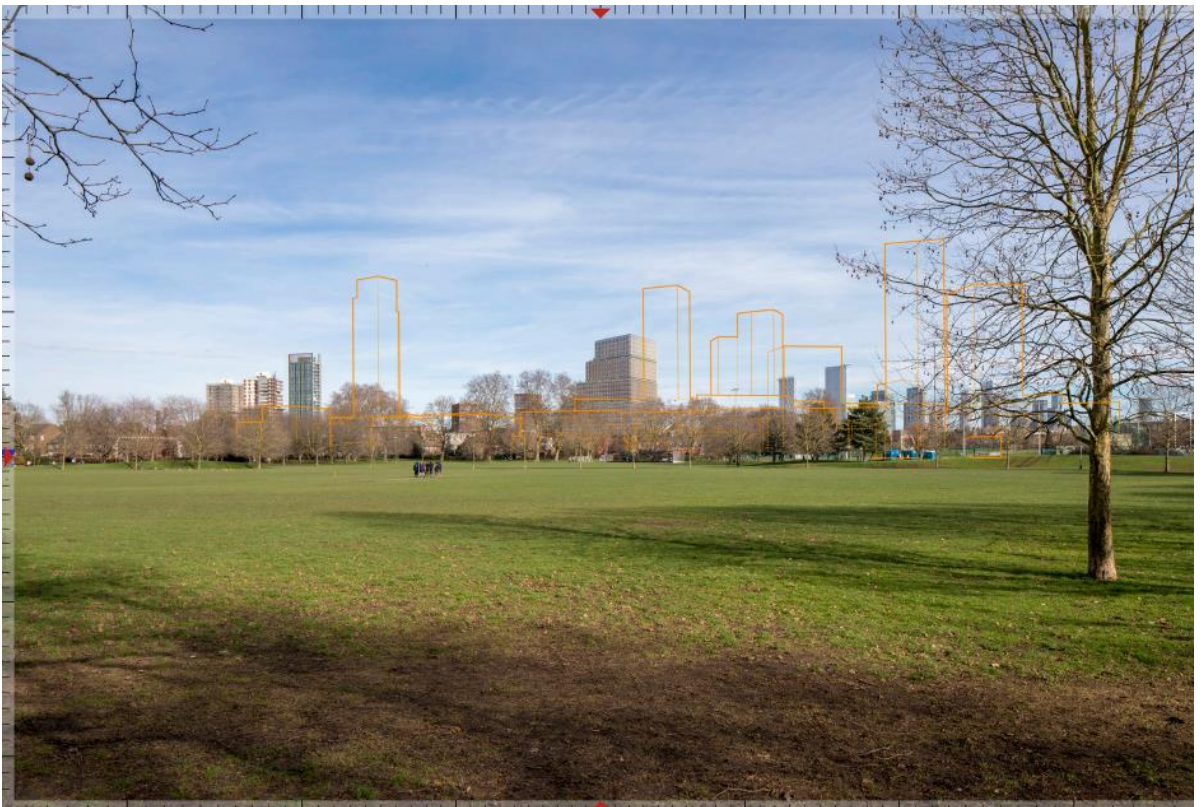


**Image below:** Verified view from London Bridge (Point B) with BL Proposals shown in wire line





**Image above:** Verified view from Waterside Gardens with BL Proposals shown in wire line



**Image above:** Verified view from Southwark Park with BL Proposals shown in wire line.





**Image above:** Verified view from North side of Surrey Quays with BL Proposals shown in wire line.



**Image above:** CGI view of A1 and A2 from the western edge of the Dock



*Image above: CGI night time view of A1 and A2 from the western edge of the Dock*

## **Landscaping, trees and urban greening**

251. London Plan Policy G7 and NSP Policy P61 recognise the importance of retaining and planting new trees wherever possible within new developments, Policy G5 requires major development proposals to contribute to the greening of London by including urban greening as a fundamental element of site and building design, and by incorporating measures such as high-quality landscaping (including trees), green roofs, green walls and nature-based sustainable drainage. The policy identifies a scoring system for measuring urban greening on a particular site (Urban Greening Factor) and suggests a target score of 0.3 for predominately commercial development.
252. At the present time the site comprises mainly buildings and hard landscaping consistent with a commercial site although there are a number of trees within the car park. Therefore the opportunity exists for significant improvements to be made in terms of soft landscaping proposals and contribution towards urban greening.

### Landscaping

253. The Landscaping strategy for Plot A of the Canada Water Dockside Masterplan relates specifically to the Waterfront Square, the Boulevard, Maritime Street, Surrey Quays Road, Printworks Place and the Dock Edge Walk. The proposals include the planting of new trees and works to existing trees, the provision of new planting, street furniture and play equipment, all in accordance with the principles for the public realm set out within the Design Code for Landscaping approved as part of the Canada Water Dockside Masterplan.

254. The landscaping strategy has been designed to provide year-round visual interest and reflect seasonality and change, and provide an attractive setting for people. The following principles have been established :-
- Select planting to reinforce and aid connections and integration with local green spaces.
  - Select species with high value to wildlife, especially insects.
  - Specify a variety of species to increase biodiversity in relationship to the site's condition today.
  - Include edible planting (such as blackberries) to reinforce the connection and opportunities for engagement with the landscape.
  - Ensure the planting does not detract from a safe environment.
  - Selecting a varied species palette that will create a layering of vegetation and spatial forms.
  - Planting native species and non-native species of value to wildlife, including planting for pollinators.

The detailed proposals for each area were discussed in the public realm section earlier in this report.

255. The OPP confirmed that the whole Masterplan site (including Plot B) contains 92 trees of varying qualities, of which 30 were secured to be retained. Furthermore, the OPP stated that lateral pruning of 19 of the retained trees would be necessary. The Outline proposals confirmed the planting of 94 new trees (including the trees to be planted in Maritime Street). This number of trees to be planted is secured by condition 14 attached to the OPP. The s106 includes an obligation for retention of the trees as set out in the parameter plans as well as a financial sum to be paid to the council for off-site planting should any of the existing trees to be retained subsequently need to be felled as result of the development. This includes 4 willow trees sited immediately beyond the red line boundary within the British Land masterplan site.
256. As part of the detailed design evolution it has become apparent that 2 trees within the site boundary for Plot A that were identified as being retained will need to be removed and additional pruning will be required for one of the retained trees. The removal is required to facilitate safe construction access into the site. Due to the only access being off Surrey Quays Road which is a main traffic route through Canada Water, located close to bus stops and construction access points into adjacent plots, a site meeting was held with the Urban Forester and Network Management Team and it was deemed necessary to remove the trees. This matter has been duly considered by the Urban Forester and removal of the two trees has been deemed to be acceptable. To mitigate this loss, replacement trees will be provided within the landscaped area for Plot A and a financial contribution will be payable in line with the s106 requirements. The necessary payment is £77,105. The tree retention parameter plan has been updated as part of the recently approved NMA.
257. Furthermore, the Arboriculture Method Statement (AMS) Addendum submitted with the RMAs confirms that it is necessary to remove the four willow trees outside of the site boundary, on adjacent land owned by British Land and forming part of the Canada Water Masterplan. As allowed for within the s106 a financial payment will be secured to mitigate the loss of these trees. The necessary payment is £93,916 (less the value of replacement planting, to be confirmed.).

258. The AMS submitted set out the scope of works required on-site and tree protection measures. This document has been scrutinised by the Urban Forrester and found to be acceptable.
259. The planting proposals across the site were amended to address initial comments from the Council's Urban Forrester and are now considered to be acceptable.
260. The detailed designs for Buildings A1 and A2 show that facades and terraces will accommodate a significant amount of planting which will both soften the appearance of the buildings and make a very valuable contribution towards Urban Greening. Both buildings will also include elements of green roof. Appropriate sections drawings have been provided to demonstrate that the structure of the buildings can accommodate the soil volumes and irrigation for the planting proposed. Planting species have been selected taking account of the site conditions in terms of climate, shade and habitat creation.
261. The Council's Urban Forrester has reviewed the landscaping proposals and Arboricultural Impact Assessment and is satisfied with the proposal.
262. Condition 25 attached to the OPP secures the implementation and maintenance of detailed landscaping proposals including hard and soft landscaping features, seating, signage and play equipment.

### Urban Greening

263. The OPP was subject to an Urban Greening Factor Assessment which suggested that the development would exceed the 0.3 target for commercial development. The RMAs have been accompanied by updated UGF assessments.
264. A comprehensive approach to greening has been taken at multiple levels, be that at public realm on street and within the new buildings themselves through landscaped terraces and green roofs. This introduction of new green spaces together with the retention of existing mature tree cover looks to significantly enhance the Urban Greening Factor of the existing CWDM site.
265. The details submitted with the RMAs indicate that the Plot A development will achieve the following UGF scores through a combination of existing trees, new tree planting, ground floor and terrace planting as well as green roofs.
- Building A1 a UGF score of 0.43 will be achieved
  - Building A2 a UGF score of 0.35 will be achieved
  - Maritime Street a UGF score of 0.42 will be achieved.
266. The Council's Ecology Officer and Urban Forrester have scrutinised the UGF details and confirmed that all opportunities have been maximised, exceeding the 0.3 target in the London Plan, this being a significant benefit of the proposal
267. Condition 25 attached to the OPP secures the submission of an updated UGF Assessment once the detailed landscaping proposals have evolved.

### **Ecology and biodiversity**



268. The protection and enhancement of opportunities for biodiversity is a material planning consideration. London Plan Policy G6 requires development proposals to manage impacts on biodiversity and aim to secure net biodiversity gain. This should be informed by the best available ecological information and addressed from the start of the development process. Southwark Plan Policy P60 seeks to protect and enhance the nature conservation value of Sites of Importance for Nature Conservation (SINCs), enhance populations of protected species and increase biodiversity net gains by requiring developments to include features such as green and brown roofs, green walls, soft landscaping, nest boxes, habitat restoration and expansion, improved green links and buffering of existing habitats.
269. The impact of the development upon ecology including the impact upon the Dock which is a protected SINC was robustly considered as part of the OPP (within the Environmental Statement) when the principle of the development was established. Appropriate ecological surveys and overshadowing assessments were submitted. Any impact upon ecology was deemed to be acceptable.
270. Ecological enhancements for this RMA will comprise features to be incorporated into the building fabric (biodiverse roofs) and planting for the terraces.
271. The Outline Planning Permission was designed to meet the target of 10%, and indicated a potential net gain of 136.78%. Since this calculation was undertaken, the methodology for calculating BNG has been updated by DEFRA. The RMAs were accompanied by a BNG Assessment which established a baseline by way of habitat and hedgerow surveys undertaken at the site as well as a detailed tree survey. Using the new calculator and with no material change in the proposed tree and greening across Plots A1 and A2 (and using the illustrative scheme for Plot B), the masterplan overall now achieves a net gain of 41.97%. This is significantly in excess of the requirement of 10%, indicating that the proposals for Plots A1 and A2 meet and exceed the policy requirements and that Plot B can be designed in a manner that allows the BNG requirements to be met and exceeded across the CWDM as a whole.
272. There are already conditions attached to the OPP in respect of soft landscaping, green/brown/biodiverse roofs and walls, biodiversity, habitat and ecology features, precautionary bat surveys and ecologically sensitive lighting. The impact of the proposal upon ecology has been fully considered and opportunities to enhance ecology have been maximised.
273. Condition 25 attached to the OPP secures the submission of an updated BNG Assessment once the detailed landscaping proposals have evolved.

### **Design Review Panel**

274. This application was presented to Southwark Design Review Panel in October 2022.
275. Building A1

The Panel felt the design of this building was well advanced and promising. The challenges of proposing a substantial building in such a prominent location were acknowledged, including that this design would need to fully confront its environmental impact, both in construction and in its whole-life operation.

The panel welcomed the use of tactile and enduring materials such as the brick and

terracotta but encouraged a greater consideration of the level of variations between the five main 'layers'. The final colour choices of the materials will be secured via a condition of planning permission which will require an in person inspection of all materials to be used in the scheme. This will allow for an assessment of the colour variations and to ensure the difference is sufficient to achieve the design ambitions of the proposals.

Bay studies were also requested as well as further information on the design of the terraces. This has been submitted as part of the reserved matters application in the relevant sections of the Design and Access Statement.

#### 276. Building A2

The Panel welcomed the exciting design for this building and considered many of its original ideas refreshing and interesting. The panel however felt that the proposal risked trying to incorporate too many complex ideas into one building and that the original design ideas had been diluted. The panel felt the concept of the 'island' was strong with a clearly defined 'shoulder', and that the stepped terraces arranged on the upper floors held some promise. However, a focus on construction, future maintenance and standardisation was encouraged. Bay studies were requested and have been submitted as part of the reserved matters application in the relevant sections of the Design and Access Statement.

Concern was raised about the use of brick slips, which were proposed as the dominant material in the scheme at this stage in the pre-application process. Although it was acknowledged that brick is a principal material within the local context of Canada Water, there was concern regarding the robustness of the brick slips and the use with the complex form of the building. Brick slips have since been removed from the scheme and the use of precast concrete proposed instead.

#### 277. Conclusion on design

In conclusion, the Panel welcomed the ambitious and high quality design approach taken with this development and supported the focus on sustainability and active travel. The panel generally supported the direction of travel in the design and welcomed the collaboration of multiple designers, encouraging closer coordination between the landscape and the building/s as well as between the buildings themselves.

The panel noted that the proposals generally conformed with the OPP but encouraged a greater expression of the key design principles such as the treatment of the buildings up to the shoulder, the delivery of accessible and safe public spaces as well as the design of workspaces that are attractive to large corporations and SMEs alike.

### **Designing out crime**

278. Policy D3 of the London Plan 2021 states that measures to design out crime should be integral to development proposals and be considered early in the design process. Developments should ensure good natural surveillance, clear sight lines, appropriate lighting, logical and well-used routes and a lack of potential hiding places. Policy P16 of the Southwark Plan 2022 reinforces this and states that development must provide clear and uniform signage that helps people move around and effective street lighting to illuminate the public realm.

279. These principles have been incorporated into the design of this buildings. The development will be required to achieve SBD accreditation. Compliance has been secured by way of Condition 32 attached to the OPP.

### **Fire safety**

280. Fire Statements have been submitted for Buildings A1 and A2 including the combined basement. The statements demonstrate compliance with the requirements of London Plan Policies D5 and D12 and have been prepared by a suitably qualified expert.
281. The Building A1 fire strategy has been developed to operate (from a fire strategy perspective) independently of Building A2 and makes no reliance on the fire safety provisions in Building A2. The only exception to this is that it is proposed for the two buildings to share sprinkler tanks and pumps.
282. For Buildings A1 access for firefighting facilities would be provided from Surrey Quays Road via the Boulevard and southern edge of the building. The building will have three full fire-fighting shafts, each will comprise, a fire-fighting stair having a clear width of 1300 mm, a fire-fighting lift, a mechanically ventilated fire-fighting lobby on all levels, an automatic opening vent at the head of the stair and wet fire mains having outlets on all levels and located within the fire-fighting lobbies. The building will be provided with a fire control centre (FCC) located at the ground level and accessed directly off the firefighter access route for Fire-fighting Shaft 2, to allow the fire-fighters to coordinate their operations.
283. The statement for Building A1 reviews the building construction and materials and sets out principles to be applied in respect of means of escape, provisions to stop the spread of fire within the buildings and externally, passive and active safety provisions, ventilation, maintenance and access for emergency vehicles. The building will be designed to operate a phased evacuation strategy throughout. The basement and mezzanine levels will be served by three fire-fighting stairs, the office levels 1 to 4 will be served by the three fire-fighting stairs for escape; Levels 5 to 23 will be served by two fire-fighting stairs. As two goods lifts will serve every level of the building (noting that this is not the case for the passenger lifts), the goods lifts will be designed as evacuation lifts to support MIP evacuation. These lifts will not double up as firefighting lifts. Further, there will also be a fire-fighting lift within each of the three firefighting shafts.
284. For Building A2 vehicle access to the site is via Surrey Quays Road to the east of the building. An access route will be available along the south side of Building A2 between it and Building A1 and another route along Maritime Street. This provides sufficient perimeter access to the building. The building will have two full fire-fighting shafts, which each will comprise, a fire-fighting stair having a clear width of 1150 mm, a fire-fighting lift, a mechanically ventilated fire-fighting lobby on all levels, an AOV at the head of the stair and dry fire mains having outlets on all levels and located within the fire-fighting lobbies. The building will be provided with a fire control centre (FCC) located at the ground level and accessed directly off the firefighter access route for northern fire fighting lift, to allow the fire-fighters to coordinate their operations.
285. The statement for Building A1 reviews the building construction and materials and

sets out principles to be applied in respect of means of escape, provisions to stop the spread of fire within the buildings and externally, passive and active safety provisions, ventilation, maintenance and access for emergency vehicles. The building will be designed to operate a phased evacuation strategy throughout (2 floors at a time). The building has been designed with 3 stairs, two firefighting and 1 protected. The protected stair serves ground floor to level 6. The two firefighting stairs serve all above ground floors (inclusive). The basement is served by two stairs which should be at least 1200mm wide and are accessed from within the firefighting stair, separated within the staircase at ground floor level by fire-resisting construction including an FD 30S self-closing door. The building is provided with two firefighting lifts, one in each of the two firefighting shafts. The main central lift bank has five passenger lifts and two passenger/evacuation lifts. As such, the number of evacuation lifts / evacuation provisions is considered appropriate from a life safety perspective, in providing means for management to assist in the evacuation of disabled users from the building. It is, therefore, considered that a reasonable provision has been provided.

286. Overall, the Fire Statements for both building are considered appropriate to satisfy London Plan policies requirements.

### **Impact of proposed development on amenity of adjoining occupiers and surrounding area**

287. The importance of protecting neighbouring amenity is set out Southwark Plan Policy P56 which states “Development should not be permitted when it causes an unacceptable loss of amenity to present or future occupiers or users”. The adopted 2015 Technical Update to the Residential Design Standards SPD 2011 expands on policy and sets out guidance for protecting amenity in relation to privacy, daylight and sunlight.
288. In terms of impact on adjacent residential occupiers, the closest existing residents are those at Porters Edge adjacent to Plot A2.
289. It should be noted that outline permission for the British Land Masterplan allows for residential or commercial use for Plots F, H and D which would be affected by the development.

### **Solar glare and light spill**

290. Solar glare implications were discussed as part of the EIA section earlier in this report.
291. A high level lighting strategy has been submitted. External lighting will need to strike a balance between being suitable for way finding and providing safety and security, whilst limiting harm to amenity and ecology from light spill. The overall strategy identifies a range of lighting Lux levels for different areas across the site according to their intended use. The strategy will comprise lighting integrated into the facades of buildings as well as within the landscape. Furthermore, the intelligent use of a site wide control system will ensure satisfactory lighting is always provided and that running and maintenance costs will be consistently reduced.
292. Condition 33 attached the OPP seeks to control detailed external lighting proposals in order to protect residential amenity and ecology.



## Outlook and privacy

293. In order to prevent harmful overlooking, the 2015 Technical Update to the Residential Design Standards SPD 2011 requires developments to achieve:
- A distance of 12 metres between windows on a highway-fronting elevation and those opposite at existing buildings, and;
  - A distance of 21 metres between windows on a rear elevation and those opposite at existing buildings
294. The impact in this respect was fully assessed and deemed to be acceptable at outline stage. Specifically; a distance of 17m would be retained between Porters Edge and the north elevation of Plot A2 separated by Maritime Street and a distance of 12.6m between the main building façade proposed for Building A1 and the lower rise element of Plot D on the BL Masterplan site separated by the planned pedestrianised route (Dock Edge Walk).
295. As part of the detailed design evolution for Building A1 and in order to address wind mitigation it is proposed to increase the maximum building parameter line on the southern façade 250mm further south, but with a corresponding reduction in the depth of the wind mitigation zone from 1m to 750mm so that the position of the outer edge of the wind mitigation zone remains the same as approved under the OPP. This amendment does not affect the other facades of Building A1. This minor change to the parameter plans was approved as part of the NMA 23/AP/0571.
296. As there is no amendment to the outer edge of the wind mitigation zone the distance between Building A1 and the closest adjoining building (Development Plot D in the Canada Water Masterplan) will remain the same as originally approved at Outline stage. When assessing the relationship between Building A1 and the adjacent Plot D a worst case scenario was considered (that being the minimum distance that would be retained between the two plots using the line of the wind mitigation zone and canopy projection for Plot A1 and the maximum building line as approved for Zone D). This relationship was deemed to be acceptable when OPP was granted in terms of daylight and sunlight impacts, potential for overlooking and the impact on the public realm between the two sites. At that time it was concluded that the relationship between A1 and the adjacent plot would not preclude the development of Zone D as allowed for within the approved British Land CW Masterplan parameters. That conclusion would not be affected by the NMA which has been granted or the detailed proposals as demonstrated in the RMA.

## Daylight and sunlight

297. The NPPF sets out guidance with regards to daylight/sunlight impact and states “when considering applications for housing, authorities should take a flexible approach in applying policies or guidance relating to daylight and sunlight, where they would otherwise inhibit making efficient use of a site”. The intention of this guidance is to ensure that a proportionate approach is taken to applying the BRE guidance in urban areas. London Plan Policy D6 sets out the policy position with regards to this matter and states “the design of development should provide sufficient daylight and sunlight to new and surrounding houses that is appropriate for its context”. Policy D9 (Tall Buildings) states that daylight and sunlight conditions around the building(s) and neighbourhood must be carefully considered. Southwark Plan Policies identify the

need to properly consider the impact of daylight/sunlight without being prescriptive about standards.

298. The Building Research Establishment guidance (updated in 2022) sets out the rationale for testing the daylight impacts of new development through various tests.
299. As the applications subject of this report relate to a commercial development only it is not necessary to carry out a technical assessment of the daylight levels that will be achieved within the building.
300. In terms of the impact upon neighbouring buildings the OPP included a full assessment of daylight, sunlight and overshadowing impact based on a maximum 3D envelope for each plot as created by the limitations of the parameter plans. Various scenarios were tested including the cumulative impact of building out all of the planned development on the Canada Water Dockside and British Land Canada Water Masterplan sites.
301. The impact on neighbours in this respect was deemed to be acceptable at the time of granting the OPP. This included an assessment of the impact upon the adjacent Decathlon site which contains residential dwellings and the planned development on the British Land Masterplan Plots. Given the assessment undertaken at outline stage and the fact that there has been no significant change in baseline conditions it would not be reasonable to reassess daylight impact as part of the RMAs.
302. There have been no significant changes to baseline conditions since approval of the OPP and the RMAs subject of this report do not give rise to any new or additional impacts in terms of daylight or sunlight impact upon neighbouring occupiers. As such it is not necessary or reasonable to reassess that impact as part of the RMA process.
303. The minor amendments to the position of the southern façade on Building A1 are not considered to be a significant change to the worst case scenario tested at outline stage as confirmed by the technical specialist responsible for preparing the ES Statement of Conformity (discussed above).

### Overshadowing of amenity spaces

304. As with the above daylight analysis the OPP assessed sunlight impacts arising from the Masterplan development on the basis of the maximum 3D envelope. This analysis included the potential impact on the Dock. It is not necessary or appropriate to re-visit that analysis as part of this RMA.

### **Noise and vibration**

305. London Plan Policy D14 and Southwark Plan Policy P56 require developments to manage the impacts of noise. Noise impact arising from the redevelopment as a whole was assessed in the ES submitted with the OPP and appropriate conditions were attached to prevent any harm arising in terms of plant, equipment and soundproofing as well as restricting operating hours for the commercial units, servicing hours and use of the terraces.
306. The council's environmental protection team have reviewed the application and have not raised an objection subject to the recommended conditions.

### Agent of change principles (ability for commercial and residential uses to co-exist)

307. London Plan Policy D13 requires all developments to consider ‘agent of change’ principles to ensure that where new developments are proposed close to existing noise-generating uses, they are designed in a more sensitive way to protect the new occupiers, such as residents and businesses from noise and other impacts. There are no residential uses proposed as part of this application. However, there are existing and planned residential uses within the immediate vicinity of the site.
308. The potential impacts arising from the redevelopment of the Canada Water Dockside site were duly considered and deemed to be acceptable at the time of granting OPP. Several mitigation measures have been incorporated into the conditions attached to the OPP to ensure that a variety of uses can exist side by side without giving rise to unacceptable impacts.
309. To conclude, it is considered that the OPP and this RMA has been designed to ensure that the technical considerations such as adequate servicing, ventilation, mitigation of noise and vibration have been robustly considered and secured so that the development is attractive and usable by the intended future occupiers in accordance with Policy D13.

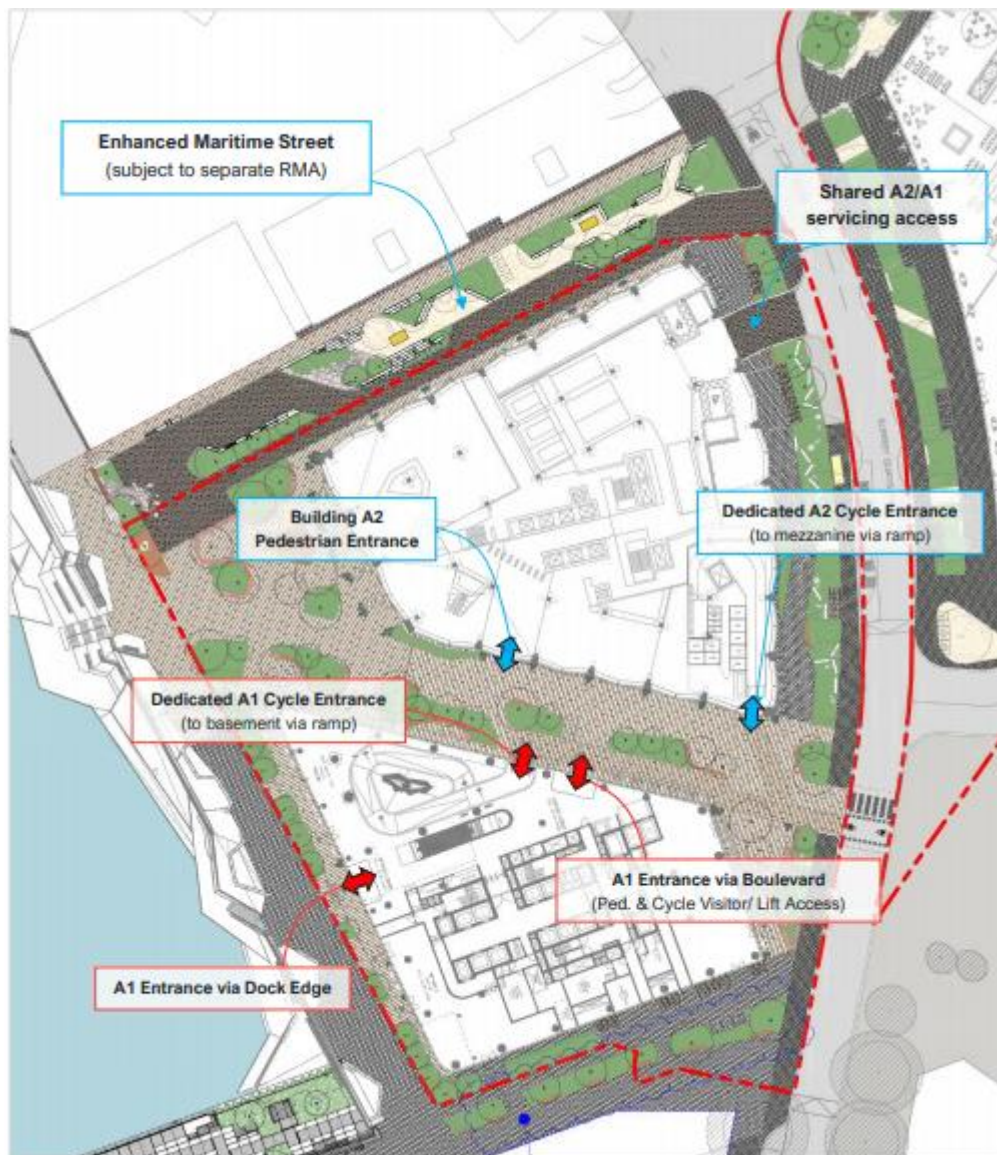
### **Transport and highways**

310. Chapter 9 of the NPPF seeks to ensure that transport issues are properly addressed as part of development proposals. Proposals must assess the impact upon existing transport networks, promote and maximise opportunities for sustainable transport modes whilst mitigating any adverse transport related environmental effects and must make a significant contribution to improving accessible movement and permeability as a key priority for place making. Paragraph 111 states “development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe”. This approach is reflected in Chapter 10 of the London Plan and Southwark Plan Policies P49 – P55, which require development proposals to maximise sustainable modes of transport by minimising car journeys, to deliver enhanced walking and cycling opportunities and safe, accessible routes to public transport. Developments should be car free save for disabled parking provision and mitigation will be secured where necessary to address impacts upon the road and public transport networks to serve new developments.
311. The OPP was subject to robust scrutiny of the transport impacts that may arise from the wholesale redevelopment of the CWD Masterplan site. The OPP secured a range of mitigation measures including substantial contributions to improve public transport infrastructure. The following transport mitigation was secured:
- Improvements to the Albion footbridge and Maritime Street
  - Provision of land and financial contribution towards making Printworks Street a two-way street
  - £2.3 million towards transport improvements to Lower Road
  - £9,317,000 towards strategic transport improvements (bus and train enhancements)

- £440,000 towards cycle hire docking stations
  - £107,000 towards bus infrastructure
  - £36,000 towards legible London signage
  - Management Plans for construction, delivery and servicing, estate management and travel plans
  - CPZ restrictions
  - 2 cycle hire docking stations onsite
312. The applications subject of this report were accompanied by Transport Statements Travel Plans and a Service and Delivery Plan specific to the proposed uses each building. The documents have been reviewed by the Council's Transport Policy and Highways Teams and TfL.

### **Site layout**

313. The development provides the opportunity to greatly improve the pedestrian and cycling environment, moving away from the current car-based and car parking dominated layout of the retail stores. In particular, the improved Maritime Street and 'Boulevard' would overcome the severance of the current layout, providing key pedestrian routes between the Canada Water Station and central quarter of the Canada Water Masterplan, and the existing and emerging residential developments.
314. The proposal has been designed to accommodate vehicle movements associated with servicing and deliveries, car parking for mobility impaired motorists, and access for emergency vehicles. Vehicular access to Buildings A1 and A2 is proposed in one location only. Specifically into the ground floor of Building A2 from Surrey Quays Road close to the junction with Maritime Street. The existing vehicle access points on Surrey Quays Road would be removed as these would no longer be required and would therefore be reinstated with footways to provide enhanced public realm.



**Image above:** Buildings A and A2 access overview

315. The proposed service arrangement for both buildings via the loading bay within the ground floor of A2 with a single point of access/egress accommodates all servicing requirements entirely 'within plot' and as approved as part of the OPP.
316. Emergency access for Buildings A1 and A2 would predominately be via Surrey Quays Road, with emergency access also proposed to be provided via Maritime Street and the new diagonal cut (The Boulevard). An emergency only access would also be provided to the south of Building A1.
317. In order to improve permeability through the site, a public cycle and pedestrian only route is proposed between the buildings (The boulevard) as well as enhanced pedestrian and cycle routes along Maritime Street and the Dock Edge.
318. The site layout accords with the OPP and is supported from a transport perspective.

### **Trip Generation**

319. Given the car-free nature of the proposals (apart from two Blue Badge parking spaces), the trips associated with the commercial uses will predominately be by



sustainable travel modes including on public transport, by bicycle and on foot.

320. The trip generation impact was robustly considered as part of the OPP. At the time of assessing the OPP it was clear from the modelling undertaken that the development would introduce a substantial increase in public transport trips over the current situation and extant residential permission for the site. During the 3-hour AM peak period (0700-1000), it was estimated that an additional 5,485 passengers would alight at the Canada Water station with an additional 1,270 passengers across the 3-hour PM peak period. Analysis of the rail passenger flows demonstrated an increase in crowding on both Jubilee Line and London Overground services in the AM and PM periods. It was not anticipated that there would be a significant impact on Bermondsey or Rotherhithe stations. It was envisaged that Surrey Quays Station will become a more attractive option for accessing this site and the wider town centre as a way of avoiding congestion at Canada Water Station (especially when improvements to the station are delivered).
321. In terms of bus services, it was forecast that the CWD development will attract approximately 366 additional alighters and 330 additional boarders at the bus stop located at Surrey Quays Road and Canada Street during the 3-hour AM and PM peak periods, respectively. Overall, the proposed CWD development would lead to an increase in bus demand in the local area. Based on the number of bus services serving the site (typically 34 services per peak hour in each direction). At OPP stage it was determined that this level of additional demand could be accommodated subject to a contribution towards bus improvements.
322. The OPP assessment and above conclusions were based on the entire masterplan (including Plot B). Updated Travel Plans were provided as part of the RMAs. The information submitted suggests that Building A1 is forecast to generate a total of 1,895 and 1,726 two-way person trips in the AM and PM peak hours, respectively, with the majority of these undertaken by public transport. Building A2 is forecast to generate a total of 1,098 and 1,000 two-way person trips in the AM and PM peak hours, respectively, with the majority of these undertaken by public transport.
323. The proposed quantum of development and land uses to be delivered within the RMA proposals accord with the assumptions made at Outline stage and therefore the trip impact assessment and associated mitigation package remains valid. The updated Travel Plans set out a range of measures to promote walking and cycling to help ease congestion on public transport and the development has been designed to promote and celebrate cycling as the main mode of transport.

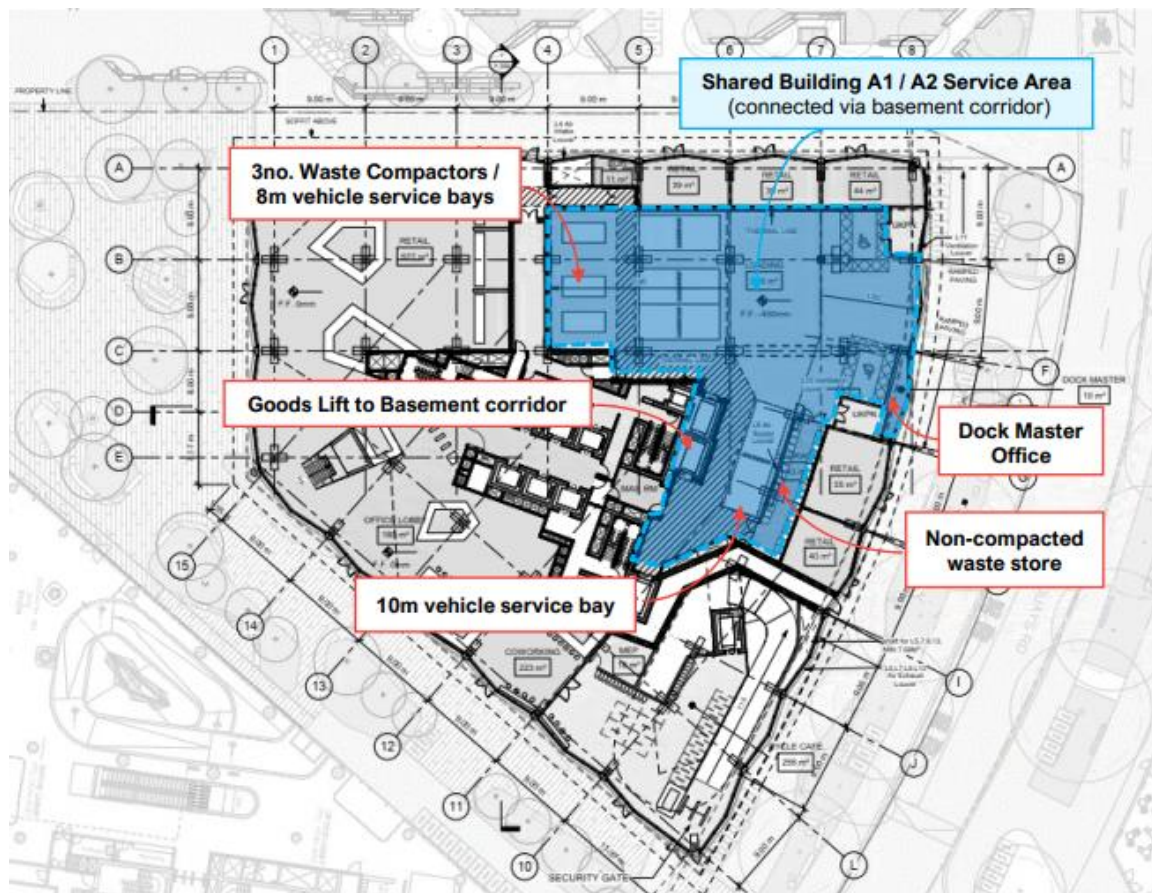
### **Pedestrian Comfort**

324. This Outline application included an assessment of Pedestrian Comfort Levels (PCL) in accordance with TfL Guidance. The assessment demonstrated that it is necessary to widen the existing Albion footbridge to provide comfort levels in line with TfL recommended PCL guidance in order to accommodate the additional capacity which will be generated by the proposal. These improvements have been secured in the s106 agreement and details will be submitted to the local planning authority in due course.

## Servicing and deliveries

325. The OPP included an approved site-wide Delivery and Servicing Management Plan. An updated Delivery and Service Management Plan (DSMP) has been produced to accord with the framework and principles set out in the approved site-wide Delivery and Servicing Management Plan.
326. Both Building A1 and A2 will have separate off-street servicing areas for delivery and servicing activity within the combined loading bay in Building A2. In order to minimise and manage the number of vehicle movements and control the vehicle size and type arriving at the site, the use of off-site consolidation of deliveries is proposed.
327. The servicing area provides a total of:
- 4no. vehicle servicing bays, comprising:
  - 3no. 8m vehicle bays (shared with portable waste compactors)
  - 1no. 10m vehicle bays
  - 2no. accessible Blue Badge parking bays

All of the parking and servicing bays will be fully equipped with electric vehicle charging facilities. It is noted that the service bays shared with the waste compactors would accommodate up to 8m vehicles for general servicing activity. The vehicle servicing area has been designed to allow all vehicles to enter and exit the site in a forward gear. Goods to Building A1 will be transferred via a basement corridor, with goods lifts provided in each building to allow the movement of goods between different levels.



**Image above:** Vehicle servicing area and layout

328. With a consolidation strategy in place, maximum daily deliveries are estimated at 111 vehicles per day (a management strategy will ensure that deliveries are avoided during the peak hour). A 12 hour delivery and servicing window is proposed working on 0700-2100hrs (excluding the typical morning and evening peak hour). This would mean an average of 9 vehicles per hour. The assumption is that delivery and servicing vehicles are only at the site for 15mins, which would mean capacity for 16 vehicles per hour. The applicant's analysis suggests a peak of 14 vehicles in an hour. Personal deliveries at work will be restricted through tenancy agreements. Servicing is restricted to 07:00 to 21:00 on Mondays to Saturdays and 09:00 to 18:00hrs on Sundays & Bank Holidays (with an additional exclusion of 08:00 – 09:00 and 17:00 – 18:00 to minimise peak hour traffic). This is already controlled via conditions attached to the OPP.
329. Deliveries by bicycle will be encouraged and made direct to the site. Any servicing timing restrictions do not apply to cargo bike deliveries. Cargo bikes will make use of the on-site post room located in the ground floor of Building A1 or the dedicated servicing area in Building A2, that offer facilities to enable for the safe and efficient use of cargo bikes within the site. Whilst the majority of vehicular servicing deliveries will be consolidated, a proportion will be 'direct to site' including local suppliers that are closer to the site than the off-site consolidation centre; and specific retail supplies where the double-handling of goods may not be acceptable, for instance fresh and perishable goods. These deliveries will be pre-booked within the delivery booking system accordingly.
330. Proposals for Maritime Street are that access would be managed by bollard-controlled access for any servicing, delivery or emergency service activity. Meaning that at all other times the area would serve a shared surface. Any servicing and delivery activity to be undertaken via a shared management scheme. Maritime Street predominantly provides servicing and delivery access for the Porters Edge development. The proposals for the site, following consultation with Notting Hill Genesis (NHG) propose 5no. LGV service bays to accommodate demands.
331. The application Transport Assessment does highlight that over a 0700-2200hrs time period 62 delivery and servicing activities were recorded. A maximum vehicle accumulation of 4 delivery and servicing vehicles was observed across the survey period – and this was only experienced for a total of 5 minutes. The transport Assessment shows vehicular access (with tracking) for all the proposed and servicing and refuse activity. As well as emergency fire tender access. The plans did highlight that for refuse activity and tracking – the two of the illustrated loading bays at the western end of Maritime Street would have to be kept clear of any vehicles. Therefore - the DSMP will need to highlight that management and dwell times of these two bays will need be strictly observed on refuse collection day to ensure clear refuse access and manoeuvre.
332. The delivery and servicing strategy accords with the principles established at Outline stage and is acceptable in its detail. A detailed DSMP will be submitted prior to occupation of the development as secured in the S106 agreement.

## **Refuse and waste management**

333. The applications were accompanied by a Waste Management Strategy. The strategy identifies likely volumes of waste that will be generated and required storage capacity. It is proposed that the commercial tenants, will as part of their fit-out provide suitably sized interim waste storage areas within their tenanted premises for the temporary storage of waste, mixed recyclables, glass and food waste (where relevant). On a regular basis, facilities management staff will transport waste from both A1 and A2 offices into the basement. Refuse storage and collection will be managed within the basement and taken to a collection point in the loading bay within Building A2. There is already a Condition (42) to control this attached to the OPP.

### **Car parking**

334. London Plan Policy T6 seeks to encourage car free and car limited development as much as possible and sets maximum car parking standards for different uses whilst recognising the need for an appropriate provision of disabled parking and adequate arrangements for servicing. Non-residential uses should provide a minimum of 1 disabled space. All car parking spaces must be fitted with electric vehicle charging points. Southwark Plan Policies P54 and P55 set out car parking standards for various land uses and echo the requirements of the London Plan in terms of setting maximum car parking standards and promoting car free development save for minimum disabled provision.
335. The development is proposed to be 'car free' with the exception of 2 accessible car parking spaces to be provided within the ground floor loading bay/service yard in Building A2. This provision is in line with the OPP.
336. The servicing area will be highly managed through the use of a vehicle booking system and a loading Dock Manager will be present at all times to manage/direct vehicles upon arrival. The Blue Badge parking bays will also be managed as part of this, with servicing vehicles movements held by the Dock Manager until the service area is cleared by the driver of the vehicle to prevent vehicle-pedestrian conflicts.
337. The service area will also provide demarcated pedestrian routes using surface hatching to maximise safe movements. In terms of routing from the A2 parking area to Building A1, a route via the A2 lobby has been identified as part of the design coordination process to minimise travel distance for users. The appropriate access clearance will be controlled as part of the Building Management strategy accordingly. An alternative route is also provided via the public realm, should users prefer.

### **Cycle parking and cycling facilities**

338. London Plan Policy T5 sets minimum cycle parking standards for different uses. Southwark Plan Policy P53 sets out a higher requirement than the London Plan standards. The OPP secures compliance with Southwark Plan standards.
339. For Building A1 a total of 1,210 long stay and 236 short stay spaces will be provided, of which 34 spaces will be sized for accessible/non-standard spaces. The cycle facilities will include secure storage, 948 lockers and 101 showers. The facilities are located within the 3 lower floors of the building. The provision is high quality and gives appropriate emphasis to cycling as a means of travel to work.
340. For Building A2 a total of 672 long stay and 80 short stay spaces will be provided, of

which 48 spaces will be sized for accessible/non-standard spaces. The cycle facilities will include secure storage, 530 lockers and 60 showers. The provision is high quality and gives appropriate emphasis to cycling as a means of travel to work.

341. Condition 34 attached to the OPP secures implementation and long term maintenance of cycle parking facilities.
342. The s106 for the OPP includes an obligation for 2 new cycle hire docking stations to be provided within the vicinity of the site.
343. The development would also benefit from Legible London signs as secured in the s106 agreement.

### **Healthy Streets**

344. London Plan Policy T2 requires development proposals to demonstrate how they will deliver improvements that support the ten Healthy Streets Indicators in line with Transport for London guidance. The development provides the opportunity to greatly improve the pedestrian and cycling environment, moving away from the current car-based and car parking dominated layout of the town centre.
345. This application is car free save for 2 disabled space thus promoting walking, cycling and use of public transport. Contributions have been secured under the OPP for sustainable transport modes to accommodate the demand created by future occupiers of the site. The scheme has been designed to enhance public realm around the site. The scheme has been designed to minimise air and noise pollution as much as possible.
346. Travel plans for Buildings A1 and A2 have submitted. The plans set out the measures that will be taken to maximum sustainable modes of transport for staff and visitors.

### **Transport Summary**

347. Overall the transport and traffic related implications have been fully considered. The Council's Highways and Transport Teams are satisfied with the proposal. The scheme minimises vehicle movements by prioritising use of public transport, walking and cycling, and by encouraging consolidation of deliveries. As such it conforms with the policies promoting sustainable travel and is consistent with the OPP. A range of improvements to public transport infrastructure, and to local streets, are important and necessary to mitigate the impacts of this large scale development. The necessary mitigation has already been secured as part of the OPP.

### **Environmental matters**

#### **Construction management**

348. Construction is proposed to be phased over the two sites. The phasing proposed at this stage is:-
  - Demolition of existing Buildings on Plot A
  - Construction of the basement for Buildings A1 and A2
  - Construction of Building A2
  - Construction of Building A1



Demolition and construction associated with this RMA is anticipated to take 3 years. With an anticipated commencement date in Q2 2024 and completion in Q2 2027

349. The construction related impacts of this development (including any impact on the ecology of the Dock) were considered as part of the ES submitted with the OPP. Schedule 8 of the s106 agreement (to which this RMA will be bound) secures the provision of detailed Demolition and Construction Management Plans for each development plot.
350. Subject to submission of a detailed CEMP being submitted at the appropriate time it is not anticipated that an unacceptable long terms impacts will arise as a result of the necessary construction process.

### **Water resources, flood risk and SUDs**

351. Policy SI 12 of the London Plan 2021 states that development proposals should ensure that flood risk is minimised and mitigated and natural flood management methods should be employed in development proposals due to their multiple benefits including flood storage and creating recreational areas and habitat. Policy SI 13, Sustainable drainage reinforces this and states that development proposals should ensure that surface water run-off is managed as close to its source as possible. Drainage should be designed and implemented in ways that promote multiple benefits including increased water use efficiency, improved water quality and enhanced biodiversity, urban greening, amenity and recreation.
352. Policy P68, Reducing flood risk, of the Southwark Plan 2022 states that development must not increase flood risk on or off site and champions the use of water sensitive urban design and Sustainable Urban Drainage Systems (SUDs). The rate of surface run-off (and so the related flood risk) can be significantly reduced through the careful design of developments and the inclusion of Sustainable Urban Drainage Systems (SUDs). This policy and the use of SUDs is also reflected in current Southwark policies.
353. The application site is located within Flood Zone 3, with a high risk of tidal flooding but benefitting from the Thames Tidal defences. The OPP was accompanied by a Flood Risk Assessment (FRA). An updated FRA has been submitted with the RMAs.
354. The updated FRA confirms that an assessment of groundwater levels indicates the risk of groundwater flooding to be high, particularly when considering the proposals for development at basement level. This will require a number of mitigation measures such as a waterproofing strategy, raising of thresholds at basement entrances, provision for emergency egress and suitable flood resilient/resistant construction materials and techniques. The full details of each mitigation measure will be determined as the detailed construction design evolves.
355. The landscape proposals include SUDs features such as bio-retention rain gardens and tree pit storage systems.
356. Both buildings A1 and A2 will have a greywater harvesting tank and infrastructure, sized to achieve significant on-site water reuse. This will be used for toilet flushing and irrigation. Low flow fixtures and flow rates in line with BREEAM requirements are also proposed.

357. Condition 17 attached the OPP secures the submission and implementation of a sustainable drainage strategy across the site. The s106 agreement secures a maximum Greenfield Run-off rate of no more than 2.2 litres per second (taking into account 40% climate change allowance). If the Developer cannot meet this then a financial contribution will be required to mitigate the impact.
358. Two drainage options have been proposed for the Plot. The preference is to discharge via gravity, unrestricted into the dock utilising an outfall through the dock edge wall. A further option to restrict the amount of water into the Dock by providing a below ground attenuation tank has been proposed. In both options the principles of the proposed surface water strategy as follows:
- Surface water run-off from each building roof and open terraces will be attenuated at roof level by blue roofs (a roof system specifically designed to store water), through the incorporation of geo-cellular crates or similar landscaping build-up. These systems will be inclusive of a flow control device on the outlets from the roof. The building roof systems will be conveyed via new surface water pipes under gravity into the respective drainage systems.
  - The surface water runoff from the hardstanding and adjacent soft landscape areas will be incorporated for treatment and source control via tree pits. An overflow gully will be incorporated within the soft landscaping to prevent water ponding and flooding adjacent areas.
  - Flow controls will be installed into the proposed manholes upstream of the discharge outfalls to limit the surface water discharge rates where required.

The attenuation tank is not the preferred option from the applicant due to the carbon emissions associated with the construction of the tank.

359. Maritime Street, located along the northern boundary of A1 and A2, is integral to the wider SuDS features proposed for the site. Rain gardens and tree pits are proposed along the length of Maritime Street as means of surface water source control and improving bioretention. Furthermore, permeable paving is proposed in sections along Maritime Street aiding surface water entering the below ground drainage network.
360. The strategy to discharge into the Dock formed part of the Outline strategy and has been considered by Drainage and Ecology Officers and confirmed to be acceptable in principle subject to the applicant demonstrating that they have maximised onsite SUDs and cleaned any water that will eventually discharge into the Dock. However, in order to discharge into the Dock, permission will be required from British Land as they are responsible for managing the Dock. Should such consent not be granted by BL then the applicant will be required to connect to the sewer. They have demonstrated that there is sufficient capacity for this. There is a detailed drainage condition attached to the OPP, as part of this condition the applicant will be required to demonstrate that any water proposed to be discharged into the Dock meets CIRIA Suds standards or in the event that discharge to the sewer is required the strategy to be submitted under Condition 17 will need to fully demonstrate compliance with maximising SUDs onsite and meeting policy restricted runoff rates

## **Land Contamination**

361. A desk top ground investigation assessment report was submitted at OPP stage and appropriate conditions requiring further intrusive surveys, remediation and verification

have been attached to the OPP (Condition 16).

## **Air quality**

362. A key priority for the London Plan is to tackle poor air quality (Policy GG3 and SI 1). This is reinforced in Southwark Plan Policy P65 which seeks to ensure that developments achieve or exceed air quality neutral standards; and address the impacts of poor air quality on building occupiers and public realm users by reducing exposure to and mitigating the effects of poor air quality.
363. At OPP stage air quality was addressed within Chapter 9 the ES. The OPP s106 agreement to which this RMA will be bound secures necessary mitigation measures during demolition and construction works. The operation of the proposed development is not predicted to result in any significant effects on air quality and the air quality for future users of the development would also be acceptable.
364. The council's environmental protection team has reviewed the submission and advised that there is no objection to the proposal.

## **Energy and sustainability**

365. Chapter 9 of the London Plan deals with all aspects of sustainable infrastructure and identifies the reduction of carbon emissions as a key priority. Policy SI2 requires all developments to be net zero carbon with a minimum onsite reduction of 35% for both commercial and residential. Non-residential development should achieve 15 per cent reduction through energy efficiency measures.
366. Southwark Plan Policies P69 and P70 reflect the approach of the London Plan by seeking to ensure that non-residential developments achieve a BREEAM rating of 'Excellent' and include measures to reduce the effects of overheating using the cooling hierarchy. The policies reflect the London Plan approach of 'lean, green and clean' principles. London Plan Policy SI2 'Minimising Greenhouse Gas Emissions' requires all major development to be net zero carbon with a minimum on-site reduction of 35% against the Part L 2013 baseline for residential and non-residential uses. Southwark Plan Policy P70 'Energy' also requires major non-residential development to be net zero carbon, but with a minimum on-site reduction of at least 40% against the Part L 2013 baseline. As Policy P70 'Energy' is more recently adopted than London Plan Policy SI2, the 40% minimum onsite reduction is therefore required for major non-residential development within the borough.
367. Where a development cannot reduce its operational carbon emissions to zero, any residual carbon emissions must be offset to meet the net zero target. This is achieved by way of a financial contribution towards the 'Green Buildings Fund', Southwark's Carbon Offset Fund.
368. Part L Building Regulations 'Conservation of Heat and Power' have now been updated from Part L 2013 to Part L 2021. This update results in the baseline performance of new development improving by ~27% for non-residential development. In practice, this means to meet this regulation that buildings must now be built to use less energy and heat that results in less carbon emissions being emitted through their operation. New development must achieve further carbon emission reduction over a higher part L baseline to meet planning policy compliance

with London Plan Policy SI2 'Minimising Greenhouse Gas Emissions' and Southwark Plan Policy P70 'Energy'.

369. Following the resolution of Part L software modelling issues in December 2022, The GLA has updated its Energy Assessment Guidance 2022 to confirm that all new major planning applications submitted from 1 January 2023 should now be assessed against Part L 2021 when assessing policy compliance for SI2. All major development planning applications that were submitted before 1 January 2023 will continue to be assessed and determined using Part L 2013. The developments subject of the RMAs set out in this report will be required to comply with Part L 2021.
370. The energy strategy for new developments must follow the London Plan Hierarchy (be lean/ be clean/ be green/be seen) and this must be demonstrated through the submission of an Energy Strategy with applications and post construction monitoring for a period of 5 years.
371. A site wide approach to energy and carbon emission reductions was approved as part of the OPP. Schedule 9 of the s106 agreement (to which this RMA will be bound) sets out the necessary obligations for each RMA. The following obligations have been secured:-
- Submission of a detailed energy strategy prior to implementation of any Plot
  - Each building would need to achieve a carbon saving of minimum 51% on site utilising ASHP and PVs
  - Necessary carbon offset contributions for each Plot must be calculated according to calculations in place at the time of submitting the Energy Strategy
  - Necessary carbon contributions to be paid prior to Implementation of any Plot
  - 5 year monitoring reports to be submitted post construction
  - Future proofed connection to a District Heat Network
372. The applications subject of this report were accompanied by Energy and Sustainability Plans as well as Whole Life Carbon Assessment and Circular Economy Statement to address current policy requirements (discussed further below).

### **Whole life cycle and carbon capture**

373. A Whole Life Carbon (WLC) Assessment was submitted for each building. The assessments analyse both the embodied carbon of materials and the operational carbon due to the energy and water use. The assessments identify options for reducing embodied carbon, identifying targets to be met at practical completion and life cycle stage. Both assessments were subject to independent third party review and have been scrutinised by officers in the planning policy team.
374. There is already a post construction assessment for WLC secured by conditions attached to the OPP.

## Carbon emission reduction

375. GLA carbon emissions spreadsheets have been submitted using both the Part L 2013 and 2021 baselines. This level of detail has been submitted to demonstrate the impact of the updated Part L baseline conditions.
376. The information submitted demonstrates that the baseline regulated carbon emissions are significantly lower under the 2021 regulations (so greater reductions are being achieved at the outset) and whilst the carbon emission savings achieved above the baseline is much lower in percentage terms (using the 2021 baseline) this is as a result in the change to methodology and requirements as part of the baseline conditions and does not mean there has been a reduction in the design or performance of the buildings. The information submitted demonstrates that the development would meet the requirements of the OPP and would exceed policy requirements using the 2013 baseline.
377. Building A1 will achieve a building energy performance of 53% carbon emission reduction over 2013 Part L of the Building Regulations. Building A2 will achieve a building energy performance of 51% carbon emission reduction over 2013 Part L of the Building Regulations This exceeds the 40% requirement of the Southwark Plan and 35% of the new London Plan and would also meet the OPP target.
378. When assessed against 2021 Part L Building A1 would achieve a 13% reduction and Building A2 a 17% reduction. The main reasons for this are because the new methodology incorporates Air Source Heat Pumps into the baseline and because of the way the modelling for the notional building is designed, it is harder to achieve 'be lean' savings and easier to deliver 'be green' savings. It is not possible to achieve savings under 'be clean' until it becomes possible to connect to a District Heat Network.
379. The shortfall between 40% and zero carbon will be met by way of a carbon offset payment which would accord with current adopted policies and the OPP. To this end the following payments would be payable using the 2021 Part L baseline
- Building A1 £564,585 (198.1 tonnes x 30 x £95)
  - Building A2 £248,805 (87.3 tonnes x 30 x £95)

### Be Lean (use less energy)

380. The proposed development for Building A1 will incorporate the following passive design features:
- A window G-value of 0.28 in the office areas to reduce overheating risk and reliance on mechanical cooling
  - The use of exposed concrete slabs internally provides high thermal mass to moderate the cooling loads particularly with the potential for night cooling offered by openings in the facade
  - Low air permeability reduces leakage through the façade
  - High levels of envelope insulation to reduce energy demand
  - Provision of the potential for natural ventilation through the year
  - Optimised glazing ratio to reduce solar gains whilst ensuring access to daylight.



- Energy efficient lighting and lifts
381. The proposed development for Building A2 will incorporate the following passive design features:
- A window G-value of less than 0.3 in the office areas to reduce overheating risk and reliance on mechanical cooling
  - Optimising the solid-glazing ratio to both reduce excessive solar gains and the need for cooling and encourage the benefits of daylighting.
  - High performance opaque envelope elements.
  - Efficient double-glazed windows.
  - Improving airtightness.
  - Optimising thermal insulation for opaque elements, minimising heat loss.
  - Mitigating thermal bridging by detailing wherever possible to limit heat loss at initial stage.
  - Including openable windows to enable effective mixed-mode natural ventilation and passive cooling.
  - Low Emissivity coated glass and low G value to reduce unwanted solar gains.
  - Passive shading through protrusions in façade design.
  - Demand control ventilation.
382. Façade design can have a large impact on whole building energy consumption by controlling the flows of energy in and out of the building. For both Buildings A1 and A2 solar analysis was conducted to understand the Proposed Development's solar exposure. This was then used to determine the glazing ratios and shading requirements across and up the buildings, and on each facade face.
383. For both Buildings A1 and A2 this enabled the facade treatment to be more glazed on the North façades to maximise daylight availability. On the South, West and East façades the articulation of the solid facade areas, reveal depths and shading fins reflect the need to limit solar gain in some areas more than others. This initial solar analysis and facade articulation was then used to develop the energy model to understand the energy performance. Alongside this solar study daylight analysis was also undertaken to understand the balance between limiting solar gain and maximising daylight.
384. Under the 2013 Part L baseline the implementation of these measures would reduce regulated CO<sub>2</sub> emissions by 26% for buildings A1 and 23% for Building A2 thus meeting London Plan targets for energy reduction. It should be noted that under the 2021 Part L baseline the 'be lean' measures equate to 0% reduction due to the change in methodology.

### Be Clean (supply energy efficiently)

385. The possibility of employing a decentralised energy network was investigated at OPP stage and again at the point of preparation of the RMAs for Buildings A1 and A2. Currently there is no district heating network available. However, a plan is under development and a new network might become available in the coming years. The development has been designed to allow future connection to a district heating network should one become available. Locations for intake rooms have been identified on the ground floor for each building, which provides direct access from outside and has a clear route for below-ground pipework to enter the buildings.

Ongoing review of the possibility to connect is secured in the s106 agreement.

### Be Green (Use low or carbon zero energy)

386. For building A1 and all electric energy system is proposed comprising installation of 444 sqm of PV panels and ASHP for heating and cooling. The implementation of these measures would reduce regulated CO2 emissions by 26% using the 2013 baseline and 13% using the 2021 baseline. The reduction in percentage under 2021 baseline is due to the fact that ASHP contribute to the baseline savings.
387. An all-electric building with roof mounted PVs and heating and cooling systems serviced by hybrid air source heat pumps and water cooled chillers with high seasonal efficiencies have been proposed for Building A2. Two ASHPs will be provided on the roof level plant area with all associated equipment in the basement plant area. Further to this, cooling towers will provide hydraulic free cooling capabilities. To deliver fresh air, demand control ventilation systems with low SFPs and heat recovery will be used. The implementation of these measures would reduce regulated CO2 emissions by 28% using the 2013 baseline and 17% using the 2021 baseline. The reduction in percentage under 2021 baseline is due to the fact that ASHP contribute to the baseline savings.

### Be Seen (Monitor and review)

388. The London Plan asks developers to monitor energy use during the occupation and to incorporate monitoring equipment to enable occupants to monitor and reduce their energy use. 5 years post completion monitoring has been secured in the OPP s106 agreement to which this RMA will be bound.

### **Circular economy**

389. A Circular Economy is defined as one where materials are retained in use at their highest value for as long as possible and are then reused or recycled, leaving a minimum of residual waste. London Plan Policy SI7 seeks to promote resource conservation, waste reduction, increases in material re-use and recycling, and reductions in waste going for disposal through the requirement of new development to submit a circular economy statement. Such statements must demonstrate how all materials arising from demolition and remediation works will be re-used and/or recycled; how the proposal's design and construction will reduce material demands and enable building materials, components and products to be disassembled and re-used at the end of their useful life; opportunities for managing as much waste as possible on site; adequate and easily accessible storage space and collection systems to support recycling and re-use; specify how much waste the proposal is expected to generate, and how and where the waste will be managed in accordance with the waste hierarchy. The statement must also identify how performance will be monitored and reported.
390. A site wide pre-demolition audit was undertaken at the outline application stage. It has been re-reviewed the RMA submissions and there have been no changes. Deconstruction of the existing buildings will be carefully carried out, and any elements or materials that are deemed not feasible to be reused in the new development will be appropriately recycled or re-used off-site where possible.

Circular Economy Statements for both buildings have been submitted in line with the GLA's requirements. The statements propose the following measures:-

- At least 95% of recyclable construction, demolition and excavation waste is targeted to be reused or recycled.
  - There will be an exploration of material reuse between the wider British Land masterplan and the proposed masterplan.
  - During construction, phased areas of site will be used to store materials for reuse
  - A project-specific material life cycle carbon analysis has been carried out to inform the design and specifications.
  - Reuse, recycling and choice of materials with low embodied carbon will be prioritised.
  - All timber and timber products will to be sourced from suppliers accredited under Forest Stewardship Council (FSC) or Programme for the Endorsement of Forestry Certification (PEFC).
  - Steel with at least 20% recycled content is to be procured, with an ambition for maximising recycled content in steel pending market availability and feasibility.
  - Recycled and local materials with third party verified sound environmental credentials and environmental product declarations will be prioritised.
  - Pre-fabricated products with standard dimensions will be prioritised to avoid manufacturing and construction waste, while also improving maintenance and reuse.
  - Robust design principles will be adopted where damage risk is greater due to high usage.
  - Material efficiency measures will continue to be monitored at each design stage through collaborative workshops attended by all stakeholders.
  - Non-hazardous construction waste will be reduced to achieve at least 1 credit under BREEAM Wst01 by diverting unavoidable demolition and construction waste to reuse and recycling.
  - Construction energy and water use will be monitored.
  - Installation of speculative interior finishes, particularly ceilings, will be avoided where practicable.
  - Adequate dedicated storage space for recyclable waste will be provided.
  - Operational energy and water use will be monitored through extensive sub-metering to encourage economy in use and enable corrective action.
  - Stamping of grade and size of steel members will be considered to facilitate their future reuse.
  - Bolted structural connections that enable disassembly will be prioritised over welded connections where this is structurally and technically feasible without incurring material inefficiency
391. The proposed commitments to minimise the quantities of materials and other resources (energy, land, water) used, and measures for sourcing materials responsibly and sustainably are considered to be acceptable in principle. Compliance with the CES has been secured by Conditions attached to the OPP.

### **Cooling and Overheating**

392. London Plan SI4 requires major development proposals to demonstrate through an energy strategy how they will reduce the potential for internal overheating and reliance on air conditioning systems in accordance with the London Plan cooling

hierarchy. The Energy Strategies submitted demonstrate that through passive design measures (energy efficient lighting and appliances, hot water supplied by ASHP, using façade treatment to balance solar gains, providing high ceilings and well ventilated spaces, underfloor air supply and openable windows) it will be possible to minimise the need for cooling. Additional mechanical ventilation will be used to reduce cooling demand and when required during peak periods comfort cooling will be provided via ASHP and after-cooled chillers.

## **BREEAM**

393. Southwark Plan Policies P69 requires the development to achieve BREEAM 'excellent'. A BREEAM Pre-assessment report has been undertaken for both Buildings A1 and A2 which confirms that the development is on target to achieve a BREEAM Outstanding rating for the office spaces and a BREEAM Excellent rating for the retail spaces. A condition has already been attached to the OPP to secure compliance with BREEAM standards.

## **Health Impact Assessment**

394. The OPP was accompanied by a Health Impact Assessment (sitting outside of the Socio-economic chapter of the ES) that sets out a range of features to be included in the development which aim to promote health and wellbeing for future users. The design of the scheme has also been assessed in the context of the WELL Community Standard. This standard focuses on ten concepts to support the development of health-focused, integrated and supported communities. The scheme is targeting a Gold Standard. The report concludes that overall, the Proposed Development is likely to have a positive effect on workplace health as set out above.
395. The development will enhance access to open space and nature by virtue of the public realm to be delivered within the site as well as enhanced pedestrian and cycle routes through the site.
396. Air quality and noise impact, economic and employment benefits were fully assessed as part of the OPP.
397. The OPP allows for the provision of up to 3,000 sqm of medical/healthcare floorspace (Use Class E(e)), and up to 750 sqm of learning and non-residential institutions and/or community floorspace (Use Class F1 and F2). These uses are not proposed for Buildings A1 or A2 so would remain to be provided in Plot B at a later date.

## **Digital Connectivity**

398. London Plan Policy SI6 introduces the need for new developments to address London's requirements for enhanced digital connectivity. The policy requires development proposals to ensure that sufficient ducting space for full fibre connectivity infrastructure is provided to all end users, to meet expected demand for mobile connectivity generated by the development, to take appropriate measures to avoid reducing mobile connectivity in surrounding areas; and to support the effective use of rooftops and the public realm (such as street furniture and bins) to accommodate well-designed and suitably located mobile digital infrastructure.

399. In order to address this policy requirement Condition 12 of the OPP secures full fibre connection to the site.
400. In terms of impact on existing infrastructure in the area, this is addressed as part of Schedule 10 of the s106 agreement.

### Summary

401. In conclusion, the documents submitted with this RMA reflect the principles established by the OPP and meet the requirements of the s106 agreement and current development plan policies in respect of climate change.

### **Planning obligations (S.106 agreement)**

402. London Plan Policy Df1 and Southwark Plan Policy IP3 advise that planning obligations can be secured to overcome the negative impacts of a generally acceptable proposal.
403. This application is bound by the s106 obligations secured in the legal agreement attached to 21/AP/2655 and 21/AP/2610.
404. The following obligations were secured at Outline stage:-
- Affordable workspace (10% of GIA to be affordable, 25% discount on rent values, service charge capped, fit out specified and 6 months rent free)
  - Onsite public realm with minimum quantum's specified (including toilet and water fountain provision)
  - Delivery of Dock Edge enhancements
  - Improvements to the Albion footbridge and Maritime Street
  - Provision of land and financial contribution towards making Printworks Street a two-way street
  - Community space to be provided onsite (min 300sqm in Plot B)
  - Heath hub provision onsite (Plot B – Reasonable endeavours)
  - Policy compliant Employment and training opportunities
  - Management Plans for construction, delivery and servicing, estate management and travel plans
  - Carbon green fund contribution (to offset as required following updated Energy strategies).
  - Future proofed connections to a DHN
  - Controlled greenfield run-off rates or offset payment
  - CPZ restrictions
  - Full fibre technology installed onsite and provision to rectify any issues created with nearby receptors
  - 2 cycle hire docking stations onsite
  - £150,000 towards environmental protection monitoring during construction
  - £2.3 million towards transport improvements to Lower Road
  - £9,317,000 towards strategic transport improvements (bus and train enhancements)
  - £440,000 towards cycle hire docking stations
  - £107,000 towards bus infrastructure
  - £36,000 towards legible London signage



- £11,171 archaeology contribution
  - £25,000 towards ecology improvements and monitoring
  - £8,000 or CAVAT Value for any trees removed as a result of the proposal (applies to trees shown as being retained in the OPP)
405. There is no requirement for additional mitigation beyond that secured at Outline stage.

### **Mayoral and borough community infrastructure levy (CIL)**

406. Section 143 of the Localism Act states that any financial contribution received as community infrastructure levy (CIL) is a material 'local financial consideration' in planning decisions. The requirement for payment of the Mayoral or Southwark CIL is therefore a material consideration. However, the weight attached is determined by the decision maker. The Mayoral CIL is required to contribute towards strategic transport invests in London as a whole, primarily Crossrail. Southwark's CIL will provide for infrastructure that supports growth in Southwark.
407. The site is located within Southwark CIL Zone 2, and MCIL Band 2 Zone. Based on information obtained from the application form dated 20-Feb-23, the gross amount of CIL is approximately £6, 511, 197.50. It should be noted that this is an estimate, floor areas will be checked when related CIL Assumption of Liability form is submitted, after planning approval has been secured.

### **Other matters**

408. Archaeology was fully considered as part of the OPP and appropriate conditions and s106 obligations have been secured.

### **Community involvement and engagement**

409. The application was accompanied by a Statement of Community Involvement and Engagement Summary from the Development Consultation Charter. The documents confirm that pre application engagement was undertaken as a combined exercise for:
- The A1 and A2 buildings (including basements)
  - Maritime Street and
  - The separate full planning application for the Dock Edge Scheme.

The following engagement was undertaken:

- 10 x Key Stakeholders contacted at every stage of consultation (Ward Cllrs, Cabinet Members, Rotherhithe Area Housing Forum, Canada Water Consultative Forum, Green Connections 106, Surrey Docks Angling Club and Our Rotherhithe)
- 7 x Key Stakeholder Meetings held
- 10 x Public Exhibition Sessions at the permanent exhibition space inside Decathlon Surrey Quays
- 1 x Public Forum
- 1,897 x Website Users – [www.canadawaterdockside.co.uk](http://www.canadawaterdockside.co.uk)
- 385 x Email Subscribers
- 25 local resident responses (online and postal)
- 39,000 x Newsletters distributed in the local area

- 7 x E-Newsletters issued to subscribers
  - 3,853 x impressions recorded across Social Media Advertisement on Twitter
  - To tackle the consultation deficit for those that are unable to access the internet, the applicant provided a phone line and postal address for residents to utilise.
410. The Statement of Community Involvement includes details of the public forum questions that arose, questions used on the feedback forms and copies of the newsletters issued as well as a summary of feedback from the received from each stage of the consultation. It is considered that the applicant has sought to actively engage with the local community throughout the process so far and they have stated a commitment to continue engagement beyond the planning stage.
411. The Development Consultation Charter confirms that the site was acquired by the applicant in 2020 when it consisted of underutilised car parking and warehouse buildings. As meanwhile uses the following occupiers have been on site since 2020
- Construction Skills Centre at the former-Hawker House site
  - Bow Arts Trust
  - Fully immersive open world adventure, Phantom Peak
  - Venue Labs

The document sets out details of the demographics of the area as well as any site constraints. Alfred Salter Primary School is the closest school to the site, located at the corner of Quebec Way and Canada Street. The applicant confirmed that they consulted Alfred Salter School throughout the pre application stages of the Outline Planning Application, and updates have been provided during the consultation on the Reserved Matters Applications and Dock Edge scheme.

The document further summarises the approach to tackling climate change as part of the detailed proposal.

412. As part of its statutory requirements, the Local Planning Authority sent letters to local residents, issued a press notice publicising the planning application, put up site notices and advertised the application on the website. Adequate efforts have, therefore, been made to ensure the community has been given the opportunity to participate in the planning process. Full details of consultation undertaken by the Local Planning Authority are set out in Appendix 4. The responses received are summarised at the start of this report

### **Consultation responses from internal external and statutory consultees**

413. **London Fire Service (all RMAs):** The London Fire Brigade (LFB) has been consulted with regard to the above-mentioned premises and have no further observations to make.

**Officer Response:** Noted

414. **Ecologist (all RMAs):** The UGF score is fine and the mix of habitats and species provide ecological interest Biodiversity Net Gain. The BNG calculation exceeds minimum requirements.

415. **Local Economy (all RMAs):** The proposed affordable workspace allocations are acceptable.

In respect of Buildings A1

- This part of the Masterplan would be expected to deliver 121 sustained jobs to unemployed Southwark residents, 121 short courses, and take on 30 construction industry apprentices during the construction phase, or meet the Employment and Training Contribution.
- The maximum Employment and Training Contribution is £583,450.00 (£520,300 against sustained jobs, £18,150 against short courses, and £45,000 against construction industry apprenticeships).

In respect of Building A2

- This part of the Masterplan would be expected to deliver 70 sustained jobs to unemployed Southwark residents, 70 short courses, and take on 17 construction industry apprentices during the construction phase, or meet the Employment and Training Contribution.
- The maximum Employment and Training Contribution is £337,000 (£301,000 against sustained jobs, £10,150 against short courses, and £25,500 against construction industry apprenticeships).

In respect of the Basement

- This part of the Masterplan would be expected to deliver 11 sustained jobs to unemployed Southwark residents, 11 short courses, and take on 2 construction industry apprentices during the construction phase, or meet the Employment and Training Contribution.
- The maximum Employment and Training Contribution is £51,950 (£47,300 against sustained jobs, £1650 against short courses, and £3000 against construction industry apprenticeships).
- An employment, skills and business support plan should be included in the S106 obligations.

End use of the development jobs / requirements

- A1 - This part of the Masterplan with the proposed employment densities would be expected to deliver 375 sustained jobs for unemployed Southwark Residents at the end phase, or meet any shortfall through the Employment in the End Use Shortfall Contribution. The maximum Employment in the End Use Shortfall Contribution is £1612500 (based on £4300 per job).
- A2 - This part of the Masterplan with the proposed employment densities would be expected to deliver 217 sustained jobs for unemployed Southwark Residents at the end phase, or meet any shortfall through the Employment in the End Use Shortfall Contribution. The maximum Employment in the End Use Shortfall Contribution is £933,100.00 (based on £4300 per job).

**Officer comment:** the above obligations are already secured in Schedule 7 of the s106 agreement to which the RMAs will be bound.

416. **Environmental Protection (all RMAs):** In respect of air quality, the Air Quality Assessment finds that proposed emergency generators will not lead to exceedance of annual average NO<sub>2</sub> objectives. In respect of 1 hour NO<sub>2</sub> objectives, the generators may lead to exceedance of the 200µg limit value however the generators

are limited to full power testing for a maximum of 5 hours per year which makes exceedance of the air quality objective (200µg not to be exceeded more than 18 times) very unlikely. The affected receptors are not in locations where there is likely to be any significant cumulative impact with other sources, such as from a busy road. Overall the assessment finds the impact is negligible and we agree that the impact is not significant. EPT therefore do not object to the proposed location of emergency generators.

The submitted ES statement of conformity states that the development impacts are in accordance with the original ES for all matters of relevance to EPT, with the exception of Air Quality. Conditions are placed on the outline consent to cover various environmental issues. In light of this EPT do not have any objections to the other RMA details submitted.

- 417. **Highways (all RMAs):** No objection – detailed comments provided at Outline stage.
- 418. **Transport Policy (all RMAs – summary):** No objections, the proposals accords with the OPP.
- 419. **Waste (all RMAs):** No comment given commercial nature of proposals.
- 420. **Transport for London (all RMAs):** The applications are for approval of details of a previously consented development with a signed s106 agreement to which TfL had extensive input, and there do not appear to be any new strategic transport impacts or considerations arising, so no further comments.
- 421. **London Underground (all RMAs):** No comment to make
- 422. **GLA:** Do not wish to comment on the Reserved Matters Applications
- 423. **Civil Aviation Authority:** No response
- 424. **London City Airport (all RMAs):** No safeguarding objections to the proposed development.
- 425. **London Borough of Lewisham:** No response
- 426. **London Borough of Tower Hamlets (all RMAs):** No comment
- 427. **Environment Agency (all RMAs):** Do not wish to comments on Reserved matters Applications. Conditions attached to the Outline Permission should be applied.  
  
**Officer comment:** This application will be bound by the conditions attached to the OPP.
- 428. **Historic England (all RMAs):** No objection refer to local guidance.
- 429. **Secure by Design (all RMAs):** I can confirm that the design team have been in contact regarding this project and that engagement has been positive. There is a SBD planning condition for the initial approved plans so consultation with the design team will continue as this development progresses.

**Officer comment:** There is already a relevant condition attached to the OPP.

430. **Thames Water (all RMAs):** Do not wish to comments at RMA stage.

### **Community impact and equalities assessment**

431. The council must not act in a way which is incompatible with rights contained within the European Convention of Human Rights

432. The council has given due regard to the above needs and rights where relevant or engaged throughout the course of determining this application.

433. The Public Sector Equality Duty (PSED) contained in Section 149 (1) of the Equality Act 2010 imposes a duty on public authorities to have, in the exercise of their functions, due regard to three "needs" which are central to the aims of the Act:

1. The need to eliminate discrimination, harassment, victimisation and any other conduct prohibited by the Act
2. The need to advance equality of opportunity between persons sharing a relevant protected characteristic and persons who do not share it. This involves having due regard to the need to:
  - Remove or minimise disadvantages suffered by persons who share a relevant protected characteristic that are connected to that characteristic
  - Take steps to meet the needs of persons who share a relevant protected characteristic that are different from the needs of persons who do not share it
  - Encourage persons who share a relevant protected characteristic to participate in public life or in any other activity in which participation by such persons is disproportionately low
3. The need to foster good relations between persons who share a relevant protected characteristic and those who do not share it. This involves having due regard, in particular, to the need to tackle prejudice and promote understanding.

434. The protected characteristics are: race, age, gender reassignment, pregnancy and maternity, disability, sexual orientation, religion or belief, sex, marriage and civil partnership.

435. This application was accompanied by an Equalities Impact Assessment prepared by the applicant to assist the Council with considering the impact of the development in this respect. The study area considered for the assessment focused on the local area (i.e. Rotherhithe Ward) and at the borough level (i.e. London Borough of Southwark) although data for the Greater London area has also been provided for wider context. This is considered to be appropriate to form a baseline for the assessment.

436. The document confirms that the applicant has undertaken community consultation to develop an understanding of the needs and aspirations of local residents and key stakeholders. This was discussed in detail earlier in the committee report and is



considered to be proportionate and appropriate.

437. The assessment identifies a range of potential impacts on the local community during construction and operation. Potential impacts in terms of open space, infrastructure, environmental factors, amenity, accessibility, employment creation and health have been discussed in detail in the relevant sections of this committee report and any necessary mitigation to limit adverse impacts has been secured through s106 obligations and planning conditions (for example construction impacts will be minimised through the use of a CEMP).
438. This application would deliver a significant amount of commercial development including a large quantum of affordable workspace thus presenting opportunities to enhance access to employment for all residents of the borough including those with protected characteristics. There will also be positive benefits in terms of the health centre provision and community uses due to come forward in the latter phases of the development (Plot B) as well as positive health and amenity benefits through enhanced public realm. The positive impacts arising from the development would benefit those groups with protected characteristics as well as the wider community.
439. The detailed proposals have been designed to ensure inclusive access for all. All public realm areas have appropriate gradients and slopes instead of steps wherever possible. The landscaped areas will incorporate appropriately designed benches and play equipment for a range of users. There is level access into the buildings and internally the design incorporates an appropriate provision of wheelchair accessible toilets, lifts, wide corridors, doors and circulation areas. Furthermore both buildings provide DDA cycle parking facilities and parking spaces.
440. It is recognised that there are existing uses on the site which would be displaced as a result of this permission being implemented. However, the existing uses on the site are all meanwhile uses subject to temporary planning permissions. At the time of occupying the site this would have been the known position. The OPP established the principle of redeveloping the site and existing uses being displaced. Once the development is complete there will be an opportunity for some of the existing uses to occupy the new buildings.
441. There are very likely to be individuals from groups with protected characteristics who make use of the existing uses taking place within the buildings. However the uses are not specific to any one group, and as stated the uses are meanwhile uses. For those reasons the grant of permission is not considered to have an unacceptable equalities impact.

### **Human rights implications**

442. This planning application engages certain human rights under the Human Rights Act 2008 (the HRA). The HRA prohibits unlawful interference by public bodies with conventions rights. The term 'engage' simply means that human rights may be affected or relevant.
443. This application has the legitimate aim of delivering commercial development. The rights potentially engaged by this application, including the right to a fair trial and the right to respect for private and family life are not considered to be unlawfully interfered with by this proposal.

## Positive and proactive statement

444. The council has published its development plan on its website together with advice about how applications are considered and the information that needs to be submitted to ensure timely consideration of an application. Applicants are advised that planning law requires applications to be determined in accordance with the development plan unless material considerations indicate otherwise.
445. The council provides a pre-application advice service that is available to all applicants in order to assist applicants in formulating proposals that are in accordance with the development plan, and this service was used by the applicant in this case.
446. **Positive and proactive engagement: summary table**

Was the pre-application service used for this application?	YES
If the pre-application service was used for this application, was the advice given followed?	YES
Was the application validated promptly?	YES
If necessary/appropriate, did the case officer seek amendments to the scheme to improve its prospects of achieving approval?	NO
To help secure a timely decision, did the case officer submit their recommendation in advance of the agreed Planning Performance Agreement date?	YES

## CONCLUSION

447. In land use terms the proposal accords with the site allocation and, other relevant development plan policies and is consistent with the principles established by the OPP
448. The proposed quantum of floorspace and range of uses to be provided sits comfortably within the approved Development Specification.
449. The proposed development would deliver a significant amount of commercial floorspace creating the opportunity for circa 6,000 FTE jobs, which would make a positive contribution employment opportunities in the Borough.
450. Obligations secured as part of the OPP include provision of affordable retail and workspace. A policy compliant quantum will be delivered within this phase.
451. In design terms the proposal for each building accords with the principles established by the OPP and subsequent s96a application.
452. The proposed buildings are of an appropriate form and scale and the materials pallet chosen will result in robust, high quality buildings, which will make a positive contribution to the townscape and character of this part of the town centre.

453. The site layout, provision of public realm and detailed design for Waterfront Square accords with the details approved in the OPP Design Codes. Subject to high quality execution, as secured by the conditions attached to the OPP the proposal will have a positive place making benefit on this part of the town centre.
454. Subject to conditions already attached to the OPP the proposal would not give rise to significant harm to neighbouring amenity by way of daylight/sunlight, overlooking, loss of privacy, noise or disturbance.
455. An EIA Statement of Conformity has been provided to demonstrate that the assumptions, conclusions and mitigation secured at outline stage are still fit for purpose and that the RMA's subject of this report would not give rise to new significant effects.
456. Subject to the necessary mitigation already secured as part of the OPP s106 obligation (to which the RMA's will be bound) the proposals would not give rise to unacceptable transport impacts.
457. Subject to compliance with the detailed energy and sustainability strategies submitted and payment of the Carbon Green Fund, the proposals satisfactorily address climate change policies.
458. It is therefore recommended that planning permission be granted for the Reserved Matters Applications subject of this report.

Namely;

**23/AP/0562** - Reserved Matters Approval for construction of Building A1 with landscaping, plant and associated works.

**23/AP/0564** - Reserved Matters Approval for construction of Building A2 with cycle parking, landscaping, loading bay, plant and associated works.

**23/AP/0565** - Reserved Matters Approval for construction of a basement spanning Plots A1 and A2 comprising cycle parking, plant space and ancillary floorspace and associated works to support office and retail/food and beverage uses in Plots A1 and A2.

**23/AP/0566** - Approval of Reserved Matter in respect of hard and soft landscaping to create a shared public realm space for use by pedestrians and vehicles on Maritime Street.

## BACKGROUND DOCUMENTS

Background Papers	Held At	Contact
Southwark Local Development Framework and Development Plan Documents	Environment Neighbourhoods and Growth. 160 Tooley Street London SE1 2QH	Planning enquiries telephone: 020 7525 5403 Planning enquiries email: planning.enquiries@southwark.gov.uk Case officer telephone: 0207 525 0254 Council website: www.southwark.gov.uk

## APPENDICES

No.	Title
Appendix 1	Recommendation (draft decision notices)
Appendix 2	Relevant planning policy
Appendix 3	Planning history of the site and nearby sites
Appendix 4	Consultation undertaken
Appendix 5	Consultation responses received.

## AUDIT TRAIL

<b>Lead Officer</b>	Stephen Platts, Director of Planning and Growth	
<b>Report Author</b>	Gemma Usher, Team Leader	
<b>Version</b>	Final	
<b>Dated</b>	25 August 2023	
<b>Key Decision</b>	No	
<b>CONSULTATION WITH OTHER OFFICERS / DIRECTORATES / CABINET MEMBER</b>		
<b>Officer Title</b>	<b>Comments Sought</b>	<b>Comments included</b>
Strategic Director of Finance	No	No
Strategic Director of Environment Neighbourhoods and Growth	No	No
Strategic Director of Housing	No	No
<b>Date final report sent to Constitutional Team</b>		30 August 2023

## Recommendation – Planning Permission 23/AP/0562

This document shows the case officer's recommended decision for the application referred to below.

This document is not a decision notice for this application.

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<b>Applicant</b>	Charlotte AIRE UK Canada Water GP PropCos Ltd, acting as general pa...	<b>Reg. Number</b>	23/AP/0562
<b>Application Type</b>	Approval of Reserved Matters		
<b>Recommendation</b>	APPROVE reserved matters	<b>Case Number</b>	468-C

### Draft of Decision Notice

**Reserved matters is APPROVED for the following development:**

Approval of Reserved Matters (access, appearance, landscaping, layout and scale) pursuant to outline planning permission 21/AP/2655 for the comprehensive commercial development of the Canada Water Dockside Masterplan site. Reserved Matters Approval sought for construction of Building A1 (maximum height 110m AOD) comprising the following uses; offices (Class E), retail (E) and food and beverage (E), with landscaping, plant and associated works. This is an application for subsequent consent accompanied by an Environmental Statement. Consequently the application is accompanied by a Statement of Conformity submitted pursuant to the Town and Country Planning (Environmental Impact Assessment) regulations 2017. This ES Statement of Conformity should be read in conjunction with the Canada Water Dockside Masterplan ES which can be viewed in full on the Councils website (21/AP/2655).

Units 1 And 4 Canada Water Retail Park London Southwark

**In accordance with application received on 1 March 2023 and Applicant's Drawing Nos.:**



## Existing Plans

## Proposed Plans

Plans - Proposed 230606\_CW A2\_POROUS WIND MITIGATION SCREEN (004)  
received 11/07/2023

Plans - Proposed CWD-TLA-RMA-XX-RP-L-0002-XX\_P00 (004). Wind Mitigation  
Details received 11/07/2023

Plans - Proposed CWD-BIG-A1-XX-DR-A-1000-XX LOCATION PLAN - A1  
BUILDING received 06/03/2023

Plans - Proposed CWD TLA RMA XX DR L 3003 XX REV P00 STREET  
SURFACE PLAN - PLOT A AND MARITIME STREET received 06/03/2023

Plans - Proposed CWD-BIG-A1-XX-DR-A-1001-XX SITE PLAN received 06/03/2023

Plans - Proposed CWD-TLA-A1-XX-DR-L-3005-XX REV P00 SURFACE  
FINISH PLAN - PLOT A1 received 06/03/2023

Plans - Proposed CWD-TLA-A1-XX-DR-L-5002-XX REV P00 TREE PLANTING  
PLAN - PLOT A received 06/03/2023

Plans - Proposed CWD-TLA-A1-XX-DR-L-7002-XX REV P00 PUBLIC REALM  
SECTIONS received 06/03/2023

Plans - Proposed CWD-TLA-RMA-XX-DR-L-3002-XX REV P00 LANDSCAPE  
MASTERPLAN - PUBLIC REALM received 06/03/2023

Plans - Proposed CWD BIG A1 XX DR A 4001 XX PROPOSED WEST  
ELEVATION received 06/03/2023

Plans - Proposed CWD BIG A1 XX DR A 4002 XX PROPOSED NORTH  
ELEVATION received 06/03/2023

Plans - Proposed CWD BIG A1 XX DR A 4003 XX PROPOSED SOUTH  
ELEVATION received 06/03/2023

Plans - Proposed CWD BIG A1 XX DR A 4004 XX PROPOSED EAST  
ELEVATION received 06/03/2023

Plans - Proposed CWD-BIG-A1-01-DR-A-2002-XX PROPOSED GA PLAN: LEVEL  
1 received 06/03/2023

Plans - Proposed CWD-BIG-A1-02-DR-A-2003-XX PROPOSED GA PLAN: LEVEL  
2 received 06/03/2023

Plans - Proposed CWD-BIG-A1-03-DR-A-2004-XX PROPOSED GA PLAN: LEVEL  
3 received 06/03/2023

Plans - Proposed CWD-BIG-A1-04-DR-A-2005-XX PROPOSED GA PLAN: LEVEL  
4 received 06/03/2023

Plans - Proposed CWD-BIG-A1-05-DR-A-2006-XX PROPOSED GA PLAN: LEVEL  
5 received 06/03/2023

Plans - Proposed CWD-BIG-A1-06-DR-A-2007-XX 6 received 06/03/2023	PROPOSED GA PLAN: LEVEL
Plans - Proposed CWD-BIG-A1-07-DR-A-2008-XX 7 received 06/03/2023	PROPOSED GA PLAN: LEVEL
Plans - Proposed CWD-BIG-A1-08-DR-A-2009-XX 8 received 06/03/2023	PROPOSED GA PLAN: LEVEL
Plans - Proposed CWD-BIG-A1-09-DR-A-2010-XX 9 received 06/03/2023	PROPOSED GA PLAN: LEVEL
Plans - Proposed CWD-BIG-A1-10-DR-A-2011-XX 10 received 06/03/2023	PROPOSED GA PLAN: LEVEL
Plans - Proposed CWD-BIG-A1-11-DR-A-2012-XX 11 received 06/03/2023	PROPOSED GA PLAN: LEVEL
Plans - Proposed CWD-BIG-A1-12-DR-A-2013-XX 12 received 06/03/2023	PROPOSED GA PLAN: LEVEL
Plans - Proposed CWD-BIG-A1-13-DR-A-2014-XX 13 received 06/03/2023	PROPOSED GA PLAN: LEVEL
Plans - Proposed CWD-BIG-A1-14-DR-A-2015-XX 14 received 06/03/2023	PROPOSED GA PLAN: LEVEL
Plans - Proposed CWD-BIG-A1-15-DR-A-2016-XX 15 received 06/03/2023	PROPOSED GA PLAN: LEVEL
Plans - Proposed CWD-BIG-A1-16-DR-A-2017-XX 16 received 06/03/2023	PROPOSED GA PLAN: LEVEL
Plans - Proposed CWD-BIG-A1-17-DR-A-2018-XX 17 received 06/03/2023	PROPOSED GA PLAN: LEVEL
Plans - Proposed CWD-BIG-A1-18-DR-A-2019-XX 18 received 06/03/2023	PROPOSED GA PLAN: LEVEL
Plans - Proposed CWD-BIG-A1-19-DR-A-2020-XX 19 received 06/03/2023	PROPOSED GA PLAN: LEVEL
Plans - Proposed CWD-BIG-A1-20-DR-A-2021-XX 20 received 06/03/2023	PROPOSED GA PLAN: LEVEL
Plans - Proposed CWD-BIG-A1-21-DR-A-2022-XX 21 received 06/03/2023	PROPOSED GA PLAN: LEVEL
Plans - Proposed CWD-BIG-A1-22-DR-A-2023-XX 22 received 06/03/2023	PROPOSED GA PLAN: LEVEL
Plans - Proposed CWD-BIG-A1-23-DR-A-2024-XX 23 received 06/03/2023	PROPOSED GA PLAN: LEVEL
Plans - Proposed CWD-BIG-A1-24-DR-A-2025-XX 24 received 06/03/2023	PROPOSED GA PLAN: LEVEL

Plans - Proposed CWD-BIG-A1-GF-DR-A-2000 -XX PLAN:GROUND FLOOR received 06/03/2023	PROPOSED GA
Plans - Proposed CWD-BIG-A1-M1-DR-A-2001-XX MEZZANINE received 06/03/2023	PROPOSED GA PLAN:
Plans - Proposed CWD-BIG-A1-RF-DR-A-2026-XX LEVEL received 06/03/2023	PROPOSED GA PLAN: ROOF
Plans - Proposed CWD-BIG-A1-XX-DR-A-3000-XX received 06/03/2023	PROPOSED SECTION AA
Plans - Proposed CWD-BIG-A1-XX-DR-A-5001-XX FACADE DETAIL 2 received 06/03/2023	PROPOSED GROUND FLOOR
Plans - Proposed CWD-BIG-A1-XX-DR-A-3001-XX received 06/03/2023	PROPOSED SECTION BB
Plans - Proposed CWD-BIG-A1-XX-DR-A-3002-XX received 06/03/2023	PROPOSED SECTION CC
Plans - Proposed CWD-BIG-A1-XX-DR-A-5000-XX FACADE DETAIL 1 received 06/03/2023	PROPOSED GROUND FLOOR
Plans - Proposed CWD-BIG-A1-XX-DR-A-5002-XX FACADE DETAIL - WEST. received 06/03/2023	PROPOSED TYPICAL
Plans - Proposed CWD-BIG-A1-XX-DR-A-5003-XX FACADE DETAIL - EAST received 06/03/2023	PROPOSED TYPICAL
Plans - Proposed CWD-BIG-A1-XX-DR-A-5004-XX FACADE DETAIL - NORTH received 06/03/2023	PROPOSED TYPICAL
Plans - Proposed CWD-BIG-A1-XX-DR-A-5005-XX FACADE DETAIL - SOUTH received 06/03/2023	PROPOSED TYPICAL
Plans - Proposed CWD-BIG-A1-XX-DR-A-5006-XX TERRACE DETAIL received 06/03/2023	PROPOSED TYPICAL MAIN
Plans - Proposed CWD-BIG-A1-XX-DR-A-5007-XX RECESSED TERRACE DETAIL received 06/03/2023	PROPOSED TYPICAL
Plans - Proposed CWD-ASD-DE-XX-DR-L-0700-XX-P01 MITIGATION MEASURES received 14/07/2023	TEMPORARY WIND

#### Other Documents

Energy statement 60952\_CWD A1\_PART L  
2013\_GLA CARBON EMISSION REPORTING\_SPREADSHEET\_V1.2\_2020  
received 11/07/2023

Energy statement 60952\_CWD A1\_PART L  
2021\_GLA CARBON EMISSION REPORTING\_SPREADSHEET\_V2.0\_0 received

11/07/2023

Energy statement 60952\_ENERGY STRATEGY\_REV03 PLOT A1 received  
11/07/2023

Sustainability statement CWD-A10-A1-XX-RP-Y-0001-XX-P02 SUSTAINABILITY  
STATEMENT PLOT A1 received 11/07/2023

Energy statement CWD-A10-A1-XX-RP-Y-0002-XX-P01 WLC ASSESSMENT A1.  
received 11/07/2023

Energy statement CWD-A10-A1-XX-RP-Y-0002-XX-P01 WLC SPREADSHEET A1.  
received 11/07/2023

Energy statement CWD-A10-A1-XX-RP-Y-0003-XX-P01 REVISED CES PLOT A1  
received 11/07/2023

Energy statement SITE WIDE PART L 2013  
GLA CARBON EMISSION REPORTING SPREADSHEET\_V1.2\_2020 received  
11/07/2023

Energy statement SITE WIDE PART L  
2021\_GLA CARBON EMISSION REPORTING SPREADSHEET\_V2.0\_0 received  
11/07/2023

Tree survey and assessment CWD-TMA-RMA-XX-RP-Z-0002-XX-P02  
ARBORICULTURAL ADDENDUM received 11/07/2023

Document Utilities Report CWD-CBD-RMA-XX-RP-Y-0001-XX-P00 received  
06/03/2023

Fire Statement C W D -OF R-A1-X X-R P -Y-000 1-X X-P 00 received 06/03/2023

Design and access statement CWD-BIG-A1-XX-RP-A-0001-XX-P00 received  
06/03/2023

Tree survey and assessment AIA C WD-TMA-RMA-X X-RP-Z-000 1-X X-P00  
received 06/03/2023

Document DSMP CWD-WSP-RMA-XX-RP-Y-0001-XX-P00 received 06/03/2023

Document Development Consultation Charter CWD-KAN-DE-XX-RP-Y-0002-XX-P00  
received 06/03/2023

Waste Management Strategy CWD-WSP-RMA-XX-RP-Y-0003-XX-P00 received  
06/03/2023

Document Planning Compliance Report C W D -DP 9-R M A-X X-R P -T-0002-X X-P  
00 received 06/03/2023

Tree survey and assessment AIA addendum 201145-CWD-TMA-RMA-XX-RP-Z-0002  
XX P00 ARBORICULTURAL ADDENDUM received 06/03/2023

Document Landscape DAS C W D-T L A-R M A-X X-R P -L-000 1-X X-P 00 received  
06/03/2023

Transport assessment/statement CWD-WSP-RMA-XX-RP-Y-0002-XX-P00 received

06/03/2023

Travel plan CWD-WSP-A1-XX-RP-Y-0001-XX-P00 received 06/03/2023

Ecology assessment/Nature conservation BNG Report C WD-SLO-RMA-X X-RP-Z-000 1-P00 received 06/03/2023

Tree survey and assessment AIA Addendum CWD-TMA-RMA-XX-RP-Z-0002-XX-P02 received 13/06/2023

Environmental Statement Statement of Conformity received 22/05/2023

Document Equalities Impact Assessment CWD-TRI-RMA-X X-RP-Y-0002-X X-P00 received 20/03/2023

Document SCI CWD-KAN-RMA-XX-RP-Y-0001-XX-P00 received 06/03/2023

Planning statement CWD - RMA PLANNING STATEMENT V2. received 14/07/2023

Document CWD\_PLOT A RMA EIA STATEMENT OF CONFORMITY\_JULY 2023 received 14/07/2023

**Permission is subject to the following Pre-Occupation Condition(s)**

2.
  - a) Not to commence the development hereby approved until the developer has submitted details of the temporary wind mitigation measures shown on the plan hereby approved (CWD-ASD-DE-XX-DR-L-0701-XX-P01\_LANDSCAPE PLAN FOR TEMPORARY WIND MITIGATION) to the Council and the Council has approved the mitigation measures. The details shall include the location, design, material, dimension and height of all planters, the species of planting and size and a wind mitigation assessment demonstrating the mitigation measures achieve the relevant comfort category.
  - b) Not to commence above ground works of the development hereby approved until the developer has confirmed to the Council that the necessary consents relating to the delivery of the temporary wind mitigation approved under part a) have been obtained. If the necessary consents cannot be obtained, the developer shall submit details of alternative temporary wind mitigation to the council for approval.
  - c) The wind mitigation assessment submitted under Part a) or Part b) shall identify the full impacts of wind within the public realm to the East and Southern edges of The Dock and the public realm to the South and West of Canada Water Dockside Plot A1 (using the Lawson criteria) to match the scope of the Wind Report prepared by RWDI contained within the approved EIA Statement of Conformity and to detail point at which the Temporary Wind Mitigation will no longer be required.



- d) Not to occupy the development hereby approved until the mitigation measures approved by the Council pursuant to Part a) or Part b) above have been provided and, unless otherwise agreed by the Council, not to remove or permit the removal of such mitigation measures until practical completion of the development in Development Zone D of the adjacent Canada Water Masterplan development site (allocated local planning authority (18/AP/1604).

Reason : To ensure that satisfactory wind conditions can be achieved within the public realm in accordance with the strategy proposed in the plans and documents submitted as part of this application and to accord with policies D8 Public realm of the London Plan (2021) and P13 Design of places and P56 Protection of amenity of the Southwark Plan (2022).

## Informatives

- 1 Paragraph 3.12.9 of Policy D12 explains that Fire Statements should be produced by someone who is:
 

"third-party independent and suitably-qualified" The Council considers this to be a qualified engineer with relevant experience in fire safety, such as a chartered engineer registered with the Engineering Council by the Institution of Fire Engineers, or a suitably qualified and competent professional with the demonstrable experience to address the complexity of the design being proposed. This should be evidenced in the fire statement. The Council accepts Fire Statements in good faith on that basis. The duty to identify fire risks and hazards in premises and to take appropriate action lies solely with the developer.

The fire risk assessment/statement covers matters required by planning policy. This is in no way a professional technical assessment of the fire risks presented by the development. The legal responsibility and liability lies with the 'responsible person'. The responsible person being the person who prepares the fire risk assessment/statement not planning officers who make planning decisions.
- 2 The Applicant is reminded that in order to discharge into the Dock, consent will be required from all relevant land owners. Should such consent not be granted the Applicant will be required to connect to the sewer. In any event it will be necessary to submit a formal application to discharge Condition 17 of 21/AP/2655.

As part of this formal submission for discharge of Condition 17 the Applicant will be required to demonstrate that any water proposed to be discharged into the Dock meets CIRIA Suds standards or in the event that discharge to the

sewer is required the strategy to be submitted under Condition 17 will need to fully demonstrate compliance with maximising SUDs onsite and meeting policy restricted runoff rates

## **Relevant Policies - 23/AP/0562, 23/AP/0564, 23/AP/0565, 23/AP/0566**

### National Planning Policy Framework

The revised National Planning Policy Framework ('NPPF') was published on 20 July 2021 which sets out the national planning policy and how this needs to be applied. The NPPF focuses on sustainable development with three key objectives: economic, social and environmental. Paragraph 218 states that the policies in the Framework are material considerations, which should be taken into account in dealing with applications.

The following chapters are relevant:

Chapter 2 Achieving sustainable development

Chapter 6 Building a strong, competitive economy

Chapter 7 Ensuring the vitality of town centres

Chapter 8 Promoting healthy and safe communities

Chapter 9 Promoting sustainable transport

Chapter 11 Making effective use of land

Chapter 12 Achieving well-designed places

Chapter 14 Meeting the challenge of climate change, flooding and coastal change

Chapter 16 Conserving and enhancing the historic environment

### New London Plan 2021 Policies

On 2 March 2021, the Mayor of London published the London Plan 2021. The spatial development strategy sets a strategic framework for planning in Greater London and forms part of the statutory Development Plan for Greater London.

The relevant policies are:

GG1 Building strong and inclusive communities

GG2 Making the best use of land

GG3 Creating a healthy city

GG5 Growing a good economy

GG6 Increasing efficiency and resilience

Policy SD1 Opportunity Areas

Policy SD6 Town centres and high streets

Policy SD7 Town centres: development principles and Development Plan Documents

Policy SD8 Town centre network

Policy SD9 Town centres: Local partnerships and implementation

Policy SD10 Strategic and local regeneration

Policy D1 London's form, character and capacity for growth

Policy D2 Infrastructure requirements for sustainable densities

Policy D3 Optimising site capacity through the design-led approach

Policy D4 Delivering good design

Policy D5 Inclusive design

Policy D8 Public realm

Policy D9 Tall buildings

Policy D10 Basement development  
 Policy D11 Safety, security and resilience to emergency  
 Policy D12 Fire safety  
 Policy D13 Agent of Change  
 Policy D14 Noise  
 Policy S1 Developing London's social infrastructure  
 Policy S6 Public toilets  
 Policy E1 Offices  
 Policy E2 Providing suitable business space  
 Policy E3 Affordable workspace  
 Policy E8 Sector growth opportunities and clusters  
 Policy E9 Retail, markets and hot food takeaways  
 Policy E10 Visitor infrastructure  
 Policy E11 Skills and opportunities for all  
 Policy HC1 Heritage conservation and growth  
 Policy HC3 Strategic and Local Views  
 Policy HC4 London View Management Framework  
 Policy HC6 Supporting the night-time economy  
 Policy G1 Green infrastructure  
 Policy G5 Urban greening  
 Policy G6 Biodiversity and access to nature  
 Policy G7 Trees and woodlands  
 Policy SI 1 Improving air quality  
 Policy SI 2 Minimising greenhouse gas emissions  
 Policy SI 3 Energy infrastructure  
 Policy SI 4 Managing heat risk  
 Policy SI 5 Water infrastructure  
 Policy SI 6 Digital connectivity infrastructure  
 Policy SI 7 Reducing waste and supporting the circular economy  
 Policy SI 8 Waste capacity and net waste self-sufficiency  
 Policy SI 12 Flood risk management  
 Policy SI 13 Sustainable drainage  
 Policy T1 Strategic approach to transport  
 Policy T2 Healthy Streets  
 Policy T3 Transport capacity, connectivity and safeguarding  
 Policy T4 Assessing and mitigating transport impacts  
 Policy T5 Cycling  
 Policy T6 Car parking  
 Policy T6.2 Office parking  
 Policy T6.3 Retail parking  
 Policy T6.5 Non-residential disabled persons parking  
 Policy T7 Deliveries, servicing and construction  
 Policy T9 Funding transport infrastructure through planning

### Southwark Plan 2022

ST1 Southwark's Development targets  
 ST2 Southwark's Places  
 SP2 Southwark Together  
 SP3 Great start in life  
 SP4 Green and inclusive economy  
 SP5 Thriving neighbourhoods and tackling health inequalities

SP6 Climate Change  
AV.15 Rotherhithe Area Vision  
P13 Design of places  
P14 Design quality  
P16 Designing out crime  
P17 Tall buildings  
P18 Efficient use of land  
P20 Conservation areas  
P21 Conservation of the historic environment and natural heritage  
P22 Borough views  
P23 Archaeology  
P28 Access to employment and training  
P30 Office and business development  
P31 Affordable workspace  
P35 Town and local centres  
P44 Broadband and digital infrastructure  
P45 Healthy developments  
P47 Community uses  
P49 Public transport  
P50 Highways impacts  
P51 Walking  
P53 Cycling  
P54 Car Parking  
P55 Parking standards for disabled people and the mobility impaired  
P56 Protection of amenity  
P57 Open space  
P59 Green infrastructure  
P60 Biodiversity  
P61 Trees  
P62 Reducing waste  
P64 Contaminated land and hazardous substances  
P65 Improving air quality  
P66 Reducing noise pollution and enhancing soundscapes  
P67 Reducing water use  
P68 Reducing flood risk  
P69 Sustainability standards  
P70 Energy  
NSP80 Decathlon Site and Mulberry Business Park  
Sustainable Transport (2009)



### Relevant planning history - Planning Permission 23/AP/0562

Reference and Proposal	Status
<p><b>21/AP/2655</b></p> <p>Outline planning permission (all matters reserved) for a commercial masterplan containing offices and other complementary town centre uses with no housing. The proposals involve the demolition of all buildings and structures and the comprehensive redevelopment of the site to provide three Development Plots (A1, A2 and B) which will contain three Buildings (A1, A2 and B) above ground with basements. The proposed land uses are: offices (Class E), retail/professional services/food and drink (E), learning and non-residential institutions/local community (F1/F2), medical or health (E) and indoor sport, recreation or fitness (E). - Works of hard and soft landscaping are proposed to create a series of new and improved public realm spaces within the site. This will include the replacement of some trees and the planting of additional new trees. - New vehicular access points are proposed to be created from Surrey Quays Road and Canada Street, along with other incidental works.</p>	<p>GRANTED - Major Application 25/01/2023</p>

## Consultation undertaken - Planning Permission 23/AP/0562

**Site notice date:** 13/03/2023

**Press notice date:** 16/03/2023

**Case officer site visit date:** n/a

**Neighbour consultation letters sent:** 09/03/2023

### Internal services consulted

Formal consultation and response to Pol  
 Community Infrastructure Levy Team  
 Archaeology  
 Section 106 Team  
 Design and Conservation Team [Formal]  
 Local Economy  
 Ecology  
 Environmental Protection  
 Highways Development and Management  
 Flood Risk Management & Urban Drainage  
 Urban Forester  
 Waste Management  
 Transport Policy

### Statutory and non-statutory organisations

Environment Agency  
 London Fire & Emergency Planning Authori  
 London Underground  
 Metropolitan Police Service (Designing O  
 Transport for London  
 Thames Water  
 Historic England

### Neighbour and local groups consulted:

Portacabins Canada Water Retail Park Surrey Quays Road	602 Pavillion House Water Gardens Square London
Surrey Quays Leisure Park Surrey Quays Road London	9 Maritime Street London Southwark Rear Of Room Quays Unit A2 Toronto House Surrey Quays Road
Phantom Peak Surrey Quays Road London	47 Montreal House Surrey Quays Road London
642 Pavillion House Water Gardens Square London	Flat 10 Toronto House Surrey Quays

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 Flat 7 Toronto House Surrey Quays  
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 Flat 5 Toronto House Surrey Quays  
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 Unit C Toronto House Surrey Quays  
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 643 Pavillion House Water Gardens  
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 Flat 17 7 Maritime Street London  
 Flat 14 7 Maritime Street London  
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 25 Maritime Street London Southwark  
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Flat 44 7 Maritime Street London	Flat 44 11 Maritime Street London
Flat 39 7 Maritime Street London	56 Montreal House Surrey Quays Road London
Flat 38 7 Maritime Street London	55 Montreal House Surrey Quays Road London
21 Maritime Street London Southwark	53 Montreal House Surrey Quays Road London
17 Maritime Street London Southwark	51 Montreal House Surrey Quays Road London
3 Maritime Street London Southwark	50 Montreal House Surrey Quays Road London
1 Maritime Street London Southwark	48 Montreal House Surrey Quays Road London
Flat 38 29 Surrey Quays Road London	45 Montreal House Surrey Quays Road London
Flat 35 29 Surrey Quays Road London	44 Montreal House Surrey Quays Road London
Flat 34 29 Surrey Quays Road London	42 Montreal House Surrey Quays Road London
Flat 32 29 Surrey Quays Road London	40 Montreal House Surrey Quays Road London
Flat 31 29 Surrey Quays Road London	37 Montreal House Surrey Quays Road London
Flat 29 29 Surrey Quays Road London	36 Montreal House Surrey Quays Road London
Flat 28 29 Surrey Quays Road London	34 Montreal House Surrey Quays Road London
Flat 25 29 Surrey Quays Road London	32 Montreal House Surrey Quays Road London
Flat 23 29 Surrey Quays Road London	31 Montreal House Surrey Quays Road London
Flat 21 29 Surrey Quays Road London	28 Montreal House Surrey Quays Road London
Flat 18 29 Surrey Quays Road London	26 Montreal House Surrey Quays Road London
Flat 16 29 Surrey Quays Road London	24 Montreal House Surrey Quays Road London
Flat 15 29 Surrey Quays Road London	23 Montreal House Surrey Quays Road London
Flat 13 29 Surrey Quays Road London	21 Montreal House Surrey Quays Road London
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Flat 70 11 Maritime Street London	
Flat 68 11 Maritime Street London	
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136 Montreal House Surrey Quays Road London	143 Montreal House Surrey Quays Road London
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132 Montreal House Surrey Quays Road London	118 Montreal House Surrey Quays Road London
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128 Montreal House Surrey Quays Road London	115 Montreal House Surrey Quays Road London
125 Montreal House Surrey Quays Road London	114 Montreal House Surrey Quays Road London
Flat 4 Toronto House Surrey Quays Road	112 Montreal House Surrey Quays Road London
Flat 1 Toronto House Surrey Quays Road	110 Montreal House Surrey Quays Road London
Unit C Montreal House Surrey Quays Road	107 Montreal House Surrey Quays Road London
Unit A Montreal House Surrey Quays Road	105 Montreal House Surrey Quays Road London
169 Montreal House Surrey Quays Road London	104 Montreal House Surrey Quays Road London
165 Montreal House Surrey Quays Road London	102 Montreal House Surrey Quays Road London
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144 Montreal House Surrey Quays Road	80 Montreal House Surrey Quays Road London

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Canada Water Library 21 Surrey Quays  
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15 Maritime Street London Southwark  
156 Montreal House Surrey Quays Road  
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138 Montreal House Surrey Quays Road  
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109 Montreal House Surrey Quays Road  
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72 Montreal House Surrey Quays Road  
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Flat 50 29 Surrey Quays Road London	692 Pavillion House Water Gardens Square London
Flat 49 7 Maritime Street London	683 Pavillion House Water Gardens Square London
Flat 46 7 Maritime Street London	681 Pavillion House Water Gardens Square London
Flat 28 Toronto House Surrey Quays Road	665 Pavillion House Water Gardens Square London
Unit A2 Toronto House Surrey Quays Road	623 Pavillion House Water Gardens Square London
535 Giverny House Water Gardens Square London	564 Giverny House Water Gardens Square London
534 Giverny House Water Gardens Square London	562 Giverny House Water Gardens Square London
532 Giverny House Water Gardens Square London	554 Giverny House Water Gardens Square London
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524 Giverny House Water Gardens Square London	444 Heligan House Water Gardens Square London
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573 Giverny House Water Gardens Square London	422 Heligan House Water Gardens Square London
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542 Giverny House Water Gardens Square London	Flat 60 Toronto House Surrey Quays Road
501 Giverny House Water Gardens Square London	Flat 58 Toronto House Surrey Quays Road
454 Heligan House Water Gardens Square London	Flat 55 Toronto House Surrey Quays Road
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101 Montreal House Surrey Quays Road  
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81 Montreal House Surrey Quays Road  
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 464 Heligan House Water Gardens  
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 326 Eden House Water Gardens Square  
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 33 Montreal House Surrey Quays Road  
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 Flat 1 5 Maritime Street London  
 Flat 49 29 Surrey Quays Road London  
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 Flat 13 7 Maritime Street London  
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 Flat 1 7 Maritime Street London  
 Flat 57 11 Maritime Street London  
 147 Montreal House Surrey Quays Road  
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 302 Eden House Water Gardens Square  
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 Flat 67 29 Surrey Quays Road London  
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 Flat 4 11 Maritime Street London  
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 Flat 58 7 Maritime Street London  
 Flat 55 7 Maritime Street London  
 Flat 52 7 Maritime Street London  
 Flat 7 5 Maritime Street London  
 Flat 4 5 Maritime Street London  
 19 Maritime Street London Southwark  
 13 Maritime Street London Southwark  
 Flat 25 5 Maritime Street London  
 Flat 22 5 Maritime Street London  
 Flat 19 5 Maritime Street London  
 Flat 16 5 Maritime Street London  
 Flat 10 5 Maritime Street London  
 Flat 54 11 Maritime Street London  
 Flat 49 11 Maritime Street London  
 Flat 46 11 Maritime Street London  
 634 Pavillion House Water Gardens  
 Square London  
 632 Pavillion House Water Gardens  
 Square London  
 631 Pavillion House Water Gardens  
 Square London  
 621 Pavillion House Water Gardens  
 Square London  
 615 Pavillion House Water Gardens  
 Square London  
 Unit 4 Canada Water Retail Park Surrey



Quays Road  
 Unit 1 Toronto House Surrey Quays Road  
 Tinos Mobile Pizza Deal Porter Square London  
 Unit 1 Canada Water Retail Park Surrey Quays Road  
 Stompin Jago Canada Water Library 21 Surrey Quays Road  
 Unit B Toronto House Surrey Quays Road  
 Top Hill Gelato Deal Porter Square London  
 4 Teredo Street London Southwark  
 Pizza Hut 5 The Mast Leisure Park Teredo Street  
 Hollywood Bowl 3A The Mast Leisure Park Teredo Street  
 7-9 Surrey Quays Shopping Centre Redriff Road London  
 Thrive Business Hub The Mast Leisure Park Teredo Street  
 Tesco Instore Pharmacy Surrey Quays Shopping Centre Redriff Road  
 14 Surrey Quays Shopping Centre Redriff Road London  
 35 Surrey Quays Shopping Centre Redriff Road London  
 5 Surrey Quays Shopping Centre Redriff Road London  
 10-12 Surrey Quays Shopping Centre Redriff Road London  
 37 Surrey Quays Shopping Centre Redriff Road London  
 19 Surrey Quays Shopping Centre Redriff Road London  
 28 Surrey Quays Shopping Centre Redriff Road London  
 2-4 Surrey Quays Shopping Centre Redriff Road London  
 Flat 1 1 Teredo Street London  
 Management Suite Surrey Quays Shopping Centre Redriff Road  
 3 Surrey Quays Shopping Centre Redriff Road London  
 First Floor 59 Surrey Quays Shopping Centre Redriff Road  
 45 Surrey Quays Shopping Centre Redriff Road London  
 1 Surrey Quays Shopping Centre Redriff Road London  
 Unit D1 Surrey Quays Shopping Centre Redriff Road  
 22 Surrey Quays Shopping Centre Redriff Road London  
 42 Surrey Quays Shopping Centre Redriff Road London  
 18 Surrey Quays Shopping Centre Redriff Road London  
 44 Surrey Quays Shopping Centre Redriff Road London  
 53-55 Surrey Quays Shopping Centre Redriff Road London  
 6 Surrey Quays Shopping Centre Redriff Road London  
 30-34 Surrey Quays Shopping Centre Redriff Road London  
 16 Surrey Quays Shopping Centre Redriff Road London  
 11-13 Surrey Quays Shopping Centre Redriff Road London  
 20 Surrey Quays Shopping Centre Redriff Road London  
 1 Teredo Street London Southwark  
 46 - 50 Surrey Quays Shopping Centre Redriff Road London  
 First Floor 63 Surrey Quays Shopping Centre Redriff Road  
 Flat 43 11 Maritime Street London  
 Flat 40 11 Maritime Street London  
 Flat 37 11 Maritime Street London  
 Flat 19 29 Surrey Quays Road London  
 Flat 17 29 Surrey Quays Road London  
 Flat 14 29 Surrey Quays Road London  
 Flat 11 29 Surrey Quays Road London  
 Flat 8 29 Surrey Quays Road London  
 Flat 5 29 Surrey Quays Road London  
 Flat 2 29 Surrey Quays Road London  
 Flat 69 11 Maritime Street London  
 Flat 66 11 Maritime Street London  
 Flat 63 11 Maritime Street London  
 Flat 60 11 Maritime Street London  
 Flat 34 11 Maritime Street London  
 Flat 31 11 Maritime Street London  
 Flat 28 11 Maritime Street London  
 Flat 25 11 Maritime Street London  
 Flat 22 11 Maritime Street London  
 Flat 19 11 Maritime Street London

95 Montreal House Surrey Quays Road  
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92 Montreal House Surrey Quays Road  
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90 Montreal House Surrey Quays Road  
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87 Montreal House Surrey Quays Road  
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84 Montreal House Surrey Quays Road  
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82 Montreal House Surrey Quays Road  
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18 Montreal House Surrey Quays Road  
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15 Montreal House Surrey Quays Road  
London  
13 Montreal House Surrey Quays Road  
London  
10 Montreal House Surrey Quays Road  
London  
7 Montreal House Surrey Quays Road  
London  
5 Montreal House Surrey Quays Road  
London  
2 Montreal House Surrey Quays Road  
London  
Unit B Montreal House Surrey Quays  
Road  
168 Montreal House Surrey Quays Road  
London  
166 Montreal House Surrey Quays Road  
London  
163 Montreal House Surrey Quays Road  
London  
161 Montreal House Surrey Quays Road  
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158 Montreal House Surrey Quays Road  
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155 Montreal House Surrey Quays Road  
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153 Montreal House Surrey Quays Road  
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150 Montreal House Surrey Quays Road  
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145 Montreal House Surrey Quays Road  
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139 Montreal House Surrey Quays Road  
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137 Montreal House Surrey Quays Road

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134 Montreal House Surrey Quays Road  
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131 Montreal House Surrey Quays Road  
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129 Montreal House Surrey Quays Road  
London  
126 Montreal House Surrey Quays Road  
London  
124 Montreal House Surrey Quays Road  
London  
Fattie Arbuckles The Mast Leisure Park  
Teredo Street  
27 Surrey Quays Shopping Centre  
Redriff Road London  
Uci Cinema 2 The Mast Leisure Park  
Teredo Street  
National Halal Centre Surrey Quays  
Shopping Centre Redriff Road  
Gala Bingo The Mast Leisure Park  
Teredo Street  
The Mast Leisure Park Teredo Street  
London  
49-51 Surrey Quays Shopping Centre  
Redriff Road London  
47 Surrey Quays Shopping Centre  
Redriff Road London  
43 Surrey Quays Shopping Centre  
Redriff Road London  
39-41 Surrey Quays Shopping Centre  
Redriff Road London  
33 Surrey Quays Shopping Centre  
Redriff Road London  
24-26 Surrey Quays Shopping Centre  
Redriff Road London  
21-23 Surrey Quays Shopping Centre  
Redriff Road London  
17 Surrey Quays Shopping Centre  
Redriff Road London  
15 Surrey Quays Shopping Centre  
Redriff Road London  
8 Surrey Quays Shopping Centre Redriff  
Road London  
3 Teredo Street London Southwark  
31 Surrey Quays Shopping Centre  
Redriff Road London  
25 Surrey Quays Shopping Centre  
Redriff Road London  
29 Surrey Quays Shopping Centre

Redriff Road London	London
45C Surrey Quays Shopping Centre	60 Montreal House Surrey Quays Road
Redriff Road London	London
Flat 2 1 Teredo Street London	555 Giverny House Water Gardens
First Floor 61 Surrey Quays Shopping	Square London
Centre Redriff Road	57 Montreal House Surrey Quays Road
First Floor 57 Surrey Quays Shopping	London
Centre Redriff Road	54 Montreal House Surrey Quays Road
Outside Surrey Quays Shopping Centre	London
Redriff Road	52 Montreal House Surrey Quays Road
53 Surrey Quays Shopping Centre	London
Redriff Road London	49 Montreal House Surrey Quays Road
Sushi Momoda Surrey Quays Shopping	London
Centre Redriff Road	46 Montreal House Surrey Quays Road
36 Surrey Quays Shopping Centre	London
Redriff Road London	43 Montreal House Surrey Quays Road
38-40 Surrey Quays Shopping Centre	London
Redriff Road London	41 Montreal House Surrey Quays Road
Pizza 1889 Outside Surrey Quays	London
Shopping Centre Redriff Road	35 Montreal House Surrey Quays Road
Best Fast Food Ltd Surrey Quays	London
Shopping Centre Redriff Road	30 Montreal House Surrey Quays Road
Outside Red Shipping Container Surrey	London
Quays Shopping Centre Redriff Road	27 Montreal House Surrey Quays Road
121 Montreal House Surrey Quays Road	London
London	25 Montreal House Surrey Quays Road
119 Montreal House Surrey Quays Road	London
London	22 Montreal House Surrey Quays Road
116 Montreal House Surrey Quays Road	London
London	Flat 61 Toronto House Surrey Quays
113 Montreal House Surrey Quays Road	Road
London	Flat 59 Toronto House Surrey Quays
111 Montreal House Surrey Quays Road	Road
London	Flat 56 Toronto House Surrey Quays
106 Montreal House Surrey Quays Road	Road
London	Flat 53 Toronto House Surrey Quays
100 Montreal House Surrey Quays Road	Road
London	Flat 51 Toronto House Surrey Quays
98 Montreal House Surrey Quays Road	Road
London	Flat 48 Toronto House Surrey Quays
76 Montreal House Surrey Quays Road	Road
London	Flat 46 Toronto House Surrey Quays
73 Montreal House Surrey Quays Road	Road
London	Flat 43 Toronto House Surrey Quays
71 Montreal House Surrey Quays Road	Road
London	Flat 41 Toronto House Surrey Quays
65 Montreal House Surrey Quays Road	Road
London	Flat 38 Toronto House Surrey Quays
63 Montreal House Surrey Quays Road	Road

Flat 35 Toronto House Surrey Quays  
Road  
553 Giverny House Water Gardens  
Square London  
654 Pavillion House Water Gardens  
Square London  
Flat 30 Toronto House Surrey Quays  
Road  
Flat 25 Toronto House Surrey Quays  
Road  
Flat 23 Toronto House Surrey Quays

**Re-consultation:**

152

Road  
Flat 20 Toronto House Surrey Quays  
Road  
Flat 17 Toronto House Surrey Quays  
Road  
Flat 15 Toronto House Surrey Quays  
Road  
Flat 12 Toronto House Surrey Quays  
Road

**APPENDIX 5****Consultation responses received - Planning Permission 23/AP/0562****Internal services**

formal consultation and response to Pol  
 Community Infrastructure Levy Team  
 Design and Conservation Team [Formal]  
 Local Economy  
 Ecology  
 Environmental Protection  
 Highways Development and Management  
 Urban Forester  
 Waste Management  
 Transport Policy

**Statutory and non-statutory organisations**

Environment Agency  
 London Fire & Emergency Planning Authori  
 London Underground  
 Metropolitan Police Service (Designing O  
 Transport for London  
 Thames Water  
 Historic England

**Neighbour and local groups consulted:**

Flat572, Giverny House Water Gardens Square London	York House 45 Seymour St London 9 Maritime Street London SE16 7FU
573 Giverny House Water Garden Square London	
Flat 56, 11 Maritime street London Se16 7fy	Via Email
18 Quebec Way London SE16 7ET Via Email	

**APPENDIX 1****Recommendation – Planning Permission 23/AP/0564**

This document shows the case officer's recommended decision for the application referred to below.

This document is not a decision notice for this application.

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<b>Applicant</b>	AIRE UK Canada Water GP PropCos Ltd, acting as general pa...	<b>Reg. Number</b>	23/AP/0564
<b>Application Type</b>	Approval of Reserved Matters		
<b>Recommendation</b>	APPROVE reserved matters	<b>Case Number</b>	468-C

**Draft of Decision Notice**

**reserved matters is APPROVED for the following development:**

Approval of Reserved Matters (access, appearance, landscaping, layout and scale) pursuant to outline planning permission 21/AP/2655 for the comprehensive commercial development of the Canada Water Dockside Masterplan site. Reserved Matters Approval sought for construction of Building A2 (maximum height 55.2m AOD) comprising the following uses; offices (Class E), retail (E) and food and beverage (E), with cycle parking, landscaping, loading bay, plant and associated works. This is as an application for subsequent consent accompanied by an Environmental Statement. Consequently the application is accompanied by a Statement of Conformity submitted pursuant to the Town and Country Planning (Environmental Impact Assessment) regulations 2017. This ES Statement of Conformity should be read in conjunction with the Canada Water Dockside Masterplan ES which can be viewed in full on the Councils website (21/AP/2655).

Units 1 And 4 Canada Water Retail Park London Southwark

**In accordance with application received on 1 March 2023 and Applicant's Drawing Nos.:**



## Existing Plans

## Proposed Plans

Plans - Proposed CWD-HWK-A2-XX-DR-A-104-XX PUBLIC TOILET LOCATION received 11/07/2023

Plans - Proposed 230606\_CW A2\_POROUS WIND MITIGATION SCREEN (004) received 11/07/2023

Plans - Proposed CWD-TLA-RMA-XX-RP-L-0002-XX\_P00 (004) Wind Mitigation Details received 11/07/2023

Plans - Proposed Level 7 Floorplan CWD-HWK-A2-XX-DR A 112 XX received 01/03/2023

Plans - Proposed Level 10 Floorplan CWD-HWK-A2-XX-DR A 115 XX received 01/03/2023

Plans - Proposed Level 01 Floorplan CWD-HWK-A2-XX-DR A 106 XX received 01/03/2023

Plans - Proposed Context Elevations CWD-HWK-A2-XX-DR A 200B XX received 01/03/2023

Plans - Proposed Level 2 Floorplan CWD-HWK-A2-XX-DR A 107 XX received 01/03/2023

Plans - Proposed Street Surface Plan Plot A and Maritime Street CWD TLA RMA XX DR L 3003 XX REV P00 received 01/03/2023

Plans - Proposed CWD-HWK-A2-XX-DR A 323 XX ENLARGED EXTERIOR WALL - GF SOUTH received 01/03/2023

Plans - Proposed Context Elevations CWD-HWK-A2-XX-DR A 200A XX received 01/03/2023

Plans - Proposed Level 3 Floorplan CWD-HWK-A2-XX-DR A 108 XX received 01/03/2023

Plans - Proposed CWD-HWK-A2-XX-DR A 330 XX ENLARGED EXTERIOR WALL - WT-1A & 1B received 01/03/2023

Plans - Proposed CWD-HWK-A2-XX-DR A 320 XX ENLARGED EXTERIOR WALL - GF WEST received 01/03/2023

Plans - Proposed CWD-HWK-A2-XX-DR A 116 XX LEVEL 10M FLOOR PLAN received 01/03/2023

Plans - Proposed CWD-HWK-A2-XX-DR-A 200 XX AXONOMETRIC VIEWS received 01/03/2023

Plans - Proposed CWD-HWK-A2-XX-DR A 114 XX LEVEL 9 FLOOR PLAN received 01/03/2023

Plans - Proposed CWD-HWK-A2-XX-DR A 332 XX - WT-4 & 6 received 01/03/2023	ENLARGED EXTERIOR WALL
Plans - Proposed CWD-HWK-A2-XX-DR A 203 XX 01/03/2023	NORTH ELEVATION received
Plans - Proposed CWD-HWK-A2-XX-DR A 331 XX - WT-2 & 3 received 01/03/2023	ENLARGED EXTERIOR WALL
Plans - Proposed CWD-HWK-A2-XX-DR-A 104 XX received 01/03/2023	GROUND FLOOR PLAN
Plans - Proposed CWD-HWK-A2-XX-DR A 301 XX EAST received 01/03/2023	BUILDING SECTION - WEST-
Plans - Proposed CWD-HWK-A2-XX-DR-A 335 XX received 01/03/2023	ENLARGED TERRACE
Plans - Proposed CWD-HWK-A2-XX-DR-A 202 XX 01/03/2023	WEST ELEVATION received
Plans - Proposed CWD-HWK-A2-XX-DR-A 109 XX received 01/03/2023	LEVEL 4 FLOOR PLAN
Plans - Proposed CWD-HWK-A2-XX-DR A 322 XX - GF EAST received 01/03/2023	ENLARGED EXTERIOR WALL
Plans - Proposed CWD-TLA-RMA-XX-DR-L-3002-XX REV P00 MASTERPLAN - PUBLIC REALM received 01/03/2023	LANDSCAPE
Plans - Proposed CWD-HWK-A2-XX-DR-A 321 XX - GF NORTH received 01/03/2023	ENLARGED EXTERIOR WALL
Plans - Proposed CWD-HWK-A2-XX-DR-A 110 XX received 01/03/2023	LEVEL 5 FLOOR PLAN
Plans - Proposed CWD-HWK-A2-XX-DR-A 105 XX received 01/03/2023	MEZZANINE FLOOR PLAN
Plans - Proposed CWD-TLA-A2-XX-DR-L-5003-XX REV P00 PLAN - PLOT A2 received 01/03/2023	TREE PLANTING
Plans - Proposed CWD-HWK-A2-XX-DR-A 111 XX received 01/03/2023	LEVEL 6 FLOOR PLAN
Plans - Proposed CWD-HWK-A2-XX-DR-A 113-XX received 01/03/2023	LEVEL 8 FLOOR PLAN
Plans - Proposed CWD-HWK-A2-XX-DR-A 205 XX 01/03/2023	SOUTH ELEVATION received
Plans - Proposed CWD-HWK-A2-XX-DR-A 117 XX 01/03/2023	ROOF PLAN received
Plans - Proposed CWD-HWK-A2-XX-DR-A 300 XX SOUTH received 01/03/2023	BUILDING SECTION - NORTH-

Plans - Proposed CWD-TLA-A2-XX-DR-L-3006-XX REV P00 SURFACE  
FINISH PLAN - PLOT A2 received 01/03/2023

Plans - Proposed CWD-TLA-A2-XX-DR-L-7003-XX REV P00 PUBLIC REALM  
SECTIONS received 01/03/2023

Plans - Proposed CWD-HWK-A2-XX-DR-A 104-XX GROUND FLOOR PLAN  
received 01/03/2023

Plans - Proposed CWD-TLA-A2-XX-DR-L-5003-XX REV P00 TREE PLANTING  
PLAN - PLOT A2 received 01/03/2023

Plans - Proposed CWD-TLA-RMA-XX-DR-L-3002-XX REV P00 LANDSCAPE  
MASTERPLAN - PUBLIC REALM received 01/03/2023

#### Other Documents

Energy statement 5161\_CWA2\_WLCA REVIEW FOR ARUP\_P01 received  
11/07/2023

Energy statement 60948\_CWD A2\_PART L  
2013\_GLA CARBON EMISSION REPORTING SPREADSHEET\_V1.2\_2020  
received 11/07/2023

Energy statement 60948\_CWD A2\_PART L  
2021\_GLA CARBON EMISSION REPORTING SPREADSHEET\_V2.0\_0. received  
11/07/2023

Energy statement 60948\_ENERGY STRATEGY\_REV03 PLOT A2 received  
11/07/2023

Energy statement CWA2\_GLA CES SPREADSHEET\_FEB 2023 received  
11/07/2023

Energy statement CWA2\_GLA WLCA SPREADSHEET\_DETAILED\_22 FEB 2023  
received 11/07/2023

Sustainability statement CWD-ARU-A2-XX-RP-Y-0001-P02 SUSTAINABILITY  
STATEMENT A2 received 11/07/2023

Energy statement CWD-ARU-A2-XX-RP-Y-0002-P01 WLC ASSESSMENT A2  
received 11/07/2023

Energy statement CWD-ARU-A2-XX-RP-Y-0003-P02 CES PLOT A2. received  
11/07/2023

Tree survey and assessment AIA Addendum CWD-TMA-RMA-XX-RP-Z-0002-XX-P02  
received 12/06/2023

Document Equalities Impact Assessment CWD-TRI-RMA-X X-RP-Y-0002-X X-P00  
received 01/03/2023

Document SCI CWD-KAN-RMA-XX-RP-Y-0001-XX-P00 received 01/03/2023

Document Sustainability Assessment A2 CWD-ARU-A2-XX-RP-Y-0001-XX-P00 received 01/03/2023

Fire Statement C W D -OF R-A 2-X X-R P -Y-000 1-X X-P 00 received 01/03/2023

Design and access statement CWD-HWK-A2-XX-RP-A-0001-XX-P00 received 01/03/2023

Document Dev Consultation Charter CWD-KAN-DE-XX-RP-Y-0002-XX-P00 received 01/03/2023

Document BNG Assessment C WD-SLO-RMA-X X-RP-Z-000 1-P00 received 01/03/2023

Waste Management Strategy CWD-WSP-RMA-XX-RP-Y-0003-XX-P00 received 01/03/2023

Tree survey and assessment Arbs Method Statement C WD-TMA-RMA-X X-RP-Z-000 1-X X-P00 received 01/03/2023

Document DSP CWD-WSP-RMA-XX-RP-Y-0001-XX-P00 received 01/03/2023

Document Planning Compliance Report C W D -DP 9-R M A-X X-R P -T-0002-X X-P 01 received 14/07/2023

Transport assessment/statement CWD-WSP-RMA-XX-RP-Y-0002-XX-P00 received 01/03/2023

Planning statement CWD -DP9-RMA-XX-RP -T-0001-XX-P 00 received 01/03/2023

Travel plan CWD-WSP-A2-XX-RP-Y-0001-XX-P00 received 01/03/2023

Planning statement CWD - RMA PLANNING STATEMENT V2. received 14/07/2023

Document CWD\_PLOT A RMA EIA STATEMENT OF CONFORMITY JULY 2023 received 14/07/2023

Landscaping and open space statement C W D-T L A-R M A-X X-R P -L-000 1-X X-P 00 received 01/03/2023

## **Informatives**

- 1 Paragraph 3.12.9 of Policy D12 explains that Fire Statements should be produced by someone who is:

"third-party independent and suitably-qualified" The council considers this to be a qualified engineer with relevant experience in fire safety, such as a chartered engineer registered with the Engineering Council by the Institution of Fire Engineers, or a suitably qualified and competent professional with the demonstrable experience to address the complexity of the design being proposed. This should be evidenced in the fire statement. The council accepts Fire Statements in good faith on that basis. The duty to identify fire risks and

hazards in premises and to take appropriate action lies solely with the developer.

The fire risk assessment/statement covers matters required by planning policy. This is in no way a professional technical assessment of the fire risks presented by the development. The legal responsibility and liability lies with the 'responsible person'. The responsible person being the person who prepares the fire risk assessment/statement not planning officers who make planning decisions.

- 2 The applicant is reminded that in order to discharge into the Dock, consent will be required from all relevant land owners. Should such consent not be granted the Applicant will be required to connect to the sewer. In any event it will be necessary to submit a formal application to discharge Condition 17 of 21/AP/2655.

As part of this formal submission for discharge of Condition 17 the applicant will be required to demonstrate that any water proposed to be discharged into the Dock meets CIRIA Suds standards or in the event that discharge to the sewer is required the strategy to be submitted under Condition 17 will need to fully demonstrate compliance with maximising SUDs onsite and meeting policy restricted runoff rates

## APPENDIX 3

## Relevant planning history - Planning Permission 23/AP/0564

Reference and Proposal	Status
<p><b>21/AP/2655</b></p> <p>Outline planning permission (all matters reserved) for a commercial masterplan containing offices and other complementary town centre uses with no housing. The proposals involve the demolition of all buildings and structures and the comprehensive redevelopment of the site to provide three Development Plots (A1, A2 and B) which will contain three Buildings (A1, A2 and B) above ground with basements. The proposed land uses are: offices (Class E), retail/professional services/food and drink (E), learning and non-residential institutions/local community (F1/F2), medical or health (E) and indoor sport, recreation or fitness (E). - Works of hard and soft landscaping are proposed to create a series of new and improved public realm spaces within the site. This will include the replacement of some trees and the planting of additional new trees. - New vehicular access points are proposed to be created from Surrey Quays Road and Canada Street, along with other incidental works.</p>	<p>GRANTED - Major Application 25/01/2023</p>



## Consultation undertaken - Planning Permission 23/AP/0564

**Site notice date:** 13/03/2023

**Press notice date:** 16/03/2023

**Case officer site visit date:** n/a

**Neighbour consultation letters sent:** 09/03/2023

### Internal services consulted

formal consultation and response to Pol  
 Archaeology  
 Section 106 Team  
 Design and Conservation Team [Formal]  
 Local Economy  
 Ecology  
 Environmental Protection  
 Highways Development and Management  
 Flood Risk Management & Urban Drainage  
 Transport Policy  
 Urban Forester  
 Waste Management  
 Community Infrastructure Levy Team

### Statutory and non-statutory organisations

Environment Agency  
 Historic England  
 London Fire & Emergency Planning Authori  
 London Underground  
 Metropolitan Police Service (Designing O  
 Transport for London  
 Thames Water

### Neighbour and local groups consulted:

Flat 66 29 Surrey Quays Road London	15 Maritime Street London Southwark
Flat 37 29 Surrey Quays Road London	The League Of Adventure Canada Street
Flat 20 29 Surrey Quays Road London	London
Flat 3 29 Surrey Quays Road London	Flat 50 29 Surrey Quays Road London
Flat 30 11 Maritime Street London	Flat 49 7 Maritime Street London
Flat 59 7 Maritime Street London	Flat 46 7 Maritime Street London
Flat 42 7 Maritime Street London	622 Pavillion House Water Gardens
Flat 12 7 Maritime Street London	Square London
Flat 24 5 Maritime Street London	673 Pavillion House Water Gardens
Flat 8 5 Maritime Street London	Square London

624 Pavillion House Water Gardens  
 Square London  
 Flat 23 5 Maritime Street London  
 Flat 5 5 Maritime Street London  
 23 Maritime Street London Southwark  
 Flat 26 29 Surrey Quays Road London  
 Flat 6 29 Surrey Quays Road London  
 Flat 56 11 Maritime Street London  
 Flat 36 11 Maritime Street London  
 Flat 18 11 Maritime Street London  
 Flat 60 7 Maritime Street London  
 Flat 41 7 Maritime Street London  
 Flat 35 7 Maritime Street London  
 Flat 15 7 Maritime Street London  
 Flat 16 11 Maritime Street London  
 423 Heligan House Water Gardens  
 Square London  
 Flat 52 11 Maritime Street London  
 525 Giverny House Water Gardens  
 Square London  
 452 Heligan House Water Gardens  
 Square London  
 Flat 68 29 Surrey Quays Road London  
 Flat 48 29 Surrey Quays Road London  
 558 Giverny House Water Gardens  
 Square London  
 Flat 22 29 Surrey Quays Road London  
 Flat 16 7 Maritime Street London  
 528 Giverny House Water Gardens  
 Square London  
 503 Giverny House Water Gardens  
 Square London  
 434 Heligan House Water Gardens  
 Square London  
 464 Heligan House Water Gardens  
 Square London  
 Flat 1 5 Maritime Street London  
 Flat 49 29 Surrey Quays Road London  
 East Warehouse Canada Water Retail  
 Park Surrey Quays Road  
 413 Heligan House Water Gardens  
 Square London  
 533 Giverny House Water Gardens  
 Square London  
 Flat 61 29 Surrey Quays Road London  
 Flat 19 7 Maritime Street London  
 44 Surrey Quays Shopping Centre  
 Redriff Road London  
 53-55 Surrey Quays Shopping Centre  
 Redriff Road London  
 6 Surrey Quays Shopping Centre Redriff  
 Road London  
 30-34 Surrey Quays Shopping Centre  
 Redriff Road London  
 16 Surrey Quays Shopping Centre  
 Redriff Road London  
 11-13 Surrey Quays Shopping Centre  
 Redriff Road London  
 20 Surrey Quays Shopping Centre  
 Redriff Road London  
 Surrey Quays Leisure Park Surrey  
 Quays Road London  
 First Floor 63 Surrey Quays Shopping  
 Centre Redriff Road  
 Fattie Arbuckles The Mast Leisure Park  
 Teredo Street  
 Uci Cinema 2 The Mast Leisure Park  
 Teredo Street  
 National Halal Centre Surrey Quays  
 Shopping Centre Redriff Road  
 Gala Bingo The Mast Leisure Park  
 Teredo Street  
 The Mast Leisure Park Teredo Street  
 London  
 49-51 Surrey Quays Shopping Centre  
 Redriff Road London  
 47 Surrey Quays Shopping Centre  
 Redriff Road London  
 43 Surrey Quays Shopping Centre  
 Redriff Road London  
 39-41 Surrey Quays Shopping Centre  
 Redriff Road London  
 33 Surrey Quays Shopping Centre  
 Redriff Road London  
 24-26 Surrey Quays Shopping Centre  
 Redriff Road London  
 21-23 Surrey Quays Shopping Centre  
 Redriff Road London  
 17 Surrey Quays Shopping Centre  
 Redriff Road London  
 15 Surrey Quays Shopping Centre  
 Redriff Road London  
 8 Surrey Quays Shopping Centre Redriff  
 Road London  
 3 Teredo Street London Southwark  
 31 Surrey Quays Shopping Centre

Redriff Road London	Flat 22 7 Maritime Street London
25 Surrey Quays Shopping Centre	Flat 13 7 Maritime Street London
Redriff Road London	Flat 10 7 Maritime Street London
29 Surrey Quays Shopping Centre	Flat 7 7 Maritime Street London
Redriff Road London	Flat 4 7 Maritime Street London
45C Surrey Quays Shopping Centre	Flat 1 7 Maritime Street London
Redriff Road London	Flat 57 11 Maritime Street London
First Floor 61 Surrey Quays Shopping	Flat 58 11 Maritime Street London
Centre Redriff Road	Flat 14 11 Maritime Street London
First Floor 57 Surrey Quays Shopping	Flat 44 29 Surrey Quays Road London
Centre Redriff Road	Flat 70 29 Surrey Quays Road London
Outside Surrey Quays Shopping Centre	Flat 67 29 Surrey Quays Road London
Redriff Road	Flat 64 29 Surrey Quays Road London
Sushi Momoda Surrey Quays Shopping	Flat 43 7 Maritime Street London
Centre Redriff Road	Flat 40 7 Maritime Street London
36 Surrey Quays Shopping Centre	Flat 13 11 Maritime Street London
Redriff Road London	Flat 10 11 Maritime Street London
38-40 Surrey Quays Shopping Centre	Flat 7 11 Maritime Street London
Redriff Road London	Flat 4 11 Maritime Street London
Pizza 1889 Outside Surrey Quays	Flat 1 11 Maritime Street London
Shopping Centre Redriff Road	Flat 58 7 Maritime Street London
Best Fast Food Ltd Surrey Quays	Flat 55 7 Maritime Street London
Shopping Centre Redriff Road	Flat 52 7 Maritime Street London
Outside Red Shipping Container Surrey	Flat 7 5 Maritime Street London
Quays Shopping Centre Redriff Road	Flat 4 5 Maritime Street London
651 Pavillion House Water Gardens	19 Maritime Street London Southwark
Square London	13 Maritime Street London Southwark
633 Pavillion House Water Gardens	Flat 25 5 Maritime Street London
Square London	Flat 22 5 Maritime Street London
Flat 13 5 Maritime Street London	Flat 19 5 Maritime Street London
544 Giverny House Water Gardens	Flat 16 5 Maritime Street London
Square London	Flat 10 5 Maritime Street London
Flat 59 29 Surrey Quays Road London	Flat 54 11 Maritime Street London
Flat 56 29 Surrey Quays Road London	Flat 49 11 Maritime Street London
Flat 53 29 Surrey Quays Road London	Flat 46 11 Maritime Street London
Flat 47 29 Surrey Quays Road London	Flat 43 11 Maritime Street London
Flat 41 29 Surrey Quays Road London	Flat 40 11 Maritime Street London
Flat 39 29 Surrey Quays Road London	Flat 37 11 Maritime Street London
Flat 36 29 Surrey Quays Road London	Flat 19 29 Surrey Quays Road London
Flat 33 29 Surrey Quays Road London	Flat 17 29 Surrey Quays Road London
Flat 30 29 Surrey Quays Road London	Flat 14 29 Surrey Quays Road London
Flat 27 29 Surrey Quays Road London	Flat 11 29 Surrey Quays Road London
Flat 24 29 Surrey Quays Road London	Flat 8 29 Surrey Quays Road London
Flat 37 7 Maritime Street London	Flat 5 29 Surrey Quays Road London
Flat 34 7 Maritime Street London	Flat 2 29 Surrey Quays Road London
Flat 31 7 Maritime Street London	Flat 69 11 Maritime Street London
Flat 28 7 Maritime Street London	Flat 66 11 Maritime Street London
Flat 25 7 Maritime Street London	Flat 63 11 Maritime Street London

Flat 60 11 Maritime Street London	Square London
Flat 34 11 Maritime Street London	433 Heligan House Water Gardens
Flat 31 11 Maritime Street London	Square London
Flat 28 11 Maritime Street London	426 Heligan House Water Gardens
Flat 25 11 Maritime Street London	Square London
Flat 22 11 Maritime Street London	416 Heligan House Water Gardens
Flat 19 11 Maritime Street London	Square London
555 Giverny House Water Gardens	411 Heligan House Water Gardens
Square London	Square London
553 Giverny House Water Gardens	403 Heligan House Water Gardens
Square London	Square London
654 Pavillion House Water Gardens	471 Heligan House Water Gardens
Square London	Square London
643 Pavillion House Water Gardens	672 Pavillion House Water Gardens
Square London	Square London
635 Pavillion House Water Gardens	663 Pavillion House Water Gardens
Square London	Square London
625 Pavillion House Water Gardens	512 Giverny House Water Gardens
Square London	Square London
614 Pavillion House Water Gardens	505 Giverny House Water Gardens
Square London	Square London
611 Pavillion House Water Gardens	502 Giverny House Water Gardens
Square London	Square London
603 Pavillion House Water Gardens	572 Giverny House Water Gardens
Square London	Square London
691 Pavillion House Water Gardens	561 Giverny House Water Gardens
Square London	Square London
682 Pavillion House Water Gardens	514 Giverny House Water Gardens
Square London	Square London
644 Pavillion House Water Gardens	463 Heligan House Water Gardens
Square London	Square London
601 Pavillion House Water Gardens	436 Heligan House Water Gardens
Square London	Square London
563 Giverny House Water Gardens	401 Heligan House Water Gardens
Square London	Square London
566 Giverny House Water Gardens	661 Pavillion House Water Gardens
Square London	Square London
531 Giverny House Water Gardens	551 Giverny House Water Gardens
Square London	Square London
523 Giverny House Water Gardens	547 Giverny House Water Gardens
Square London	Square London
515 Giverny House Water Gardens	541 Giverny House Water Gardens
Square London	Square London
446 Heligan House Water Gardens	536 Giverny House Water Gardens
Square London	Square London
443 Heligan House Water Gardens	Flat 57 29 Surrey Quays Road London
Square London	Flat 55 29 Surrey Quays Road London
435 Heligan House Water Gardens	Flat 54 29 Surrey Quays Road London

Flat 52 29 Surrey Quays Road London	Flat 32 11 Maritime Street London
Flat 51 29 Surrey Quays Road London	Flat 29 11 Maritime Street London
Flat 46 29 Surrey Quays Road London	Flat 27 11 Maritime Street London
Flat 45 29 Surrey Quays Road London	Flat 26 11 Maritime Street London
Flat 43 29 Surrey Quays Road London	Flat 24 11 Maritime Street London
Flat 42 29 Surrey Quays Road London	Flat 23 11 Maritime Street London
Flat 40 29 Surrey Quays Road London	Flat 21 11 Maritime Street London
Flat 20 7 Maritime Street London	Flat 20 11 Maritime Street London
Flat 18 7 Maritime Street London	Flat 17 11 Maritime Street London
Flat 17 7 Maritime Street London	Flat 15 11 Maritime Street London
Flat 14 7 Maritime Street London	Flat 12 11 Maritime Street London
Flat 11 7 Maritime Street London	Flat 11 11 Maritime Street London
Flat 9 7 Maritime Street London	Flat 9 11 Maritime Street London
Flat 8 7 Maritime Street London	Flat 8 11 Maritime Street London
Flat 6 7 Maritime Street London	Flat 6 11 Maritime Street London
Flat 5 7 Maritime Street London	Flat 5 11 Maritime Street London
Flat 3 7 Maritime Street London	Flat 3 11 Maritime Street London
Flat 2 7 Maritime Street London	Flat 2 11 Maritime Street London
Flat 27 5 Maritime Street London	Flat 57 7 Maritime Street London
Flat 26 5 Maritime Street London	Flat 56 7 Maritime Street London
Flat 21 5 Maritime Street London	Flat 54 7 Maritime Street London
Flat 20 5 Maritime Street London	Flat 53 7 Maritime Street London
Flat 18 5 Maritime Street London	Flat 51 7 Maritime Street London
Flat 17 5 Maritime Street London	Flat 50 7 Maritime Street London
Flat 15 5 Maritime Street London	Flat 48 7 Maritime Street London
Flat 14 5 Maritime Street London	Flat 47 7 Maritime Street London
Flat 12 5 Maritime Street London	Flat 45 7 Maritime Street London
Flat 11 5 Maritime Street London	Flat 44 7 Maritime Street London
Flat 9 5 Maritime Street London	Flat 39 7 Maritime Street London
Flat 6 5 Maritime Street London	Flat 38 7 Maritime Street London
Flat 3 5 Maritime Street London	21 Maritime Street London Southwark
Flat 2 5 Maritime Street London	17 Maritime Street London Southwark
25 Maritime Street London Southwark	3 Maritime Street London Southwark
Flat 42 11 Maritime Street London	1 Maritime Street London Southwark
Flat 41 11 Maritime Street London	Flat 38 29 Surrey Quays Road London
Flat 39 11 Maritime Street London	Flat 35 29 Surrey Quays Road London
Flat 36 7 Maritime Street London	Flat 34 29 Surrey Quays Road London
Flat 33 7 Maritime Street London	Flat 32 29 Surrey Quays Road London
Flat 32 7 Maritime Street London	Flat 31 29 Surrey Quays Road London
Flat 30 7 Maritime Street London	Flat 29 29 Surrey Quays Road London
Flat 27 7 Maritime Street London	Flat 28 29 Surrey Quays Road London
Flat 26 7 Maritime Street London	Flat 25 29 Surrey Quays Road London
Flat 24 7 Maritime Street London	Flat 23 29 Surrey Quays Road London
Flat 23 7 Maritime Street London	Flat 21 29 Surrey Quays Road London
Flat 21 7 Maritime Street London	Flat 18 29 Surrey Quays Road London
Flat 38 11 Maritime Street London	Flat 16 29 Surrey Quays Road London
Flat 35 11 Maritime Street London	Flat 15 29 Surrey Quays Road London
Flat 33 11 Maritime Street London	Flat 13 29 Surrey Quays Road London

Flat 12 29 Surrey Quays Road London	Square London
Flat 10 29 Surrey Quays Road London	522 Giverny House Water Gardens
Flat 9 29 Surrey Quays Road London	Square London
Flat 7 29 Surrey Quays Road London	521 Giverny House Water Gardens
Flat 4 29 Surrey Quays Road London	Square London
Flat 1 29 Surrey Quays Road London	513 Giverny House Water Gardens
Flat 70 11 Maritime Street London	Square London
Flat 68 11 Maritime Street London	511 Giverny House Water Gardens
Flat 67 11 Maritime Street London	Square London
Flat 65 11 Maritime Street London	504 Giverny House Water Gardens
Flat 64 11 Maritime Street London	Square London
Flat 62 11 Maritime Street London	574 Giverny House Water Gardens
Flat 61 11 Maritime Street London	Square London
Flat 59 11 Maritime Street London	573 Giverny House Water Gardens
Flat 55 11 Maritime Street London	Square London
Flat 53 11 Maritime Street London	571 Giverny House Water Gardens
Flat 51 11 Maritime Street London	Square London
Flat 50 11 Maritime Street London	542 Giverny House Water Gardens
Flat 48 11 Maritime Street London	Square London
Flat 47 11 Maritime Street London	501 Giverny House Water Gardens
Flat 71 29 Surrey Quays Road London	Square London
Flat 69 29 Surrey Quays Road London	454 Heligan House Water Gardens
Flat 65 29 Surrey Quays Road London	Square London
Flat 63 29 Surrey Quays Road London	453 Heligan House Water Gardens
Flat 62 29 Surrey Quays Road London	Square London
Flat 60 29 Surrey Quays Road London	451 Heligan House Water Gardens
Flat 58 29 Surrey Quays Road London	Square London
Flat 45 11 Maritime Street London	613 Pavillion House Water Gardens
Flat 44 11 Maritime Street London	Square London
548 Giverny House Water Gardens	612 Pavillion House Water Gardens
Square London	Square London
546 Giverny House Water Gardens	604 Pavillion House Water Gardens
Square London	Square London
545 Giverny House Water Gardens	692 Pavillion House Water Gardens
Square London	Square London
538 Giverny House Water Gardens	683 Pavillion House Water Gardens
Square London	Square London
537 Giverny House Water Gardens	681 Pavillion House Water Gardens
Square London	Square London
535 Giverny House Water Gardens	665 Pavillion House Water Gardens
Square London	Square London
534 Giverny House Water Gardens	623 Pavillion House Water Gardens
Square London	Square London
532 Giverny House Water Gardens	564 Giverny House Water Gardens
Square London	Square London
526 Giverny House Water Gardens	562 Giverny House Water Gardens
Square London	Square London
524 Giverny House Water Gardens	554 Giverny House Water Gardens



Square London  
 552 Giverny House Water Gardens  
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 621 Pavillion House Water Gardens  
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 615 Pavillion House Water Gardens  
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 Unit 4 Canada Water Retail Park Surrey  
 Quays Road  
 Unit 1 Canada Water Retail Park Surrey  
 Quays Road  
 344 Eden House Water Gardens Square  
 London  
 313 Eden House Water Gardens Square  
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 354 Eden House Water Gardens Square  
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 326 Eden House Water Gardens Square  
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 302 Eden House Water Gardens Square  
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 345 Eden House Water Gardens Square  
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 342 Eden House Water Gardens Square  
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 335 Eden House Water Gardens Square  
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 332 Eden House Water Gardens Square  
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 325 Eden House Water Gardens Square  
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 322 Eden House Water Gardens Square  
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 315 Eden House Water Gardens Square

London	London
312 Eden House Water Gardens Square	Rear Of Room Quays Unit A2 Toronto
London	House Surrey Quays Road
304 Eden House Water Gardens Square	47 Montreal House Surrey Quays Road
London	London
351 Eden House Water Gardens Square	29 Montreal House Surrey Quays Road
London	London
301 Eden House Water Gardens Square	12 Montreal House Surrey Quays Road
London	London
Unit 300 Eden House Water Gardens	Flat 62 Toronto House Surrey Quays
Square	Road
346 Eden House Water Gardens Square	Flat 44 Toronto House Surrey Quays
London	Road
343 Eden House Water Gardens Square	Flat 6 Toronto House Surrey Quays
London	Road
341 Eden House Water Gardens Square	156 Montreal House Surrey Quays Road
London	London
334 Eden House Water Gardens Square	138 Montreal House Surrey Quays Road
London	London
333 Eden House Water Gardens Square	Flat 2 Toronto House Surrey Quays
London	Road
331 Eden House Water Gardens Square	109 Montreal House Surrey Quays Road
London	London
324 Eden House Water Gardens Square	72 Montreal House Surrey Quays Road
London	London
323 Eden House Water Gardens Square	Flat 28 Toronto House Surrey Quays
London	Road
316 Eden House Water Gardens Square	Unit A2 Toronto House Surrey Quays
London	Road
314 Eden House Water Gardens Square	Flat 33 Toronto House Surrey Quays
London	Road
311 Eden House Water Gardens Square	Flat 57 Toronto House Surrey Quays
London	Road
305 Eden House Water Gardens Square	Flat 36 Toronto House Surrey Quays
London	Road
303 Eden House Water Gardens Square	Flat 14 Toronto House Surrey Quays
London	Road
362 Eden House Water Gardens Square	122 Montreal House Surrey Quays Road
London	London
361 Eden House Water Gardens Square	101 Montreal House Surrey Quays Road
London	London
353 Eden House Water Gardens Square	81 Montreal House Surrey Quays Road
London	London
352 Eden House Water Gardens Square	61 Montreal House Surrey Quays Road
London	London
336 Eden House Water Gardens Square	58 Montreal House Surrey Quays Road
London	London
321 Eden House Water Gardens Square	39 Montreal House Surrey Quays Road

London	London
19 Montreal House Surrey Quays Road	10 Montreal House Surrey Quays Road
London	London
167 Montreal House Surrey Quays Road	7 Montreal House Surrey Quays Road
London	London
146 Montreal House Surrey Quays Road	5 Montreal House Surrey Quays Road
London	London
127 Montreal House Surrey Quays Road	2 Montreal House Surrey Quays Road
London	London
108 Montreal House Surrey Quays Road	Unit B Montreal House Surrey Quays
London	Road
79 Montreal House Surrey Quays Road	168 Montreal House Surrey Quays Road
London	London
68 Montreal House Surrey Quays Road	166 Montreal House Surrey Quays Road
London	London
142 Montreal House Surrey Quays Road	163 Montreal House Surrey Quays Road
London	London
Flat 24 Toronto House Surrey Quays	161 Montreal House Surrey Quays Road
Road	London
89 Montreal House Surrey Quays Road	158 Montreal House Surrey Quays Road
London	London
33 Montreal House Surrey Quays Road	155 Montreal House Surrey Quays Road
London	London
103 Montreal House Surrey Quays Road	153 Montreal House Surrey Quays Road
London	London
Flat 3 Toronto House Surrey Quays	150 Montreal House Surrey Quays Road
Road	London
38 Montreal House Surrey Quays Road	145 Montreal House Surrey Quays Road
London	London
147 Montreal House Surrey Quays Road	139 Montreal House Surrey Quays Road
London	London
95 Montreal House Surrey Quays Road	137 Montreal House Surrey Quays Road
London	London
92 Montreal House Surrey Quays Road	134 Montreal House Surrey Quays Road
London	London
90 Montreal House Surrey Quays Road	131 Montreal House Surrey Quays Road
London	London
87 Montreal House Surrey Quays Road	129 Montreal House Surrey Quays Road
London	London
84 Montreal House Surrey Quays Road	126 Montreal House Surrey Quays Road
London	London
82 Montreal House Surrey Quays Road	124 Montreal House Surrey Quays Road
London	London
18 Montreal House Surrey Quays Road	121 Montreal House Surrey Quays Road
London	London
15 Montreal House Surrey Quays Road	119 Montreal House Surrey Quays Road
London	London
13 Montreal House Surrey Quays Road	116 Montreal House Surrey Quays Road

London	Road
113 Montreal House Surrey Quays Road	Flat 59 Toronto House Surrey Quays Road
London	Road
111 Montreal House Surrey Quays Road	Flat 56 Toronto House Surrey Quays Road
London	Road
106 Montreal House Surrey Quays Road	Flat 53 Toronto House Surrey Quays Road
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100 Montreal House Surrey Quays Road	Flat 51 Toronto House Surrey Quays Road
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98 Montreal House Surrey Quays Road	Flat 48 Toronto House Surrey Quays Road
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76 Montreal House Surrey Quays Road	Flat 46 Toronto House Surrey Quays Road
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73 Montreal House Surrey Quays Road	Flat 43 Toronto House Surrey Quays Road
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71 Montreal House Surrey Quays Road	Flat 41 Toronto House Surrey Quays Road
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65 Montreal House Surrey Quays Road	Flat 38 Toronto House Surrey Quays Road
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63 Montreal House Surrey Quays Road	Flat 35 Toronto House Surrey Quays Road
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60 Montreal House Surrey Quays Road	Flat 30 Toronto House Surrey Quays Road
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57 Montreal House Surrey Quays Road	Flat 25 Toronto House Surrey Quays Road
London	Road
54 Montreal House Surrey Quays Road	Flat 23 Toronto House Surrey Quays Road
London	Road
52 Montreal House Surrey Quays Road	Flat 20 Toronto House Surrey Quays Road
London	Road
49 Montreal House Surrey Quays Road	Flat 17 Toronto House Surrey Quays Road
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46 Montreal House Surrey Quays Road	Flat 15 Toronto House Surrey Quays Road
London	Road
43 Montreal House Surrey Quays Road	Flat 12 Toronto House Surrey Quays Road
London	Road
41 Montreal House Surrey Quays Road	Flat 10 Toronto House Surrey Quays Road
London	Road
35 Montreal House Surrey Quays Road	Flat 7 Toronto House Surrey Quays Road
London	Road
30 Montreal House Surrey Quays Road	Flat 5 Toronto House Surrey Quays Road
London	Road
27 Montreal House Surrey Quays Road	Unit C Toronto House Surrey Quays Road
London	Road
25 Montreal House Surrey Quays Road	Cafe Canada Water Library 21 Surrey Quays Road
London	
22 Montreal House Surrey Quays Road	Unit A3 Toronto House Surrey Quays Road
London	Road
Flat 61 Toronto House Surrey Quays	Unit A1 Toronto House Surrey Quays

Road	141 Montreal House Surrey Quays Road
Stall 1 Deal Porter Square London	London
56 Montreal House Surrey Quays Road	140 Montreal House Surrey Quays Road
London	London
55 Montreal House Surrey Quays Road	136 Montreal House Surrey Quays Road
London	London
53 Montreal House Surrey Quays Road	135 Montreal House Surrey Quays Road
London	London
51 Montreal House Surrey Quays Road	133 Montreal House Surrey Quays Road
London	London
50 Montreal House Surrey Quays Road	132 Montreal House Surrey Quays Road
London	London
48 Montreal House Surrey Quays Road	130 Montreal House Surrey Quays Road
London	London
45 Montreal House Surrey Quays Road	128 Montreal House Surrey Quays Road
London	London
44 Montreal House Surrey Quays Road	125 Montreal House Surrey Quays Road
London	London
42 Montreal House Surrey Quays Road	Flat 4 Toronto House Surrey Quays
London	Road
40 Montreal House Surrey Quays Road	Flat 1 Toronto House Surrey Quays
London	Road
37 Montreal House Surrey Quays Road	Unit C Montreal House Surrey Quays
London	Road
36 Montreal House Surrey Quays Road	Unit A Montreal House Surrey Quays
London	Road
34 Montreal House Surrey Quays Road	169 Montreal House Surrey Quays Road
London	London
32 Montreal House Surrey Quays Road	165 Montreal House Surrey Quays Road
London	London
31 Montreal House Surrey Quays Road	164 Montreal House Surrey Quays Road
London	London
28 Montreal House Surrey Quays Road	162 Montreal House Surrey Quays Road
London	London
26 Montreal House Surrey Quays Road	160 Montreal House Surrey Quays Road
London	London
24 Montreal House Surrey Quays Road	159 Montreal House Surrey Quays Road
London	London
23 Montreal House Surrey Quays Road	157 Montreal House Surrey Quays Road
London	London
21 Montreal House Surrey Quays Road	154 Montreal House Surrey Quays Road
London	London
20 Montreal House Surrey Quays Road	152 Montreal House Surrey Quays Road
London	London
17 Montreal House Surrey Quays Road	151 Montreal House Surrey Quays Road
London	London
16 Montreal House Surrey Quays Road	149 Montreal House Surrey Quays Road
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148 Montreal House Surrey Quays Road  
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6 Montreal House Surrey Quays Road  
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4 Montreal House Surrey Quays Road  
London  
3 Montreal House Surrey Quays Road  
London  
1 Montreal House Surrey Quays Road  
London  
Canada Water Library 21 Surrey Quays  
Road London  
Flat 63 Toronto House Surrey Quays  
Road  
Flat 60 Toronto House Surrey Quays  
Road



Flat 58 Toronto House Surrey Quays Road	543 Giverny House Water Gardens Square London
Flat 55 Toronto House Surrey Quays Road	527 Giverny House Water Gardens Square London
Flat 54 Toronto House Surrey Quays Road	445 Heligan House Water Gardens Square London
Flat 52 Toronto House Surrey Quays Road	412 Heligan House Water Gardens Square London
Flat 50 Toronto House Surrey Quays Road	Flat 21 Toronto House Surrey Quays Road
Flat 49 Toronto House Surrey Quays Road	Flat 19 Toronto House Surrey Quays Road
Flat 47 Toronto House Surrey Quays Road	Flat 18 Toronto House Surrey Quays Road
Flat 45 Toronto House Surrey Quays Road	Flat 16 Toronto House Surrey Quays Road
Flat 42 Toronto House Surrey Quays Road	Flat 13 Toronto House Surrey Quays Road
Flat 40 Toronto House Surrey Quays Road	Flat 11 Toronto House Surrey Quays Road
Flat 39 Toronto House Surrey Quays Road	Flat 9 Toronto House Surrey Quays Road
Flat 37 Toronto House Surrey Quays Road	Flat 8 Toronto House Surrey Quays Road
Flat 34 Toronto House Surrey Quays Road	Unit 1 Toronto House Surrey Quays Road
Flat 32 Toronto House Surrey Quays Road	Tinos Mobile Pizza Deal Porter Square London
Flat 31 Toronto House Surrey Quays Road	Stompin Jago Canada Water Library 21 Surrey Quays Road
Flat 29 Toronto House Surrey Quays Road	Unit B Toronto House Surrey Quays Road
Flat 27 Toronto House Surrey Quays Road	Top Hill Gelato Deal Porter Square London
Flat 26 Toronto House Surrey Quays Road	Dirtybird Restaurant Printworks Surrey Quays Road
Flat 22 Toronto House Surrey Quays Road	Printworks Surrey Quays Road London
Portacabins Canada Water Retail Park Surrey Quays Road	Pizza Hut 5 The Mast Leisure Park Teredo Street
Phantom Peak Surrey Quays Road London	Hollywood Bowl 3A The Mast Leisure Park Teredo Street
642 Pavillion House Water Gardens Square London	7-9 Surrey Quays Shopping Centre Redriff Road London
602 Pavillion House Water Gardens Square London	Tesco Instore Pharmacy Surrey Quays Shopping Centre Redriff Road
9 Maritime Street London Southwark	14 Surrey Quays Shopping Centre Redriff Road London
Flat 29 7 Maritime Street London	35 Surrey Quays Shopping Centre

Redriff Road London  
 5 Surrey Quays Shopping Centre Redriff  
 Road London  
 10-12 Surrey Quays Shopping Centre  
 Redriff Road London  
 37 Surrey Quays Shopping Centre  
 Redriff Road London  
 19 Surrey Quays Shopping Centre  
 Redriff Road London  
 28 Surrey Quays Shopping Centre  
 Redriff Road London  
 2-4 Surrey Quays Shopping Centre  
 Redriff Road London  
 3 Surrey Quays Shopping Centre Redriff  
 Road London

First Floor 59 Surrey Quays Shopping  
 Centre Redriff Road  
 45 Surrey Quays Shopping Centre  
 Redriff Road London  
 1 Surrey Quays Shopping Centre Redriff  
 Road London  
 Unit D1 Surrey Quays Shopping Centre  
 Redriff Road  
 22 Surrey Quays Shopping Centre  
 Redriff Road London  
 42 Surrey Quays Shopping Centre  
 Redriff Road London  
 18 Surrey Quays Shopping Centre  
 Redriff Road London

**Re-consultation:**

**Consultation responses received - Planning Permission 23/AP/0564****Internal services**

formal consultation and response to Pol  
Design and Conservation Team [Formal]  
Local Economy  
Ecology  
Environmental Protection  
Highways Development and Management  
Flood Risk Management & Urban Drainage  
Transport Policy  
Urban Forester  
Waste Management  
Community Infrastructure Levy Team

**Statutory and non-statutory organisations**

Environment Agency  
Historic England  
London Fire & Emergency Planning Authori  
London Underground  
Metropolitan Police Service (Designing O  
Thames Water

**Neighbour and local groups consulted:**

573 Giverny House Water Garden  
Square London

18 Quebec Way London SE16 7ET  
Via Email

Via Email  
Via Email

## Recommendation – Planning Permission 23/AP/0565

This document shows the case officer's recommended decision for the application referred to below.

This document is not a decision notice for this application.

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<b>Applicant</b>	Charlotte N/A AIRE UK Canada Water GP PropCos Ltd, acting as general pa...	<b>Reg. Number</b>	23/AP/0565
<b>Application Type</b>	Approval of Reserved Matters		
<b>Recommendation</b>	APPROVE reserved matters	<b>Case Number</b>	468-C

### Draft of Decision Notice

**reserved matters is APPROVED for the following development:**

Approval of Reserved Matters (access, layout and scale) pursuant to outline planning permission 21/AP/2655 for the comprehensive commercial development of the Canada Water Dockside Masterplan site. Reserved Matters Approval sought for construction of a basement spanning Plots A1 and A2 comprising cycle parking, plant space and ancillary floorspace and associated works (all ancillary Class E) to support office and retail/food and beverage uses in Plots A1 and A2. This is as an application for subsequent consent accompanied by an environmental statement. Consequently the application is accompanied by a Statement of Conformity submitted pursuant to the Town and Country Planning (Environmental Impact Assessment) regulations 2017. This ES Statement of Conformity should be read in conjunction with the Canada Water Dockside Masterplan ES which can be viewed in full on the Council's website (21/AP/2655).

Units 1 And 4 Canada Water Retail Park London Southwark

**In accordance with application received on 1 March 2023 and Applicant's Drawing Nos.:**

## Existing Plans

## Proposed Plans

Plans - Proposed Location Plan Basement:CWD-BIG-BT-B1-DR-A-1000-XX received 01/03/2023

Plans - Proposed Location Plan Basement CWD-BIG-BT-B1-DR-A-1000-XX received 01/03/2023

Plans - Proposed Location Plan Basement CWD-BIG-BT-B1-DR-A-1000-XX received 01/03/2023

Plans - Proposed Basement Sections CWD-BIG-BT-B1-DR-A-3000-XX received 01/03/2023

Plans - Proposed Proposed GA Basement 1 CWD-BIG-BT-B1-DR-A-2000-XX received 01/03/2023

Plans - Proposed CWD-ASD-DE-XX-DR-L-0700-XX-P01 TEMPORARY WIND MITIGATION MEASURES. received 14/07/2023

Plans - Proposed CWD-ASD-DE-XX-DR-L-0701-XX-P01\_LANDSCAPE PLAN FOR TEMPORARY WIND MITIGATION received 14/07/2023

## Other Documents

Arboricultural statement AIA Addendum 201145-CWD-TMA-RMA-XX-RP-Z0002-XX-P02 received 13/03/2023

Arboricultural statement Arbs Method Statement CWD-TMA-RMA-X X-RP-Z-000 1-X X-P00 received 01/03/2023

Flood risk assessment CWD-RUK-RMA-XX-RP-C-0001-XX received 01/03/2023

Document SCI:CWD-KAN-RMA-XX-RP-Y-0001-XX-P00 received 01/03/2023

Energy statement CWD-A10-A1-XX-RP-Y-0003-XX-P01 REVISED CES PLOT A1 received 11/07/2023

Fire Statement C W D -OF R-A1-X X-R P -Y-000 1-X X-P 00 received 01/03/2023

Document Sustainability Statement C W D -A10-A1-X X-R P -Y-000 1-X X-P 04 received 01/03/2023

Design and access statement CWD-BIG-A1-XX-RP-A-0001-XX-P00 - Plot A1 received 01/03/2023

Energy statement CWD-ARU-A2-XX-RP-Y-0002-P01 WLC ASSESSMENT A2. received 11/07/2023

Energy statement CWD-A10-A1-XX-RP-Y-0002-XX-P01 WLC SPREADSHEET A1.  
received 11/07/2023

Energy statement CWD-ARU-A2-XX-RP-Y-0002-P01 WLC ASSESSMENT A2.  
received 11/07/2023

Energy statement CWD-ARU-A2-XX-RP-Y-0003-P02 CES PLOT A2 received  
11/07/2023

Document Dev Consultation Charter CWD-KAN-DE-XX-RP-Y-0002-XX-P00 received  
08/03/2023

Transport assessment/statement CWD-WSP-RMA-XX-RP-Y-0002-XX-P00 received  
01/03/2023

Travel plan for Building A1 CWD-WSP-A1-XX-RP-Y-0001-XX-P00 received  
01/03/2023

Travel plan For Buildings A2 CWD-WSP-A2-XX-RP-Y-0001-XX-P00 received  
01/02/2023

Document Basement Impact Assessment CWD-RUK-BT-X X-RP-S-0001-X X-P00  
received 01/03/2023

Document DSP CWD-WSP-RMA-XX-RP-Y-0001-XX-P00 received 01/03/2023

Document Operational Waste Management Strategy CWD-WSP-RMA-XX-RP-Y-  
0003-XX-P00 received 08/03/2023

Document Planning Compliance Report C W D -DP 9-R M A-X X-R P -T-0002-X X-P  
00 received 08/03/2023

Fire Statement Fire Statement A2 C W D -OF R-A 2-X X-R P -Y-000 1-X X-P 00  
received 01/03/2023

Design and access statement CWD-HWK-A2-XX-RP-A-0001-XX-P00 received  
01/03/2023

Document Equalities Impact Assessment CWD-TRI-RMA-X X-RP-Y-0002-X X-P00  
received 01/03/0223

Environmental Statement Statement of Conformity received 14/07/2023

Document Sustainability Statement A2 CWD-ARU-A2-XX-RP-Y-0001-XX-P00  
received 01/03/2023

Document Utilities Report CWD-CBD-RMA-XX-RP-Y-0001-XX received 01/03/2023

Energy statement 60948\_CWD A2\_PART L  
2013\_GLA CARBON EMISSION REPORTING\_SPREADSHEET\_V1.2\_2020  
received 11/07/2023

Sustainability statement CWD-A10-A1-XX-RP-Y-0001-XX-P02 PLOT A1 received  
11/07/2023

Sustainability statement CWD-ARU-A2-XX-RP-Y-0001-P02 Plot A2 received  
11/07/2023



Energy statement 5161\_CWA2\_WLCA REVIEW FOR ARUP\_P01 received  
11/07/2023

Energy statement 60948\_CWD A2\_PART L  
2021\_GLA CARBON\_EMISSION\_REPORTING\_SPREADSHEET\_V2.0\_0 received  
11/07/2023

Energy statement 60948\_ENERGY STRATEGY\_REV03 PLOT A2 received  
11/07/2023

Energy statement 60952\_CWD A1\_PART L  
2013\_GLA CARBON\_EMISSION\_REPORTING\_SPREADSHEET\_V1.2\_2020  
received 11/07/2023

Energy statement 60952\_CWD A1\_PART L  
2021\_GLA CARBON\_EMISSION\_REPORTING\_SPREADSHEET\_V2.0\_0. received  
11/07/2023

Energy statement 60952\_ENERGY STRATEGY\_REV03 PLOT A1 received  
11/07/2023

Energy statement CWA2\_GLA WLCA SPREADSHEET\_DETAILED\_22 FEB 2023.  
received 11/07/2023

Energy statement CWD-A10-A1-XX-RP-Y-0002-XX-P01 WLC ASSESSMENT A1  
received 11/07/2023

Energy statement CWD-A10-A1-XX-RP-Y-0002-XX-P01 WLC SPREADSHEET A1.  
received 11/07/2023

Energy statement CWD-A10-A1-XX-RP-Y-0003-XX-P01 REVISED CES PLOT A1  
received 11/07/2023

Planning statement CWD - RMA PLANNING STATEMENT V2. received 14/07/2023

## **Informatives**

- 1 Paragraph 3.12.9 of Policy D12 explains that Fire Statements should be produced by someone who is:

"third-party independent and suitably-qualified" The council considers this to be a qualified engineer with relevant experience in fire safety, such as a chartered engineer registered with the Engineering Council by the Institution of Fire Engineers, or a suitably qualified and competent professional with the demonstrable experience to address the complexity of the design being proposed. This should be evidenced in the fire statement. The council accepts Fire Statements in good faith on that basis. The duty to identify fire risks and hazards in premises and to take appropriate action lies solely with the developer.

The fire risk assessment/statement covers matters required by planning policy. This is in no way a professional technical assessment of the fire risks presented

by the development. The legal responsibility and liability lies with the 'responsible person'. The responsible person being the person who prepares the fire risk assessment/statement not planning officers who make planning decisions.

- 2 The Applicant is reminded that in order to discharge into the Dock, consent will be required from all relevant land owners. Should such consent not be granted the Applicant will be required to connect to the sewer. In any event it will be necessary to submit a formal application to discharge Condition 17 of 21/AP/2655.

As part of this formal submission for discharge of Condition 17 the applicant will be required to demonstrate that any water proposed to be discharged into the Dock meets CIRIA Suds standards or in the event that discharge to the sewer is required the strategy to be submitted under Condition 17 will need to fully demonstrate compliance with maximising SUDs onsite and meeting policy restricted runoff rates

## APPENDIX 3

## Relevant planning history - Planning Permission 23/AP/0565

Reference and Proposal	Status
<p><b>21/AP/2655</b></p> <p>Outline planning permission (all matters reserved) for a commercial masterplan containing offices and other complementary town centre uses with no housing. The proposals involve the demolition of all buildings and structures and the comprehensive redevelopment of the site to provide three Development Plots (A1, A2 and B) which will contain three Buildings (A1, A2 and B) above ground with basements. The proposed land uses are: offices (Class E), retail/professional services/food and drink (E), learning and non-residential institutions/local community (F1/F2), medical or health (E) and indoor sport, recreation or fitness (E). - Works of hard and soft landscaping are proposed to create a series of new and improved public realm spaces within the site. This will include the replacement of some trees and the planting of additional new trees. - New vehicular access points are proposed to be created from Surrey Quays Road and Canada Street, along with other incidental works.</p>	<p>GRANTED - Major Application 25/01/2023</p>

## Consultation undertaken - Planning Permission 23/AP/0565

**Site notice date:** 13/03/2023

**Press notice date:** 16/03/2023

**Case officer site visit date:** n/a

**Neighbour consultation letters sent:** 09/03/2023

### Internal services consulted

formal consultation and response to Pol  
 Archaeology  
 Community Infrastructure Levy Team  
 Design and Conservation Team [Formal]  
 Local Economy  
 Ecology  
 Environmental Protection  
 Highways Development and Management  
 Flood Risk Management & Urban Drainage  
 Urban Forester  
 Waste Management  
 Transport Policy

### Statutory and non-statutory organisations

Environment Agency  
 Historic England  
 London Fire & Emergency Planning Authori  
 London Underground  
 Metropolitan Police Service (Designing O  
 Transport for London  
 Thames Water

### Neighbour and local groups consulted:

Flat 33 7 Maritime Street London	Flat 32 11 Maritime Street London
Flat 32 7 Maritime Street London	Flat 29 11 Maritime Street London
Flat 30 7 Maritime Street London	Flat 27 11 Maritime Street London
Flat 27 7 Maritime Street London	Flat 26 11 Maritime Street London
Flat 26 7 Maritime Street London	Flat 24 11 Maritime Street London
Flat 24 7 Maritime Street London	Flat 23 11 Maritime Street London
Flat 23 7 Maritime Street London	Flat 21 11 Maritime Street London
Flat 21 7 Maritime Street London	Flat 20 11 Maritime Street London
Flat 38 11 Maritime Street London	Flat 17 11 Maritime Street London
Flat 35 11 Maritime Street London	Flat 15 11 Maritime Street London
Flat 33 11 Maritime Street London	Flat 12 11 Maritime Street London

Flat 11 11 Maritime Street London  
 Flat 9 11 Maritime Street London  
 Flat 8 11 Maritime Street London  
 Flat 6 11 Maritime Street London  
 Flat 5 11 Maritime Street London  
 Flat 3 11 Maritime Street London  
 Flat 2 11 Maritime Street London  
 Flat 57 7 Maritime Street London  
 Flat 56 7 Maritime Street London  
 Flat 54 7 Maritime Street London  
 Flat 53 7 Maritime Street London  
 Flat 51 7 Maritime Street London  
 Flat 50 7 Maritime Street London  
 Flat 48 7 Maritime Street London  
 Flat 47 7 Maritime Street London  
 Flat 45 7 Maritime Street London  
 Flat 44 7 Maritime Street London  
 Flat 39 7 Maritime Street London  
 Flat 38 7 Maritime Street London  
 21 Maritime Street London Southwark  
 17 Maritime Street London Southwark  
 3 Maritime Street London Southwark  
 1 Maritime Street London Southwark  
 Flat 38 29 Surrey Quays Road London  
 Flat 35 29 Surrey Quays Road London  
 Flat 34 29 Surrey Quays Road London  
 Flat 32 29 Surrey Quays Road London  
 Flat 31 29 Surrey Quays Road London  
 Flat 29 29 Surrey Quays Road London  
 Flat 28 29 Surrey Quays Road London  
 Flat 25 29 Surrey Quays Road London  
 Flat 23 29 Surrey Quays Road London  
 Flat 21 29 Surrey Quays Road London  
 Flat 18 29 Surrey Quays Road London  
 Flat 16 29 Surrey Quays Road London  
 Flat 15 29 Surrey Quays Road London  
 Flat 13 29 Surrey Quays Road London  
 Flat 12 29 Surrey Quays Road London  
 Flat 10 29 Surrey Quays Road London  
 Portacabins Canada Water Retail Park  
 Surrey Quays Road  
 Phantom Peak Surrey Quays Road  
 London  
 642 Pavillion House Water Gardens  
 Square London  
 Flat 9 29 Surrey Quays Road London  
 Flat 7 29 Surrey Quays Road London  
 Flat 4 29 Surrey Quays Road London  
 Flat 1 29 Surrey Quays Road London  
 Flat 70 11 Maritime Street London  
 Flat 68 11 Maritime Street London  
 Flat 67 11 Maritime Street London  
 Flat 65 11 Maritime Street London  
 Flat 64 11 Maritime Street London  
 Flat 62 11 Maritime Street London  
 Flat 61 11 Maritime Street London  
 Flat 59 11 Maritime Street London  
 Flat 55 11 Maritime Street London  
 Flat 53 11 Maritime Street London  
 Flat 51 11 Maritime Street London  
 Flat 50 11 Maritime Street London  
 Flat 48 11 Maritime Street London  
 Flat 47 11 Maritime Street London  
 Flat 71 29 Surrey Quays Road London  
 Flat 69 29 Surrey Quays Road London  
 602 Pavillion House Water Gardens  
 Square London  
 9 Maritime Street London Southwark  
 Flat 29 7 Maritime Street London  
 543 Giverny House Water Gardens  
 Square London  
 527 Giverny House Water Gardens  
 Square London  
 445 Heligan House Water Gardens  
 Square London  
 412 Heligan House Water Gardens  
 Square London  
 Flat 66 29 Surrey Quays Road London  
 Flat 37 29 Surrey Quays Road London  
 Flat 20 29 Surrey Quays Road London  
 Flat 3 29 Surrey Quays Road London  
 Flat 30 11 Maritime Street London  
 Flat 59 7 Maritime Street London  
 Flat 42 7 Maritime Street London  
 Flat 12 7 Maritime Street London  
 Flat 24 5 Maritime Street London  
 Flat 8 5 Maritime Street London  
 15 Maritime Street London Southwark  
 The League Of Adventure Canada Street  
 London  
 Flat 50 29 Surrey Quays Road London  
 Flat 49 7 Maritime Street London  
 Flat 46 7 Maritime Street London  
 622 Pavillion House Water Gardens  
 Square London  
 Flat 65 29 Surrey Quays Road London

Flat 63 29 Surrey Quays Road London	453 Heligan House Water Gardens
Flat 62 29 Surrey Quays Road London	Square London
Flat 60 29 Surrey Quays Road London	451 Heligan House Water Gardens
Flat 58 29 Surrey Quays Road London	Square London
Flat 45 11 Maritime Street London	613 Pavillion House Water Gardens
Flat 44 11 Maritime Street London	Square London
548 Giverny House Water Gardens	612 Pavillion House Water Gardens
Square London	Square London
546 Giverny House Water Gardens	604 Pavillion House Water Gardens
Square London	Square London
545 Giverny House Water Gardens	692 Pavillion House Water Gardens
Square London	Square London
538 Giverny House Water Gardens	683 Pavillion House Water Gardens
Square London	Square London
537 Giverny House Water Gardens	681 Pavillion House Water Gardens
Square London	Square London
535 Giverny House Water Gardens	665 Pavillion House Water Gardens
Square London	Square London
534 Giverny House Water Gardens	623 Pavillion House Water Gardens
Square London	Square London
532 Giverny House Water Gardens	564 Giverny House Water Gardens
Square London	Square London
526 Giverny House Water Gardens	562 Giverny House Water Gardens
Square London	Square London
524 Giverny House Water Gardens	554 Giverny House Water Gardens
Square London	Square London
522 Giverny House Water Gardens	552 Giverny House Water Gardens
Square London	Square London
521 Giverny House Water Gardens	444 Heligan House Water Gardens
Square London	Square London
513 Giverny House Water Gardens	442 Heligan House Water Gardens
Square London	Square London
511 Giverny House Water Gardens	441 Heligan House Water Gardens
Square London	Square London
504 Giverny House Water Gardens	432 Heligan House Water Gardens
Square London	Square London
574 Giverny House Water Gardens	431 Heligan House Water Gardens
Square London	Square London
573 Giverny House Water Gardens	425 Heligan House Water Gardens
Square London	Square London
571 Giverny House Water Gardens	424 Heligan House Water Gardens
Square London	Square London
542 Giverny House Water Gardens	422 Heligan House Water Gardens
Square London	Square London
501 Giverny House Water Gardens	556 Giverny House Water Gardens
Square London	Square London
454 Heligan House Water Gardens	565 Giverny House Water Gardens
Square London	Square London



557 Giverny House Water Gardens Square London	Unit 1 Canada Water Retail Park Surrey Quays Road
415 Heligan House Water Gardens Square London	344 Eden House Water Gardens Square London
414 Heligan House Water Gardens Square London	313 Eden House Water Gardens Square London
405 Heligan House Water Gardens Square London	354 Eden House Water Gardens Square London
404 Heligan House Water Gardens Square London	326 Eden House Water Gardens Square London
402 Heligan House Water Gardens Square London	302 Eden House Water Gardens Square London
472 Heligan House Water Gardens Square London	Unit 300 Eden House Water Gardens Square
462 Heligan House Water Gardens Square London	342 Eden House Water Gardens Square London
461 Heligan House Water Gardens Square London	335 Eden House Water Gardens Square London
421 Heligan House Water Gardens Square London	332 Eden House Water Gardens Square London
671 Pavillion House Water Gardens Square London	325 Eden House Water Gardens Square London
664 Pavillion House Water Gardens Square London	322 Eden House Water Gardens Square London
662 Pavillion House Water Gardens Square London	315 Eden House Water Gardens Square London
655 Pavillion House Water Gardens Square London	312 Eden House Water Gardens Square London
653 Pavillion House Water Gardens Square London	304 Eden House Water Gardens Square London
652 Pavillion House Water Gardens Square London	351 Eden House Water Gardens Square London
645 Pavillion House Water Gardens Square London	301 Eden House Water Gardens Square London
641 Pavillion House Water Gardens Square London	345 Eden House Water Gardens Square London
634 Pavillion House Water Gardens Square London	346 Eden House Water Gardens Square London
632 Pavillion House Water Gardens Square London	343 Eden House Water Gardens Square London
631 Pavillion House Water Gardens Square London	341 Eden House Water Gardens Square London
621 Pavillion House Water Gardens Square London	673 Pavillion House Water Gardens Square London
615 Pavillion House Water Gardens Square London	334 Eden House Water Gardens Square London
Unit 4 Canada Water Retail Park Surrey Quays Road	333 Eden House Water Gardens Square London

331 Eden House Water Gardens Square London	109 Montreal House Surrey Quays Road London
324 Eden House Water Gardens Square London	72 Montreal House Surrey Quays Road London
323 Eden House Water Gardens Square London	Flat 28 Toronto House Surrey Quays Road
316 Eden House Water Gardens Square London	Unit A2 Toronto House Surrey Quays Road
314 Eden House Water Gardens Square London	Flat 33 Toronto House Surrey Quays Road
311 Eden House Water Gardens Square London	Flat 57 Toronto House Surrey Quays Road
305 Eden House Water Gardens Square London	Flat 36 Toronto House Surrey Quays Road
303 Eden House Water Gardens Square London	Flat 14 Toronto House Surrey Quays Road
362 Eden House Water Gardens Square London	122 Montreal House Surrey Quays Road London
361 Eden House Water Gardens Square London	101 Montreal House Surrey Quays Road London
353 Eden House Water Gardens Square London	81 Montreal House Surrey Quays Road London
352 Eden House Water Gardens Square London	61 Montreal House Surrey Quays Road London
336 Eden House Water Gardens Square London	58 Montreal House Surrey Quays Road London
321 Eden House Water Gardens Square London	39 Montreal House Surrey Quays Road London
Rear Of Room Quays Unit A2 Toronto House Surrey Quays Road	19 Montreal House Surrey Quays Road London
47 Montreal House Surrey Quays Road London	167 Montreal House Surrey Quays Road London
29 Montreal House Surrey Quays Road London	146 Montreal House Surrey Quays Road London
12 Montreal House Surrey Quays Road London	127 Montreal House Surrey Quays Road London
Flat 62 Toronto House Surrey Quays Road	108 Montreal House Surrey Quays Road London
Flat 44 Toronto House Surrey Quays Road	79 Montreal House Surrey Quays Road London
Flat 6 Toronto House Surrey Quays Road	68 Montreal House Surrey Quays Road London
156 Montreal House Surrey Quays Road London	142 Montreal House Surrey Quays Road London
138 Montreal House Surrey Quays Road London	Flat 24 Toronto House Surrey Quays Road
Flat 2 Toronto House Surrey Quays Road	624 Pavillion House Water Gardens Square London

Flat 23 5 Maritime Street London	Flat 13 5 Maritime Street London
Flat 5 5 Maritime Street London	544 Giverny House Water Gardens
23 Maritime Street London Southwark	Square London
Flat 26 29 Surrey Quays Road London	Flat 59 29 Surrey Quays Road London
Flat 6 29 Surrey Quays Road London	Flat 56 29 Surrey Quays Road London
Flat 56 11 Maritime Street London	Flat 53 29 Surrey Quays Road London
Flat 36 11 Maritime Street London	Flat 47 29 Surrey Quays Road London
Flat 18 11 Maritime Street London	Flat 41 29 Surrey Quays Road London
Flat 60 7 Maritime Street London	Flat 39 29 Surrey Quays Road London
Flat 41 7 Maritime Street London	Flat 36 29 Surrey Quays Road London
Flat 35 7 Maritime Street London	Flat 33 29 Surrey Quays Road London
Flat 15 7 Maritime Street London	Flat 30 29 Surrey Quays Road London
Flat 16 11 Maritime Street London	Flat 27 29 Surrey Quays Road London
423 Heligan House Water Gardens	Flat 24 29 Surrey Quays Road London
Square London	89 Montreal House Surrey Quays Road
Flat 52 11 Maritime Street London	London
525 Giverny House Water Gardens	33 Montreal House Surrey Quays Road
Square London	London
452 Heligan House Water Gardens	103 Montreal House Surrey Quays Road
Square London	London
Flat 68 29 Surrey Quays Road London	Flat 3 Toronto House Surrey Quays
Flat 48 29 Surrey Quays Road London	Road
558 Giverny House Water Gardens	38 Montreal House Surrey Quays Road
Square London	London
Flat 22 29 Surrey Quays Road London	147 Montreal House Surrey Quays Road
Flat 16 7 Maritime Street London	London
528 Giverny House Water Gardens	95 Montreal House Surrey Quays Road
Square London	London
503 Giverny House Water Gardens	92 Montreal House Surrey Quays Road
Square London	London
434 Heligan House Water Gardens	90 Montreal House Surrey Quays Road
Square London	London
464 Heligan House Water Gardens	87 Montreal House Surrey Quays Road
Square London	London
Flat 1 5 Maritime Street London	84 Montreal House Surrey Quays Road
Flat 49 29 Surrey Quays Road London	London
East Warehouse Canada Water Retail	82 Montreal House Surrey Quays Road
Park Surrey Quays Road	London
413 Heligan House Water Gardens	18 Montreal House Surrey Quays Road
Square London	London
533 Giverny House Water Gardens	15 Montreal House Surrey Quays Road
Square London	London
Flat 61 29 Surrey Quays Road London	13 Montreal House Surrey Quays Road
Flat 19 7 Maritime Street London	London
651 Pavillion House Water Gardens	10 Montreal House Surrey Quays Road
Square London	London
633 Pavillion House Water Gardens	7 Montreal House Surrey Quays Road
Square London	London

5 Montreal House Surrey Quays Road London	106 Montreal House Surrey Quays Road London
2 Montreal House Surrey Quays Road London	100 Montreal House Surrey Quays Road London
Unit B Montreal House Surrey Quays Road	98 Montreal House Surrey Quays Road London
168 Montreal House Surrey Quays Road London	76 Montreal House Surrey Quays Road London
166 Montreal House Surrey Quays Road London	73 Montreal House Surrey Quays Road London
163 Montreal House Surrey Quays Road London	71 Montreal House Surrey Quays Road London
161 Montreal House Surrey Quays Road London	65 Montreal House Surrey Quays Road London
158 Montreal House Surrey Quays Road London	63 Montreal House Surrey Quays Road London
155 Montreal House Surrey Quays Road London	Flat 19 29 Surrey Quays Road London
153 Montreal House Surrey Quays Road London	Flat 17 29 Surrey Quays Road London
150 Montreal House Surrey Quays Road London	Flat 14 29 Surrey Quays Road London
145 Montreal House Surrey Quays Road London	Flat 11 29 Surrey Quays Road London
139 Montreal House Surrey Quays Road London	Flat 8 29 Surrey Quays Road London
137 Montreal House Surrey Quays Road London	Flat 5 29 Surrey Quays Road London
134 Montreal House Surrey Quays Road London	Flat 2 29 Surrey Quays Road London
131 Montreal House Surrey Quays Road London	Flat 69 11 Maritime Street London
129 Montreal House Surrey Quays Road London	Flat 66 11 Maritime Street London
126 Montreal House Surrey Quays Road London	Flat 63 11 Maritime Street London
124 Montreal House Surrey Quays Road London	Flat 37 7 Maritime Street London
121 Montreal House Surrey Quays Road London	Flat 34 7 Maritime Street London
119 Montreal House Surrey Quays Road London	Flat 31 7 Maritime Street London
116 Montreal House Surrey Quays Road London	Flat 28 7 Maritime Street London
113 Montreal House Surrey Quays Road London	Flat 25 7 Maritime Street London
111 Montreal House Surrey Quays Road London	Flat 22 7 Maritime Street London
	Flat 13 7 Maritime Street London
	Flat 10 7 Maritime Street London
	Flat 7 7 Maritime Street London
	Flat 4 7 Maritime Street London
	Flat 1 7 Maritime Street London
	Flat 57 11 Maritime Street London
	Flat 58 11 Maritime Street London
	Flat 14 11 Maritime Street London
	Flat 44 29 Surrey Quays Road London
	Flat 70 29 Surrey Quays Road London
	Flat 67 29 Surrey Quays Road London
	Flat 64 29 Surrey Quays Road London
	Flat 43 7 Maritime Street London
	Flat 40 7 Maritime Street London
	Flat 13 11 Maritime Street London
	Flat 10 11 Maritime Street London

Flat 7 11 Maritime Street London	Road
Flat 4 11 Maritime Street London	Flat 59 Toronto House Surrey Quays
Flat 1 11 Maritime Street London	Road
Flat 58 7 Maritime Street London	Flat 56 Toronto House Surrey Quays
Flat 55 7 Maritime Street London	Road
Flat 52 7 Maritime Street London	Flat 53 Toronto House Surrey Quays
Flat 7 5 Maritime Street London	Road
Flat 4 5 Maritime Street London	Flat 51 Toronto House Surrey Quays
19 Maritime Street London Southwark	Road
13 Maritime Street London Southwark	Flat 48 Toronto House Surrey Quays
Flat 25 5 Maritime Street London	Road
Flat 22 5 Maritime Street London	Flat 46 Toronto House Surrey Quays
Flat 19 5 Maritime Street London	Road
Flat 16 5 Maritime Street London	Flat 43 Toronto House Surrey Quays
Flat 10 5 Maritime Street London	Road
Flat 54 11 Maritime Street London	Flat 41 Toronto House Surrey Quays
Flat 49 11 Maritime Street London	Road
Flat 46 11 Maritime Street London	Flat 38 Toronto House Surrey Quays
Flat 43 11 Maritime Street London	Road
Flat 40 11 Maritime Street London	Flat 35 Toronto House Surrey Quays
Flat 37 11 Maritime Street London	Road
60 Montreal House Surrey Quays Road	Flat 30 Toronto House Surrey Quays
London	Road
57 Montreal House Surrey Quays Road	Flat 25 Toronto House Surrey Quays
London	Road
54 Montreal House Surrey Quays Road	Flat 23 Toronto House Surrey Quays
London	Road
52 Montreal House Surrey Quays Road	Flat 20 Toronto House Surrey Quays
London	Road
49 Montreal House Surrey Quays Road	Flat 17 Toronto House Surrey Quays
London	Road
46 Montreal House Surrey Quays Road	Flat 15 Toronto House Surrey Quays
London	Road
43 Montreal House Surrey Quays Road	Flat 12 Toronto House Surrey Quays
London	Road
41 Montreal House Surrey Quays Road	Flat 10 Toronto House Surrey Quays
London	Road
35 Montreal House Surrey Quays Road	Flat 7 Toronto House Surrey Quays
London	Road
30 Montreal House Surrey Quays Road	Flat 5 Toronto House Surrey Quays
London	Road
27 Montreal House Surrey Quays Road	Unit C Toronto House Surrey Quays
London	Road
25 Montreal House Surrey Quays Road	Cafe Canada Water Library 21 Surrey
London	Quays Road
22 Montreal House Surrey Quays Road	Unit A3 Toronto House Surrey Quays
London	Road
Flat 61 Toronto House Surrey Quays	Unit A1 Toronto House Surrey Quays

Road  
 Stall 1 Deal Porter Square London  
 56 Montreal House Surrey Quays Road  
 London  
 55 Montreal House Surrey Quays Road  
 London  
 53 Montreal House Surrey Quays Road  
 London  
 51 Montreal House Surrey Quays Road  
 London  
 50 Montreal House Surrey Quays Road  
 London  
 48 Montreal House Surrey Quays Road  
 London  
 45 Montreal House Surrey Quays Road  
 London  
 44 Montreal House Surrey Quays Road  
 London  
 42 Montreal House Surrey Quays Road  
 London  
 40 Montreal House Surrey Quays Road  
 London  
 Flat 60 11 Maritime Street London  
 Flat 34 11 Maritime Street London  
 Flat 31 11 Maritime Street London  
 Flat 28 11 Maritime Street London  
 Flat 25 11 Maritime Street London  
 Flat 22 11 Maritime Street London  
 Flat 19 11 Maritime Street London  
 555 Giverny House Water Gardens  
 Square London  
 553 Giverny House Water Gardens  
 Square London  
 37 Montreal House Surrey Quays Road  
 London  
 36 Montreal House Surrey Quays Road  
 London  
 34 Montreal House Surrey Quays Road  
 London  
 32 Montreal House Surrey Quays Road  
 London  
 31 Montreal House Surrey Quays Road  
 London  
 28 Montreal House Surrey Quays Road  
 London  
 26 Montreal House Surrey Quays Road  
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 159 Montreal House Surrey Quays Road

London	Square London
157 Montreal House Surrey Quays Road	691 Pavillion House Water Gardens
London	Square London
154 Montreal House Surrey Quays Road	682 Pavillion House Water Gardens
London	Square London
152 Montreal House Surrey Quays Road	644 Pavillion House Water Gardens
London	Square London
151 Montreal House Surrey Quays Road	601 Pavillion House Water Gardens
London	Square London
149 Montreal House Surrey Quays Road	563 Giverny House Water Gardens
London	Square London
148 Montreal House Surrey Quays Road	566 Giverny House Water Gardens
London	Square London
144 Montreal House Surrey Quays Road	531 Giverny House Water Gardens
London	Square London
143 Montreal House Surrey Quays Road	523 Giverny House Water Gardens
London	Square London
123 Montreal House Surrey Quays Road	515 Giverny House Water Gardens
London	Square London
120 Montreal House Surrey Quays Road	446 Heligan House Water Gardens
London	Square London
118 Montreal House Surrey Quays Road	443 Heligan House Water Gardens
London	Square London
117 Montreal House Surrey Quays Road	435 Heligan House Water Gardens
London	Square London
115 Montreal House Surrey Quays Road	433 Heligan House Water Gardens
London	Square London
114 Montreal House Surrey Quays Road	426 Heligan House Water Gardens
London	Square London
112 Montreal House Surrey Quays Road	416 Heligan House Water Gardens
London	Square London
110 Montreal House Surrey Quays Road	411 Heligan House Water Gardens
London	Square London
107 Montreal House Surrey Quays Road	403 Heligan House Water Gardens
London	Square London
654 Pavillion House Water Gardens	471 Heligan House Water Gardens
Square London	Square London
643 Pavillion House Water Gardens	672 Pavillion House Water Gardens
Square London	Square London
635 Pavillion House Water Gardens	663 Pavillion House Water Gardens
Square London	Square London
625 Pavillion House Water Gardens	512 Giverny House Water Gardens
Square London	Square London
614 Pavillion House Water Gardens	505 Giverny House Water Gardens
Square London	Square London
611 Pavillion House Water Gardens	502 Giverny House Water Gardens
Square London	Square London
603 Pavillion House Water Gardens	572 Giverny House Water Gardens



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 3 Montreal House Surrey Quays Road  
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 1 Montreal House Surrey Quays Road  
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 Canada Water Library 21 Surrey Quays  
 Road London  
 Flat 63 Toronto House Surrey Quays  
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Flat 60 Toronto House Surrey Quays Road	Flat 13 Toronto House Surrey Quays Road
Flat 58 Toronto House Surrey Quays Road	Flat 11 Toronto House Surrey Quays Road
Flat 55 Toronto House Surrey Quays Road	Flat 9 Toronto House Surrey Quays Road
Flat 54 Toronto House Surrey Quays Road	Flat 8 Toronto House Surrey Quays Road
Flat 52 Toronto House Surrey Quays Road	Unit 1 Toronto House Surrey Quays Road
Flat 50 Toronto House Surrey Quays Road	Tinos Mobile Pizza Deal Porter Square London
Flat 49 Toronto House Surrey Quays Road	Stompin Jago Canada Water Library 21 Surrey Quays Road
Flat 47 Toronto House Surrey Quays Road	Unit B Toronto House Surrey Quays Road
Flat 45 Toronto House Surrey Quays Road	Top Hill Gelato Deal Porter Square London
Flat 42 Toronto House Surrey Quays Road	Hollywood Bowl 3A The Mast Leisure Park Teredo Street
Flat 40 Toronto House Surrey Quays Road	Dirtybird Restaurant Printworks Surrey Quays Road
Flat 39 Toronto House Surrey Quays Road	Surrey Quays Leisure Park Surrey Quays Road London
Flat 37 Toronto House Surrey Quays Road	Printworks Surrey Quays Road London
Flat 34 Toronto House Surrey Quays Road	Fattie Arbuckles The Mast Leisure Park Teredo Street
Flat 32 Toronto House Surrey Quays Road	Uci Cinema 2 The Mast Leisure Park Teredo Street
Flat 31 Toronto House Surrey Quays Road	Gala Bingo The Mast Leisure Park Teredo Street
Flat 29 Toronto House Surrey Quays Road	The Mast Leisure Park Teredo Street London
Flat 27 Toronto House Surrey Quays Road	3 Teredo Street London Southwark
Flat 26 Toronto House Surrey Quays Road	1 Surrey Quays Road London Southwark
Flat 22 Toronto House Surrey Quays Road	7-9 Surrey Quays Shopping Centre Redriff Road London
Flat 21 Toronto House Surrey Quays Road	Tesco Instore Pharmacy Surrey Quays Shopping Centre Redriff Road
Flat 19 Toronto House Surrey Quays Road	14 Surrey Quays Shopping Centre Redriff Road London
Flat 18 Toronto House Surrey Quays Road	35 Surrey Quays Shopping Centre Redriff Road London
Flat 16 Toronto House Surrey Quays Road	5 Surrey Quays Shopping Centre Redriff Road London
	10-12 Surrey Quays Shopping Centre Redriff Road London
	37 Surrey Quays Shopping Centre

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 3 Surrey Quays Shopping Centre Redriff  
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 45 Surrey Quays Shopping Centre  
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 Flat 45 29 Surrey Quays Road London  
 Flat 43 29 Surrey Quays Road London  
 Flat 42 29 Surrey Quays Road London  
 Flat 40 29 Surrey Quays Road London  
 Flat 20 7 Maritime Street London  
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 Flat 14 7 Maritime Street London  
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 25 Maritime Street London Southwark  
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 Flat 39 11 Maritime Street London  
 Flat 36 7 Maritime Street London  
 18 Surrey Quays Shopping Centre  
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 44 Surrey Quays Shopping Centre  
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 53-55 Surrey Quays Shopping Centre  
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 16 Surrey Quays Shopping Centre  
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 11-13 Surrey Quays Shopping Centre  
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29 Surrey Quays Shopping Centre  
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45C Surrey Quays Shopping Centre  
Redriff Road London

First Floor 61 Surrey Quays Shopping  
Centre Redriff Road

First Floor 57 Surrey Quays Shopping  
Centre Redriff Road

Outside Surrey Quays Shopping Centre  
Redriff Road

53 Surrey Quays Shopping Centre  
Redriff Road London

Sushi Momoda Surrey Quays Shopping  
Centre Redriff Road

36 Surrey Quays Shopping Centre  
Redriff Road London

38-40 Surrey Quays Shopping Centre  
Redriff Road London

Pizza 1889 Outside Surrey Quays  
Shopping Centre Redriff Road

Best Fast Food Ltd Surrey Quays  
Shopping Centre Redriff Road

Outside Red Shipping Container Surrey  
Quays Shopping Centre Redriff Road

**Re-consultation:**

**APPENDIX 5****Consultation responses received - Planning Permission 23/AP/0565****Internal services**

formal consultation and response to Pol  
Community Infrastructure Levy Team  
Design and Conservation Team [Formal]  
Local Economy  
Ecology  
Environmental Protection  
Highways Development and Management  
Flood Risk Management & Urban Drainage  
Urban Forester  
Waste Management  
Transport Policy

**Statutory and non-statutory organisations**

Environment Agency  
Historic England  
Metropolitan Police Service (Designing O  
Transport for London  
Thames Water

**Neighbour and local groups consulted:**

Via Email

Via Email

Via Email

## Recommendation – Planning Permission 23/AP/0566

This document shows the case officer's recommended decision for the application referred to below.

This document is not a decision notice for this application.

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<b>Applicant</b>	Charlotte N/A AIRE UK Canada Water GP PropCos Ltd, acting as general pa...	<b>Reg. Number</b>	23/AP/0566
<b>Application Type</b>	Approval of Reserved Matters		
<b>Recommendation</b>	APPROVE reserved matters	<b>Case Number</b>	468-C

### Draft of Decision Notice

**reserved matters is APPROVED for the following development:**

Approval of Reserved Matters (access, appearance, landscaping, layout and scale) pursuant to outline planning permission 21/AP/2610 which proposed hard and soft landscaping to create a shared public realm space for use by pedestrians and vehicles on Maritime Street. This is an application for subsequent consent accompanied by an Environmental Statement. Consequently the application is accompanied by a Statement of Conformity submitted pursuant to the Town and Country Planning (Environmental Impact Assessment) regulations 2017. This ES Statement of Conformity should be read in conjunction with the Canada Water Dockside Masterplan ES which can be viewed in full on the Councils website (21/AP/2610).

Maritime Street Canada Water London Southwark

**In accordance with application received on 1 March 2023 and Applicant's Drawing Nos.:**

Existing Plans

## Proposed Plans

Plans - Proposed CWD-TLA-MT-XX-DR-L-3001-XX REV P00 LOCATION PLAN  
received 13/06/2023

Plans - Proposed CWD-TLA-MT-XX-DR-L-3004-XX REV P00 SURFACE  
FINISH PLAN - MARITIME STREET received 13/06/2023

Plans - Proposed CWD-TLA-MT-XX-DR-L-5001-XX REV P00 TREE PLANTING  
PLAN - MARITIME STREET received 13/06/2023

Plans - Proposed CWD-TLA-MT-XX-DR-L-7001-XX REV P00 MARITIME  
STREET SECTIONS received 13/06/2023

Plans - Proposed CWD-TLA-RMA-XX-DR-L-3002-XX REV P00 LANDSCAPE  
MASTERPLAN - PUBLIC REALM received 13/06/2023

Plans - Proposed CWD-TLA-RMA-XX-DR-L-3003-XX STREET SURFACE  
PLAN - PLOT A & MARITIME STREET received 13/06/2023

Plans - Proposed 01 Mar 2023 CWD-TLA-MT-XX-DR-L-3004-XX REV P00  
SURFACE FINISH PLAN - MARITIME STREET received 13/06/2023

## Other Documents

Tree survey and assessment CWD-TMA-RMA-XX-RP-Z-0002-XX-P02 AIA  
ADDENDUM received 13/06/2023

Document EIA Statement of Conformity received 14/07/2023

Document Equalities Impact Assessment CWD-TRI-RMA-X X-RP-Y-0002-X X-P00  
received 13/06/2023

Document SCI CWD-KAN-RMA-XX-RP-Y-0001-XX-P00 received 13/06/2023

Fire Statement C W D -OFR -M T-X X-R P -Y-000 1-X X-P 00 received 13/06/2023

Document DCC CWD-KAN-DE-XX-RP-Y-0002-XX-P00 received 13/06/2023

Design and access statement Landscape DAS C W D-T L A-R M A-X X-R P -L-000 1-  
X X-P 00 received 13/06/2023

Planning statement CWD - RMA PLANNING STATEMENT V2.PDF received  
14/07/2023

Transport assessment/statement CWD-WSP-MT-X X-RP-Y-0001-X X-P00 received  
13/06/2023

## Informatives

- 1 Paragraph 3.12.9 of Policy D12 explains that Fire Statements should be



produced by someone who is:

"third-party independent and suitably-qualified" The council considers this to be a qualified engineer with relevant experience in fire safety, such as a chartered engineer registered with the Engineering Council by the Institution of Fire Engineers, or a suitably qualified and competent professional with the demonstrable experience to address the complexity of the design being proposed. This should be evidenced in the fire statement. The council accepts Fire Statements in good faith on that basis. The duty to identify fire risks and hazards in premises and to take appropriate action lies solely with the developer.

The fire risk assessment/statement covers matters required by planning policy. This is in no way a professional technical assessment of the fire risks presented by the development. The legal responsibility and liability lies with the 'responsible person'. The responsible person being the person who prepares the fire risk assessment/statement not planning officers who make planning decisions.

## APPENDIX 3

## Relevant planning history - Planning Permission 23/AP/0566

Reference and Proposal	Status
<p><b>21/AP/2610</b> Outline planning permission (all matters reserved) for works of hard and soft landscaping to create a shared public realm space for use by pedestrians and vehicles. The proposals will retain and re-provide access for servicing vehicles to Porters Edge, and will improve the street through the provision of new trees and other planting, alongside new street furniture and surface finishes to enable the use of the space for play and recreation.</p>	GRANTED- Minor Application 18/01/2023

## Consultation undertaken - Planning Permission 23/AP/0566

**Site notice date:** 13/03/2023

**Press notice date:** 16/03/2023

**Case officer site visit date:** n/a

**Neighbour consultation letters sent:** 09/03/2023

### Internal services consulted

Archaeology

Design and Conservation Team [Formal]

Ecology

Environmental Protection

Highways Development and Management

Section 106 Team

Flood Risk Management & Urban Drainage

Urban Forester

Waste Management

Transport Policy

### Statutory and non-statutory organisations

Environment Agency

London Fire & Emergency Planning Authori

London Underground

Metropolitan Police Service (Designing O

Transport for London

Thames Water

### Neighbour and local groups consulted:

643 Pavillion House Water Gardens  
Square London

635 Pavillion House Water Gardens  
Square London

625 Pavillion House Water Gardens  
Square London

614 Pavillion House Water Gardens  
Square London

611 Pavillion House Water Gardens  
Square London

603 Pavillion House Water Gardens

Square London

691 Pavillion House Water Gardens  
Square London

682 Pavillion House Water Gardens  
Square London

644 Pavillion House Water Gardens  
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601 Pavillion House Water Gardens  
Square London

563 Giverny House Water Gardens  
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566 Giverny House Water Gardens Square London  
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433 Heligan House Water Gardens Square London  
426 Heligan House Water Gardens Square London  
416 Heligan House Water Gardens Square London  
411 Heligan House Water Gardens Square London  
403 Heligan House Water Gardens Square London  
471 Heligan House Water Gardens Square London  
672 Pavillion House Water Gardens Square London  
663 Pavillion House Water Gardens Square London  
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304 Eden House Water Gardens Square London  
351 Eden House Water Gardens Square London  
301 Eden House Water Gardens Square London  
661 Pavillion House Water Gardens Square London  
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 25 Maritime Street London Southwark  
 Flat 42 11 Maritime Street London  
 Flat 41 11 Maritime Street London  
 Flat 39 11 Maritime Street London  
 Flat 36 7 Maritime Street London  
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 555 Giverny House Water Gardens



Square London  
553 Giverny House Water Gardens  
Square London

654 Pavillion House Water Gardens  
Square London

**Re-consultation:**

**Consultation responses received - Planning Permission 23/AP/0566****Internal services**

Design and Conservation Team [Formal]  
Ecology  
Environmental Protection  
Highways Development and Management  
Flood Risk Management & Urban Drainage  
Urban Forester  
Transport Policy

**Statutory and non-statutory organisations**

Environment Agency  
London Fire & Emergency Planning Authori  
London Underground  
Metropolitan Police Service (Designing O  
Transport for London  
Thames Water

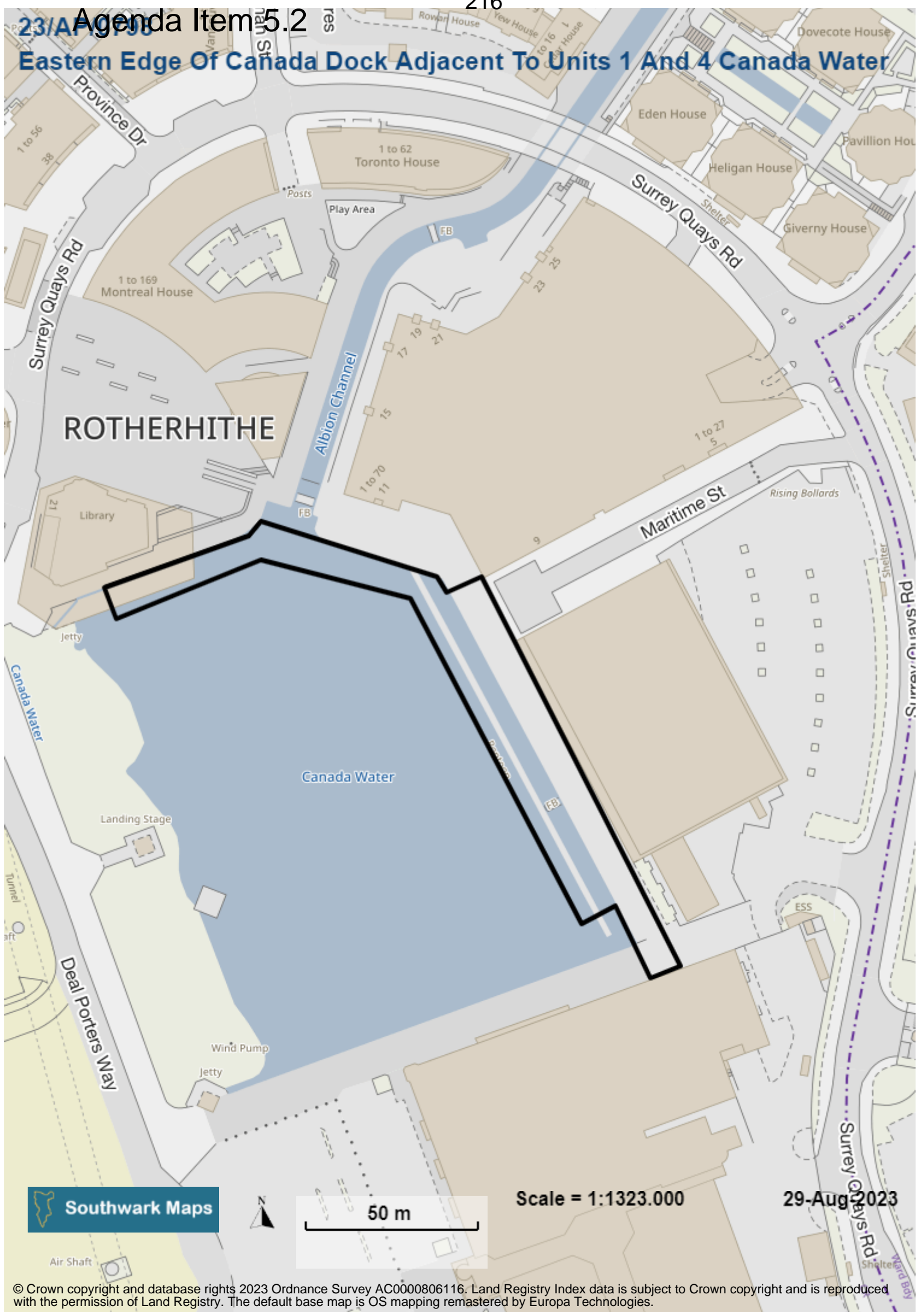
**Neighbour and local groups consulted:**

Via Email



# Agenda Item 5.2

## Eastern Edge Of Canada Dock Adjacent To Units 1 And 4 Canada Water



**ROTHERHITHE**

Canada Water



Scale = 1:1323.000

29-Aug-2023

Southwark Maps

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<b>Item No.</b> 5.2	<b>Classification:</b> OPEN	<b>Date:</b> 13 September 2023	<b>Meeting Name:</b> Planning Committee (Major Applications) B
<b>Report title:</b>	<b>Development Management planning applications:-</b> Application 23/AP/0798 for: Full Planning Application  <b>Address:</b> Eastern Edge Of Canada Dock Adjacent To Units 1 And 4 Canada Water Retail Park, London, Southwark, SE16  <b>Proposal:</b> Works to improve and enhance the eastern edge of Canada Dock, including the creation of a public pedestrian promenade at ground level and lower level boardwalk with associated amenity space, soft landscaping, tree planting and associated works together with the relocation and refurbishment of the existing fishing pontoon and the creation of a new fishing pontoon.		
<b>Ward(s) or groups affected:</b>	Rotherhithe		
<b>From:</b>	Director of Planning and Growth		
<b>Application Start Date:</b> 15.05.2023		<b>PPA Expiry Date</b> n/a	
<b>Earliest Decision Date:</b> 15.05.2023			

## RECOMMENDATION

1. That planning permission be granted subject to conditions, as set out in the draft recommendation at Appendix 1.
2. That the Planning Committee in making their decision has due regard to the potential Equalities impacts that are outlined in this report.

## EXECUTIVE SUMMARY

3. The application site, comprising the eastern edge and part of the northern edge of Canada Water Dock, is identified as a Site of Importance of Nature Conservation and Open Water Space. The site is located immediately adjacent to two major regeneration schemes, the AIRE Canada Water Dockside Masterplan (22/AP/2655) and the British Land Canada Water Masterplan

(18/AP/1604). This application has been submitted pursuant to an obligation of the Section 106 agreement attached to the Canada Water Dockside Outline Planning Permission (22/AP/2655).

4. The application seeks to provide a public pedestrian promenade at ground level and a lower level boardwalk with associated amenity space, soft landscaping, tree planting and associated works together with the relocation and refurbishment of the existing fishing pontoon and the creation of a new fishing pontoon. The proposed works are intended to transform this part of the dock complementing the recently approved (under construction) British Land proposals for the southern and western sides of the dock. The combined enhancements will deliver a significant piece of public realm for the town centre, making the dock a destination whilst respecting and enhancing its ecological status.
5. The proposed development would result in a Biodiversity Net Gain increase of 12.23%, exceeding the required 10% net gain established by the Environment Act 2021. The proposed development was Screened under the Environmental Impact Regulations and was deemed not to constitute EIA development, however the EIA Screening Opinion (22/AP/3601) stated that there is a potential risk for disturbance and contamination during construction. Construction effects can be managed by way of a Construction Management Plan which should be secured by the recommended condition. The proposed development is not considered to result in any negative amenity impacts and short term, temporary noise and vibration effects generated during the construction of the proposed development would be managed through the CEMP.
6. The Environmental Impact Report submitted with this application identifies two impacts of potential risk to aquatic ecology, however they have been addressed through recommended mitigation which adequately manages and reduces the risk (discussed in further detail in the ecology section of this report below). The Ecology Team has reviewed the report and agrees with the conclusions subject to a condition to ensure that construction works are monitored by an Ecological Clerk of Works.
7. Whilst the priority is for the proposed development to be delivered by the Applicant, as a key piece of public realm connected to the redevelopment of the Canada Water Dockside site, it is recognised that the Dock is not owned or managed by the Applicant and it may not be possible for them to obtain all necessary consents to undertake the works. Consequently there is a fall-back position, secured with the s106 agreement for 21/AP/2655 for the Applicant to use all reasonable endeavours to obtain necessary consents but in the event that it is not possible they will be required to submit a full Scope of Works including a detailed Cost Plan to the Council to demonstrate the cost of delivering the scheme. Once the full costs have been agreed by the Council the Applicant is obligated to make a payment in lieu for the Council to deliver public realm improvements. At this stage the Applicant has provided an indicative Cost



Plan (circa £4.5 million). The cost plan has not been scrutinised as part of this application and would not form part of the planning approval. A full cost plan would be subject to scrutiny and agreement under a separate s106 obligation at the appropriate time. The first priority is for the Applicant to secure planning permission for an appropriate proposal and to deliver the proposed works but sufficient controls are in place within the s106 agreement to secure a payment in lieu if necessary in the future.

8. The proposed design and landscaping of the scheme would significantly improve the existing condition of the site and would encourage walking and cycling helping to create a safe, legible and attractive piece of public realm. The proposed development would enhance the environmental quality of the site, provide new and improved amenity space, inclusive access, enhanced landscaping, planting and opportunities to dwell, socialise and experience the SINC.
9. Overall, the proposal responds positively to design, landscaping and environmental policies and subject to the appropriate mitigation during construction as secured by the recommended conditions, the proposal would not adversely affect the amenity for nearby residents. Overall the proposal accords with the objectives of the Canada Water Dockside Masterplan.

## **BACKGROUND INFORMATION**

### **Site description and its role within the Canada Water Dockside Masterplan**

10. The proposed development relates to the eastern edge and part of the northern edge of Canada Water Dock. The site comprises existing floating fishing pontoons (with controlled access, approximately 120 metres in length) and a concrete deck structure designated as adopted highway. The upper promenade of the eastern dock edge is higher than the north, south and west sides and there is limited vegetation and landscaping surrounding the existing dock edge.



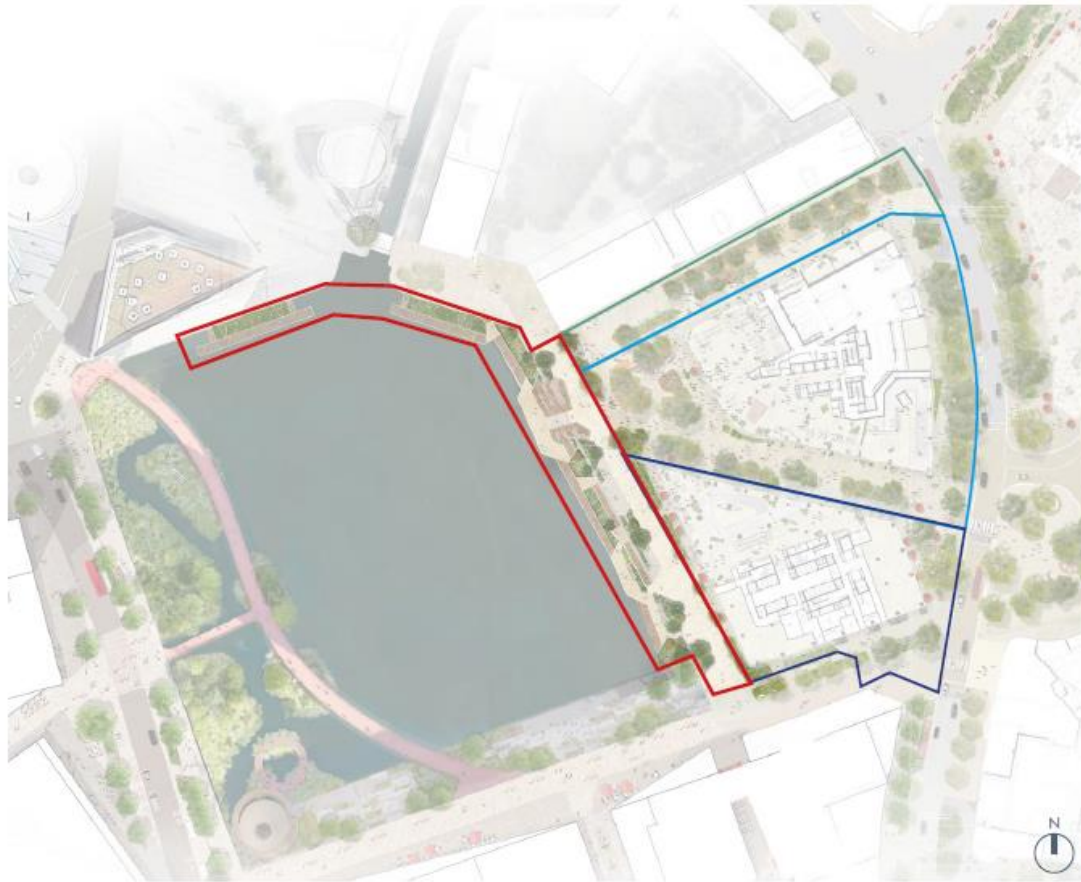
*Image above: Floating fishing pontoon located along the eastern edge of the dock*



*Image above: Existing aerial view of Canada Water eastern dock edge*

11. The following area designations apply:
- Site allocation NP80 Decathlon Site and Mulberry Business Park
  - Canada Water Major Town Centre
  - Canada Water Action Area
  - Canada Water Opportunity Area
  - Site of Importance for Nature Conservation
  - Flood Zone 2 and 3
  - Open Water Space

- Air Quality Management Area
  - Strategic Heating Area
  - Hot food takeaway secondary school exclusion zone
12. The site is located immediately adjacent to two major development schemes, the Art Invest Canada Water Dockside Masterplan (22/AP/2655) to the east, and the British Land Canada Water Masterplan to the west and south (18/AP/1604). This application has been submitted pursuant to an obligation of a Section 106 agreement attached to the Canada Water Dockside Outline Planning Permission. Schedule 6, paragraphs 1.11 to 1.24 contain the relevant provisions and details are provided in the relevant section of this report.



## Key

- Dock Edge Scheme application boundary
- Reserved Matters application boundary for the development of A1
- Reserved Matters application boundary for the development of A2
- Reserved Matters application boundary for the development of Maritime Street

Note: Areas shown outside the Dock Edge Scheme are indicative only

*Image above: The eastern dock edge application boundary and the Canada Water Dockside Masterplan application boundary*

13. In the existing condition, Canada Water Dock is bound by Surrey Quays Shopping Centre to the South, Deal Porters Way to the West, Canada Water Library, Underground station and Deal Porter Square to the north, Albion Channel to the north-east and Decathlon and Dock X to the east. Within the Canada Water Dockside Masterplan, the Dock X building would be demolished and the plot would be redeveloped to provide Buildings A1 and A2 and associated public realm. Maritime Street is also proposed to be redeveloped, to the north of Building A2 (21/AP/2610). In addition, within the British Land Canada Water Masterplan, work is progressing on Plot A, located over Deal

Porters Way to the west of the Dock (completion expected 2024) and Zone D would be located to the south (in the location of the current Shopping Centre), however a Reserved Matters Application has not yet been submitted for Zone D.

14. The existing water level in the dock is variable over the year, however the Hydrology Report measures (as an average) the Dock as 3 metres in depth. The eastern dock edge is approximately 120 metres in length and the area of dock covered by this application is approximately 1996.5 sqm, (10.7% of the entirety of the Dock area 18560 sqm). The remainder of the Dock sits outside of the scope of this application but has been granted planning permission under 21/AP/3794 (Canada Water British Land Masterplan), for the re-grading and re-planting of the SINC, construction of a new boardwalk, construction of steps and accessible slopes along the southern edge and associated public realm, informal play space and landscape improvements.
15. Canada Water Dock was constructed in 1876 on the site of two former timber ponds. The site is not located within a Conservation Area nor within the curtilage of a Listed Building. Local parks and open spaces located nearby the site include King George's Field Park approximately 280 metres west of the site, Southwark Park 380 metres south west of the site, Greenland Dock 380 metres south east, Russia Dock Woodlands 480 metres to the east and Surrey Water 600 metres to the north.
16. The site's Public Transport Accessibility Level (PTAL) rating is 6a, indicating the second highest level of access to the public transport network. The site is located approximately 140 metres from Canada Water station, which is served by the London Overground and Jubilee Line. Surrey Quays Station is approximately 430 metres from the site. Canada Water Bus Station is located above the tube station and services Central London, Canary Wharf and Stratford, with links to South London. The roads in the immediate vicinity of the site, including the dock edges are adopted highway. Cycleway 14 is in close proximity to the site and planned Cycleway 4 (from Tower Bridge to Cutty Sark) is anticipated nearby to the site.

## **Details of proposal**

17. This application relates to the eastern and part of the northern edge of Canada Dock and seeks to provide the following works:
  - Creation of upper and lower deck areas along the eastern edge of the dock, along with several amenity spaces, landscaping and pedestrian walkways/step free access connecting the various areas and connecting to the wider Canada Water Masterplan development. The existing concrete deck structure would be retained in situ (the upper deck)

- Terraced amphitheatre facing both the Dock and Waterfront Square (part of the Canada Water Docks Masterplan)
- Relocation and refurbishment of the existing fishing pontoon and the creation of a new fishing pontoon for Young Anglers (3 fishing pontoons totalling 75 metres)



*Image above: Illustrative landscape plan*





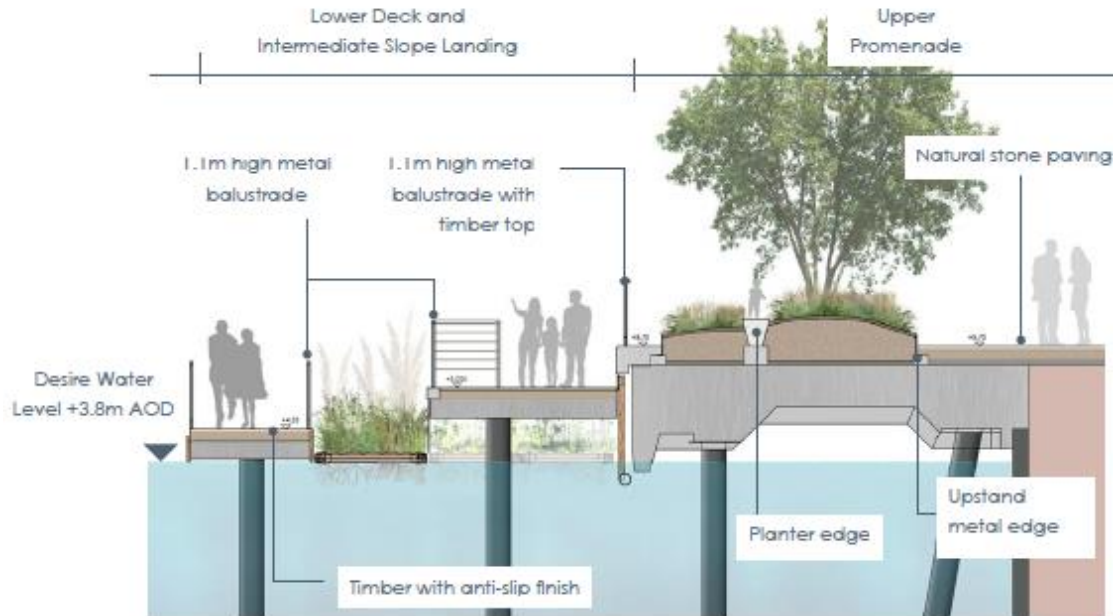
*Image above: Illustrative layout*

Upper and lower deck areas

18. The proposed development would provide an upper promenade along the dock edge, with a 1.1 metre high metal balustrade, with both stepped and sloped

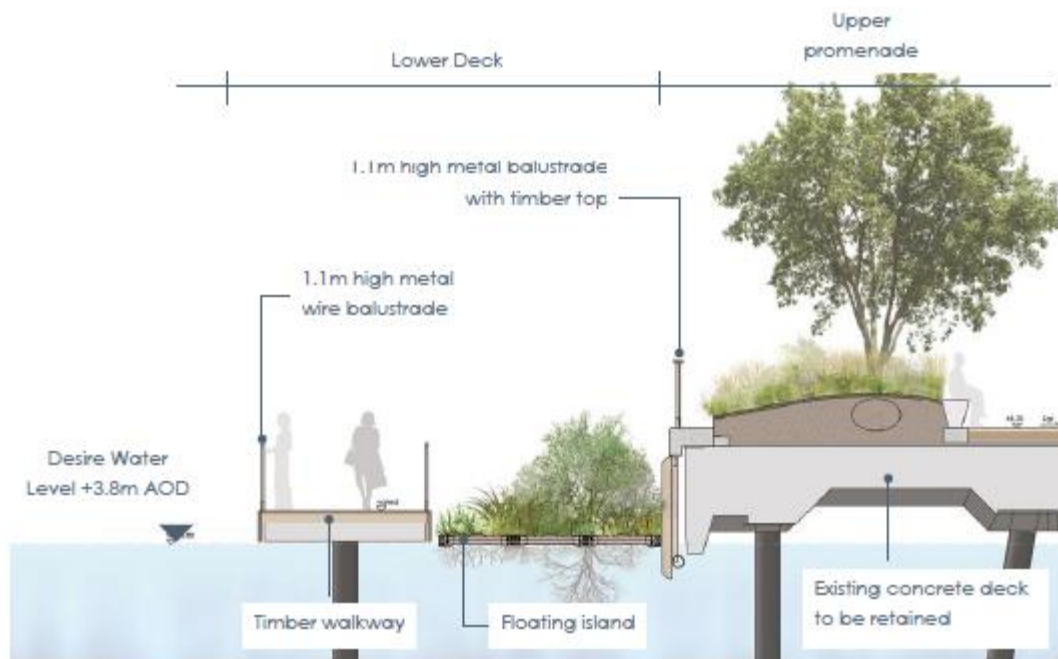


access to the lower deck (with a 1:21 gradient) and an intermediate level landing to provide opportunities for dwell time and rest. The upper deck would continue to form part of Southwark adopted highway and would provide a minimum 5 metre access for both pedestrians and cyclists. The upper deck would comprise raised planters and seating on the western edge (water side) and tree planting along the A1 building line.



*Image above: Illustrative section of upper promenade*

19. The lower deck would provide an alternative walkway closer to the water level, with smaller seating pockets and planting. The walkway would have a minimum clear width of 2 metres with anti-slip timber decking and 1.1 metre high balustrades would be provided on each side of the deck.
20. Floating islands are proposed along the lower deck which would comprise shrubs, reeds and planting.



*Image above: Illustrative section of lower deck and floating island*

### Amphitheatre

21. The proposed amphitheatre would connect the upper and lower deck and would face both the Dock and Waterfront Square. The amphitheatre would be accessed via central stairs and a step free route to the lower level from the north and south. The amphitheatre would comprise wheelchair space at the lower deck and raised planters in the design of the steps. A 1.1 metre high metal wire balustrade is proposed on the water's edge, for safety reasons.



*Image above: Illustrative view of the amphitheatre*

### Fishing pontoons

22. The existing fishing pontoon would be relocated to the northern edge of the Dock with direct access from the upper side of the dock edge. Floating planting is proposed between the dock edge and the pontoon with resurfaced decking and a balustrade proposed at the back of the pontoons. A total of three pontoons are proposed in lengths of 35 metres, 25 metres and 15 metres. The dedicated Young Anglers' area, in the south eastern corner of the Dock, would comprise a metal balustrade to demarcate the space and separate it from the public lower deck route. Access to the area would be provided from either end of the balustrade.
23. The proposed development would allow for a future pontoon connection to the southern edge of the Dock as part of the adjacent British Land scheme, however this connection does not form part of this application and there is no onus on either British Land or Artinvest to deliver a future connection. The current designs would enable the eastern and southern edge improvements to be erected independently of each other and both elements are considered to be of appropriate high quality design. The separate elements are complementary in their design and do not impede access to either element but should a future connection be feasible this could be achieved with minimal alterations.
24. In total, the construction of the development is estimated to be approximately 18 months subject to detailed construction planning, as stated in the Environmental Impact Report.
25. The proposed minimum target water level is 3.8 metres, which is to be maintained by groundwater abstraction. The maximum water level would be 3.92 metres, at which point it would flow into the Albion Channel. The Councils Ecologist has confirmed that this is an appropriate water level for the dock given its ecological condition and the detailed planting proposals for the dock in its entirety.



30. With regard to wind mitigation, the applicant has provided a temporary mitigation strategy to cover the event that the dock edge works and adjacent Building A1 on the CWD site are complete and ready for occupation before Zone D of the British Land Masterplan is complete. The temporary wind mitigation proposals are acceptable (discussed in more detail later in this report). A pre commencement condition is recommended to secure detailed design proposals and full wind modelling for the temporary mitigation features. Officers are satisfied with this approach.
31. In terms of comments regarding public realm and the need for a coordinated approach to lighting, materials and signage around the site, the Design Team consider the proposed material palette to be acceptable and noted that the materials are not the same as those proposed at the southern and western edge of the dock, however the Design Team take the view that the overall treatment is consistent. The Design Team state that the different material palette would help establish a separate character for the eastern dock edge and would highlight the different public realm it offers. Specific design details of the proposed development, including materials and signage should be secured by the recommended condition.
32. In response to comments received on lighting, the applicant updated the Lighting Proposals to include a detailed light spill assessment and anticipated Lux levels (developed in accordance with the applicant's ecologist). The details have been reviewed by the Councils Ecologist and found to be acceptable. A detailed lighting condition is recommended.
33. One of the representations in support cited the inclusion of new public spaces for the community to enjoy the environment and be closer to nature, new landscaping and greenery to support biodiversity, the provision of places to sit and relax for local people to enjoy the Dock (which has been neglected for far too long) and the proposed design as reasons for support.
34. Another representation in support was on behalf of The Friends of Southwark Park Charity who stated that the proposed development and CWD Masterplan would be a fantastic improvement on what is currently on the site. The representation states that the improvements for Canada Water Dock/Dock Edge are very welcome as it has been a long wait for the area to be transformed and the development can help people make better use of the Dock.

### **Planning history of the application site and nearby sites**

35. Appendix 3 sets out in detail the full planning history for the site as well as details of relevant applications on adjoining or nearby sites.

36. 22/AP/3601. Request for an EIA Screening Opinion for proposed works to the eastern dock edge of the Canada Water Basin. SCR- Screening Opinion – EIA Regs. 21.11.22
37. 18/AP/1604. Hybrid application seeking detailed planning permission for Phase 1 and outline planning permission for future phases, comprising:  
 Outline planning permission (all matters reserved) for demolition of all existing structures and redevelopment to include a number of tall buildings comprising the following mix of uses: retail (Use Classes A1-A5), workspace (B1), hotel (C1), residential (C3), assisted living (C2), student accommodation, leisure (including a cinema)(D2), community facilities (including health and education uses)(D1), public toilets, nightclub, flexible events space, an energy centre, an interim and permanent petrol filling station, a primary electricity substation, a secondary entrance for Surrey Quays Rail Station, a Park Pavilion, landscaping including open spaces and public realm, works to Canada Water Dock, car parking, means of access, associated infrastructure and highways works, demolition or retention with alterations to the Press Hall and/or Spine Building of the Printworks; and  
 Detailed planning permission for the following Development Plots in Phase 1:  
 Plot A1 (south of Surrey Quays Road and west of Deal Porters Way) to provide uses comprising retail (A1-A5), workspace (B1) and 186 residential units (C3) in a 6 and 34 storey building, plus basement;  
 Plot A2 (east of Lower Road and west of Canada Water Dock) to provide a leisure centre (D2), retail (A1-A5), and workspace (B1) in a 4, 5 and 6 storey building, plus basement; Plot K1 (east of Roberts Close) to provide 79 residential units (C3) in a 5 and 6 storey building;  
 Interim Petrol Filling Station (north of Redriff Road and east of Lower Road) to provide a petrol filling station with kiosk, canopy and forecourt area. Each Development Plot with associated car parking, cycle parking, landscaping, public realm, plant and other relevant works. GRMAJ – Major – GRANTED. 30.09.19
38. 21/AP/3794. Application for the approval of reserved matters (Access, Appearance, Landscaping, Layout and Scale) in relation to Canada Dock and land adjacent to Zone D pursuant to hybrid planning permission ref. 18/AP/1604 dated 29th May 2020, relating to the re-development of Canada Dock, including the re-grading and re-planting of the SINC, construction of a new boardwalk, construction of steps and accessible slopes along the southern edge and associated public realm, informal play space and landscape improvements. This is an application for subsequent consent accompanied by an Environmental Statement. Consequently the application is accompanied by a Statement of Conformity submitted pursuant to the Town and Country Planning (Environmental Impact Assessment) regulations 2017. This ES Statement of Conformity should be read in conjunction with the Canada Water Masterplan ES which can be viewed in full on the Councils website (18/AP/1604). GRAOR – Reserved Matters – GRANTED. 18.01.22



39. 21/AP/2610. Outline planning permission (all matters reserved) for works of hard and soft landscaping to create a shared public realm space for use by pedestrians and vehicles. The proposals will retain and re-provide access for servicing vehicles to Porters Edge, and will improve the street through the provision of new trees and other planting, alongside new street furniture and surface finishes to enable the use of the space for play and recreation. GRMIN – Minor – GRANTED. 21.03.22
40. 21/AP/2655. Outline planning permission (all matters reserved) for a commercial masterplan containing offices and other complementary town centre uses with no housing. The proposals involve the demolition of all buildings and structures and the comprehensive redevelopment of the site to provide three Development Plots (A1, A2 and B) which will contain three Buildings (A1, A2 and B) above ground with basements. The proposed land uses are: offices (Class E), retail/professional services/food and drink (E), learning and non-residential institutions/local community (F1/F2), medical or health (E) and indoor sport, recreation or fitness (E). - Works of hard and soft landscaping are proposed to create a series of new and improved public realm spaces within the site. This will include the replacement of some trees and the planting of additional new trees. - New vehicular access points are proposed to be created from Surrey Quays Road and Canada Street, along with other incidental works. GRMAJ – Major – GRANTED. 21.03.22

## **KEY ISSUES FOR CONSIDERATION**

### **Summary of main issues**

41. The main issues to be considered in respect of this application are:
- Principle of the proposed development in terms of land use;
  - Relationship with the Canada Water Dockside Outline Planning Permission (21/AP/2655)
  - Landscaping, trees and urban greening
  - Ecology and biodiversity
  - Environmental impact assessment
  - Land contamination
  - Design
  - Wind microclimate
  - Sustainability
  - Impact of proposed development on amenity of adjoining occupiers and surrounding area
  - Light pollution
  - Transport and highways
  - Water resources, flood risk and SUDs



- Future Dock management
  - Fire safety
  - Planning obligations
  - Mayoral and Borough Community Infrastructure Levies
  - Community involvement and engagement
  - Consultation responses from external and statutory consultees
  - Consultation responses from internal consultees
  - Community impact and equalities assessment
  - Human rights implications
  - Positive and proactive statement
42. These matters are discussed in detail in the 'Assessment' section of this report.

### **Legal Context**

43. Section 38(6) of the Planning and Compulsory Purchase Act (2004) requires planning applications to be determined in accordance with the development plan, unless material considerations indicate otherwise. In this instance the development plan comprises the London Plan 2021 and the Southwark Plan 2022. Section 66 of the Planning (Listed Buildings and Conservation Areas) Act 1990 requires decision-makers determining planning applications to pay special regard to the desirability of preserving listed buildings and their setting or any features of special architectural or historic interest which they possess.
44. There are also specific statutory duties in respect of the Public Sector Equalities Duty which are highlighted in the relevant sections below and in the overall assessment at the end of the report.

### **Planning policy**

45. The statutory development plans for the borough comprise the London Plan 2021 and the Southwark Plan 2022. The National Planning Policy Framework 2021 is a material consideration but not part of the statutory development plan. A list of policies which are relevant to this application is provided at Appendix 2. Any policies which are particularly relevant to the consideration of this application are highlighted in the report.

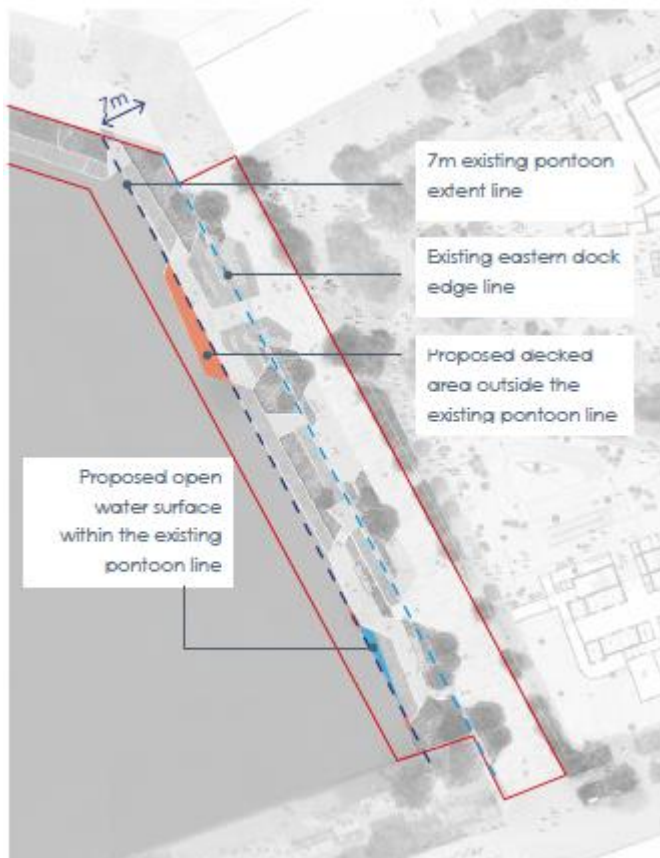
## **ASSESSMENT**

### **Principle of the proposed development in terms of land use**

46. Canada Water Dock is identified as a Site of Importance of Nature Conservation and Open Water Space. Chapter 8 of The National Planning Policy Framework recognises that access to high quality open spaces (including areas of water) is important for the health and wellbeing of communities, and can deliver wider

benefits for nature and support efforts to address climate change. The NPPF states that existing open space should not be built on unless the loss resulting from the proposed development would be replaced by the equivalent or better provision in terms of quantity and quality.

47. The proposed development encroaches into the Dock in specific areas for pedestrian safety and circulation. The proposed amphitheatre, which would exceed the 7 metre existing pontoon extent line, would provide a space for integrated seating and access down to the water level from the upper promenade and would enhance the visitor experience of the SINC. The proposed area within the 7 metre line would also include areas of planting and aquatic habitat, so the minor encroachment is not considered to result in any negative impacts on the SINC and the positive benefits of the enhanced public realm, fishing pontoons and ecological planting provide sufficient justification for encroachment into the open water space.



*Image above: Extent of proposed and open water surface diagram*

48. The principle of the proposed development in terms of land use is supported as it would provide new and improved amenity space, improved access within and to the site, enhanced landscaping, planting and opportunities to dwell, socialise and experience the SINC. P58 of the Southwark Plan 2022 supports this as it

states that development within designated Open Water Space will only be permitted when it consists of ancillary facilities which positively contribute to the setting, accessibility and quality of the open space does not affect its openness or detract from its character. In addition, development that affects designated open water spaces must ensure that all safety and navigational impacts are minimised. A Water Safety Review authored by the Royal Society for the Prevention of Accidents has been submitted to support the application.

49. Canada Water Dock is an important local asset and Policy SI 17 of the London Plan 2021 states that development proposals along docks should respect their local character, environment and biodiversity and should contribute to their accessibility and active water related uses. Policy GG2 states that development must understand what is valued about existing places and use this as a catalyst for growth, renewal, and place making. This proposal would enhance the landscaping, biodiversity and public realm of the Dock, provide fishing pontoons and promote the overall enjoyment of the generally underutilised site.
50. Policy GG2 emphasises that to create successful sustainable mixed-use places that make the best use of land, development must protect and enhance designated nature conservation sites and promote the creation of new green infrastructure, urban greening and aim to secure net biodiversity gain where possible. This is corroborated by P60 of the Southwark Plan 2022. The proposed development would provide soft landscaping, tree planting and result in a Biodiversity Net Gain increase of 12.23%, exceeding the required 10% net gain established by the Environment Act 2021.
51. The proposed development would significantly improve the existing condition of the eastern edge of the Dock through the provision of trees, soft landscaping and other green infrastructure. P13 of the Southwark Plan 2022 states that development must provide landscaping which is appropriate to the context, including the provision and retention of street trees and provide the use of green infrastructure through the principles of water sensitive urban design.
52. P13 also champions accessible and inclusive design for all ages and people with disabilities and states that there must be provision of adequate outdoor seating for visitors. P14 focuses on design quality and states that development must provide a positive pedestrian experience. London Plan Policies D5 and D8 also corroborate the need to deliver good design. The proposed development is considered to create a high quality, inclusive environment and would introduce more seating opportunities to experience the Dock at a closer level.
53. Overall, the principle of the proposed development is supported and encouraged. The design of the dock edge development would integrate with the Canada Water Dockside Masterplan and Zone D of the neighbouring Canada

Water British Land Masterplan, thereby creating a civic centre surrounding Canada Water Dock.

### **Relationship with the Canada Water Dockside Outline Planning Permission (21/AP/2655)**

54. Canada Water Dock sits outside of the applicant's ownership, however the proposed works to the eastern dock edge are pursuant to an obligation of the Section 106 Agreement attached to the Canada Water Dockside Outline Planning Permission (21/AP/2655) dated 18 January 2023.
55. Dock Edge Works are defined in the s106 agreement as works to improve the eastern side of the Dock to be prepared by the Developer and submitted to the Council for approval and which shall, unless otherwise agreed by the Council, include but not be limited to the relocation and refurbishment of the existing fishing pontoon and refurbishment of the eastern dock edge, including parts of new construction, public pedestrian promenade at ground level and lower level boardwalk together with associated amenity space. The proposals shall include significant soft landscaping which shall result in demonstrable ecological improvements to the Dock, lighting, seating, step free access between the upper and lower levels; and up to 75 metres of new fishing pontoons. The works shall be designed to a standard commensurate with the Development and surrounding Canada Water Masterplan and shall cover the location identified for indicative purposes within the area shaded green on Plan 16 in the Section 106 agreement. This submitted application satisfies this requirement.
56. Schedule 6, contains the relevant provisions for the Dock Edge Works. The obligation requires that a planning application will need to be submitted with a 'detailed scope of the Dock Edge Works', within 12 months of the outline masterplan permission being granted. The submission of this application meets that obligation.
57. Whilst the priority is for the proposed development to be delivered by the Applicant, there is a fall-back position, whereby a financial payment will be made to the Council to deliver public realm enhancements should the Applicant not be able to obtain all necessary consents to deliver the works onsite. The s106 does not specify an amount to be paid at this stage but requires the full costs of the proposal to be demonstrated and the agreed full costs for delivery to be paid at the appropriate time. An estimated cost analysis was submitted with this application. A full cost analysis will be submitted and assessed under the s106 obligation at the appropriate time. The first step and priority for the Council is for the applicant to secure planning permission for an acceptable detailed proposal.
58. Public Ream, as defined in the s106 agreement for 21/AP/2655, includes the Dock Edge Works. In terms of maintenance of the public realm, Schedule 6,

Paragraph 3 details that the Developer shall at its own expense permanently maintain, cleanse, drain and keep maintained, cleansed and drained the public realm. An effective system of lighting shall also be permanently maintained at the Developer's expense.

59. Schedule 6, Paragraph 4 of the s106 agreement sets out access arrangements for the public realm and secures full unrestricted access to the public realm 24 hours a day everyday throughout the calendar year (save for 1 day per year) for the duration that the development plot or any part of the development plot remains occupied.
60. Whilst this application for the dock edge works is separate to the Canada Water Dockside OPP and associated RMAs, the public realm proposals for the CWD site are intrinsically linked to the dock edge and the dock edge is considered to be an essential part of the overall scheme. It is for this reason that delivery of the dock edge enhancements was secured in the s106 agreement and the proposal has been designed to coordinate with the public realm proposals for the Phase 1 of the CWD masterplan. Along the eastern frontage to A1, it is proposed to include tree planting and areas for outdoor eating and socialising. The southern edge of A1 forms a continuation of the Dock Walk, proposed as part of the adjacent British Land planning permission 21/AP/3794. Proposals for the Dock Walk would integrate with the approved space, creating pockets of planting and seating.
61. The amphitheatre proposed towards the northern end of the dock edge would integrate successfully with the proposals for Waterfront Square which will be delivered as part of the CWD Phase 1 development. The OPP comprises Design Codes for Landscaping which sets out guidelines for The Waterfront, the key movement route between buildings A1 and A2 and the dock edge. The Design Codes establish indicative zones for café spill out associated with the ground floor building uses, indicative office lobby entrances and locations for seating along the dock walk. The Design Codes set out that trees and planting should be provided along The Waterfront building edge to reinforce a green character and to create a buffer for commercial spill out spaces and to contribute to wind mitigation.
62. The Soft Landscape Strategy in the Design Codes suggest incorporating multi stem trees with species to reflect characters established around the other side of the dockside and tree selection should include evergreen trees to mitigate wind. Trees along the edge of A1 should be a maximum of 5 metres in height to allow for positioning beneath the building overhang.
63. With regard to hard landscaping, a high quality and durable materials palette should be provided with warm colour tones and a moderate colour variation in paving mixes preferred. The Design Codes state that paving bonds and sizes could reference the historic context and use of the dockside to float timbers,

utilising longer module sizes. Fixed street furniture must be provided to ensure accessible seating for all, including different heights of seating and a sufficient proportion to include arm and back rests.

64. The submitted material for this application generally accords with the Design Codes for the CWD Masterplan, with further details in the relevant sections of this report and conditions secured to ensure the delivery of high quality public realm.

### **Landscaping, trees and urban greening**

65. P13 of the Southwark Plan 2022 states that development must provide landscaping which is appropriate for the context, including the provision and retention of street trees and provide the use of green infrastructure through the principles of water sensitive urban design, including quiet green spaces and tree pit rain gardens. P59 of the Southwark Plan states that green infrastructure should be designed to provide multiple benefits for the health of people and wildlife, be adaptable to climate change, integrate with the wider green infrastructure network and upgrade the walking and cycling networks between spaces to promote a sense of place and ownership for all. P60 emphasises that development must contribute to net gains in biodiversity through enhancing the nature conservation value of SINC's and including features such as soft landscaping and buffering of existing habitats.
66. P61 focuses on trees commensurate to the scale and type of development and states that they should be adaptable to climate change and support native species. Policies G1, G5, G6 and G7 of the London Plan 2021 are also relevant for this section of the report.
67. The proposed landscaping design aims to create high quality public realm incorporating a continuous network of spaces connecting the dock edge and Canada Water Dockside Masterplan to the wider area. The adjacent consented British Land scheme proposes to enhance the western and southern edges of Canada Water Dock and would deliver a boardwalk, southern steps and extensive landscaping. This application aims to sensitively develop the eastern dock edge with the proposed landscape concept to include three zones for planting; marginal, riparian and abstract nature. The proposed planting and trees would change as the ground level rises, to reflect a wetland habitat. The Design and Access Statement includes a letter of support for the proposed development from the London Wildlife Trust, who engaged in discussions and provided recommendations for the proposed scheme.
68. The proposed tree species would enhance the SINC habitat by providing increased species diversity, seasonal interest all year round and climate resilience. The lower level would comprise a floating habitat with native water tolerant shrubs and multistems. The upper level would comprise planters of





required. The irrigation strategy comprises a control system to be installed on the upper deck for trees and planting areas and for the raised planters at the lower deck. The self-sufficient high buoyancy floating island will accommodate maintenance use. The strategy includes an external bib tap and power points at the amphitheatre. The Urban Forrester recommended attaching landscape conditions to ensure that the high quality landscaping of the proposed development can be achieved.

71. Overall, the proposed landscaping would significantly improve the existing condition of the eastern dock edge, enhance the environmental quality of the site and provide both visual amenity and ecological benefits for Canada Water Dock.

### **Ecology and biodiversity**

72. Policy G6 of the London Plan 2021 states that Sites of Importance for Nature Conservation (SINCs) should be protected and development proposals should manage impacts on biodiversity and aim to secure biodiversity net gain, informed by the best available ecological information and be addressed from the start of the development process. The London Plan states that proposals which reduce deficiencies in access to nature should be considered positively.
73. P60 of the Southwark Plan 2022 corroborates Policy G6 and states that development must contribute to net gains in biodiversity through enhancing the nature conservation value of SINCs, protect and avoid damage to SINCs and include features such as soft landscaping, habitat restoration and expansion and improved green links.
74. Canada Water Dock is a designated SINC and comprises a slightly brackish water body which is connected to Surrey Water by the ornamental canal the 'Albion Channel' to the north of the dock. The proposed development would improve access to nature by introducing a lower level deck and would improve biodiversity and ecology of the site.
75. A Biodiversity Net Gain Report was submitted with the application which concludes that as a result of the proposed development, there would be a total net change in habitat units for the dock edge equating to 0.13 (using the Defra Biodiversity Metric Calculators 3.1), resulting in a Biodiversity Net Gain increase of 12.23%. This increase exceeds the required 10% net gain as established by the Environment Act 2021, and is therefore strongly supported.
76. The applicant also submitted an Environmental Impact Report which was conducted following the Chartered Institute of Ecology and Environmental Management Guidelines. The report covers topics including:
- Construction Phase
  - Operation Phase

- Legislation Planning, Police and Guidance
- Legislation and Conventions
- Policy
- Assessment Methodology
- Data Acquisition Methodology
- Assessment Criteria
- Aquatic Ecology Baseline
- Designated Sites
- Water Quality
- Sediment Quality
- Benthic Habitats and Species
- Macrophytes
- Fish
- Impact Assessment
- Construction
- Mitigation
- Mitigation to reduce the effects of changes in water quality on aquatic ecology
- Mitigation to reduce the effects of underwater noise and vibration from piling on fish
- Residual Environmental Effects
- Effects on changes in water quality on aquatic ecology
- Effects of underwater noise and vibration from piling on fish

77. Baseline data supported the findings of the report, including the results of project-specific surveys in Canada Water Dock. The following receptors were considered within the assessment for the proposed development:

- Dock wall habitats and species
- Benthic sediment habitats and species
- Fish
- Macrophytes

78. Mitigation measures embedded within the proposed development include:

- Use of soft start for piling activity;
- Use of vibro-piling as far as possible, noting that percussive piling may be required for the last 1-2 m for some piles;
- Adherence to best practice guidance to limit the introduction and spread of INNS;
- Adherence to guidance to reduce risk associated with accidental pollution events; and
- Application of measures in the Surface and Foul Water Drainage Strategy Report (Ramboll 2023).

79. The report states that with the above embedded mitigation taken into account, the impacts on aquatic ecology associated with changes in water quality and impacts on fish due to underwater noise and vibration are considered to be of moderate significance or greater.
80. Mitigation to reduce the effects of changes in water quality on aquatic ecology include deploying a silt curtain to limit the extent to which re-suspended sediments disperse into the dock during bed releveling and piling. The report states that during bed releveling, physicochemical parameters including dissolved oxygen levels will be measured regularly at predetermined locations. If there is a drop in dissolved oxygen levels below a predetermined trigger level, then bed releveling will cease until acceptable levels are recorded. Locations, trigger levels and frequency of measurements are to be agreed with relevant stakeholders. An Aquatic Ecology Clerk Of Works will visually monitor aquatic ecology during bed releveling in particular looking for any signs of fish mortality. If any fish mortality is observed then bed releveling will cease for a pre-agreed period of time (to be agreed with relevant stakeholders). The report takes the view that with these measures in place, it is considered that significance of impact would be minor adverse.
81. Mitigation to reduce the effects of underwater noise and vibration from piling on fish would also include the silt curtain (as set out above) to prevent dispersal of sediments during piling to attenuate the propagation of noise and vibration through the water column. Again, an Aquatic Ecology Clerk Of Works will visually monitor the dock during piling looking for any signs of fish mortality. If any fish mortality is observed then piling will cease for a pre-agreed period of time (to be agreed with relevant stakeholders). With these measures in place the report concludes that significance of impact would be minor adverse.
82. The below table, from the Environmental Impact Report, summarises the potential impacts associated with the activities taking place during the construction phases of the proposed development, significance of impact, proposed mitigation (where required) and residual effects.

Tables below: Summary of impacts and suggested measures for construction activities

Impact	Receptor	Magnitude	Value	Sensitivity	Significance of Effect	Mitigation Measures	Likely Residual effect
Loss of habitat	Dock wall and benthic habitats and species	Negligible	Low	Low	Negligible	Not applicable	Negligible
Physical disturbance and displacement (disturbance of bottom sediments)	Benthic habitats and species	Negligible	Low	Low	Negligible	Not applicable	Negligible
	Fish	Negligible	Negligible – Very high	Negligible	Negligible	Not applicable	Negligible
	Macrophytes	Negligible	Low	Medium	Negligible	Not applicable	Negligible
Physical disturbance and displacement (visual)	Fish	Negligible	Negligible – Very high	Negligible	Negligible	Not applicable	Negligible
Changes to water quality (release of chemicals from sediments)	Dock wall and benthic habitats and species	Low	Low	Medium	Negligible	Not applicable	Negligible
	Fish	Low	Negligible – Very high	Medium	Moderate	Mitigation measures are outlined in Section 7.	Minor
Impact	Receptor	Magnitude	Value	Sensitivity	Significance of Effect	Mitigation Measures	Likely Residual effect
	Macrophytes	Low	Low	Medium	Negligible	Not applicable	Negligible
Underwater noise and vibration	Fish	Low	Negligible – Very high	Medium	Moderate	Measures outlined in Section 7.	Minor
Spread of Non-native species	Dock wall habitats and species	Negligible	Low	Medium	Negligible	Not applicable	Negligible
Shading	Fish	Low	Negligible – Very high	Low	Negligible	Not applicable	Negligible
	Macrophytes	Low	Low	Low	Negligible	Not applicable	Negligible
Accidental Pollution Events	Dock wall habitats and species	Low	Low	Medium	Negligible	Not applicable	Negligible
	Fish	Low	Negligible – Very high	Medium	Negligible	Not applicable	Negligible
	Macrophytes	Low	Low	Medium	Negligible	Not applicable	Negligible

83. The Ecology Team stated that the submitted Environmental Impact Report is thorough and the two impacts of potential risk to aquatic ecology have been addressed through mitigation which adequately addresses and reduces the risk. The Ecology Team confirmed acceptance to the ecological clerk of works to monitor the impact of works and stated they are happy with the report's conclusions.
84. The Outline Construction Environmental and Management Plan comprises a section on ecology. To minimise disruption to bats, the CEMP states that there will be no night time working during construction and demolition and buildings will adhere to good practice guidance. The Ecology Team confirmed that the proposed lighting would meet best practice for bats.

## **Environmental impact assessment**

85. The applicant submitted a request for an EIA Screening Opinion under application reference 22/AP/3601 which confirmed that an Environmental Impact Assessment is not required for proposed works to the eastern dock edge of the Canada Water Basin.
86. The assessment undertaken for 22/AP/3601 concluded that no significant likely effects have been identified and the proposed development would not be likely to have significant effects upon the environment virtue of factors such as its nature, size or location. The report stated that the environmental matters to be considered can be adequately assessed through the submission of technical reports submitted with the planning application for the proposed works. The proposed development is therefore not considered to constitute EIA development.

## **Land contamination**

87. The EIA Screening Opinion for the Dock works (22/AP/3601) stated that there is a potential risk during construction and the potential for contamination, however construction effects can be managed by way of a Construction Management Plan. The report states that any contaminants found would have to be removed and the land remediated with a contamination assessment to be submitted at planning application stage.
88. A Geo-Environmental Desk Study was submitted with the application which evidences that whilst the site has a potentially contaminative history, it is unlikely that significant contamination would remain within the water due to natural dilution and dispersion processes over time. The report states that there is potential that contaminants may be present in dock sediments, but that would be common to the wider area and therefore not considered significant at a 'site level'.
89. The Geo-Environmental Desk Study concludes that there is a low risk of future site users being exposed to, or coming into contact with potential contaminants that may be present within the sediments and water of the dock given the proposed use of the site. The report states that on completion of the development, a full 'source pathway receptor' linkage will not be created. Risks to controlled waters or ecology are not likely to be significantly altered as a result of the proposed development, therefore risks to environmental receptors are also considered low.
90. The report states that as standard, development contractors should be aware of the potential for contaminants to be present in the sediment and water and prepare appropriate management plans with regards to construction workers health and safety and the potential for construction works to create contaminant

migration pathways. This information would be submitted via the detailed CEMP.

91. The Environmental Protection Team requested a contamination watching brief condition to be attached to this application. The condition would ensure that if during development, contamination not previously identified is found to be present at the site then no further development (unless otherwise agreed in writing with the Local Planning Authority) shall be carried out until the developer has submitted, and obtained written approval from the Local Planning Authority for, a remediation strategy detailing how unsuspected contamination be dealt with.
92. The Environment Agency stated that they have no objection to the proposed development subject to the inclusion of ground water and contaminated land conditions. The Environment Agency stated that the reported actions and analysis of risks and liabilities detailed in the submitted Geo-environmental Desk Study are agreed in principle as being in accordance with relevant guidance and good practice and recommended attaching an unexpected contamination condition and piling condition.








## **Design**

93. Policy D3 of the London Plan 2021 states that development should enhance local context by delivering spaces that positively respond to local distinctiveness through their layout, scale, appearance and shape and encourage and facilitate active travel with convenient and inclusive pedestrian and cycling routes and crossing points that are aligned with peoples' movement patterns and desire lines in the area. With regard to experience, the policy states that development should achieve outdoor environments that are comfortable and inviting for people to use and are of high quality and character. Policy P13 of the Southwark Plan corroborates Policy D3 and adds that street clutter should be avoided within the public realm and landscaping and green infrastructure should be provided, appropriate to the context including the provision and retention of street trees.
94. Policy D5 of the London Plan focuses on inclusive design and states that development should provide high quality people focused spaces that are designed to facilitate social interaction and inclusion and be convenient and welcoming with no disabling barriers, providing independent access without additional undue effort, separation or special treatment. Policy P14 of the Southwark Plan reiterates the need for accessible and inclusive design for all and advocates for a positive pedestrian experience.
95. Policy D8 of the London Plan encourages public realm that is well designed, safe, accessible, inclusive, attractive, well connected, related to the local and historic context and easy to understand, service and maintain. Landscape

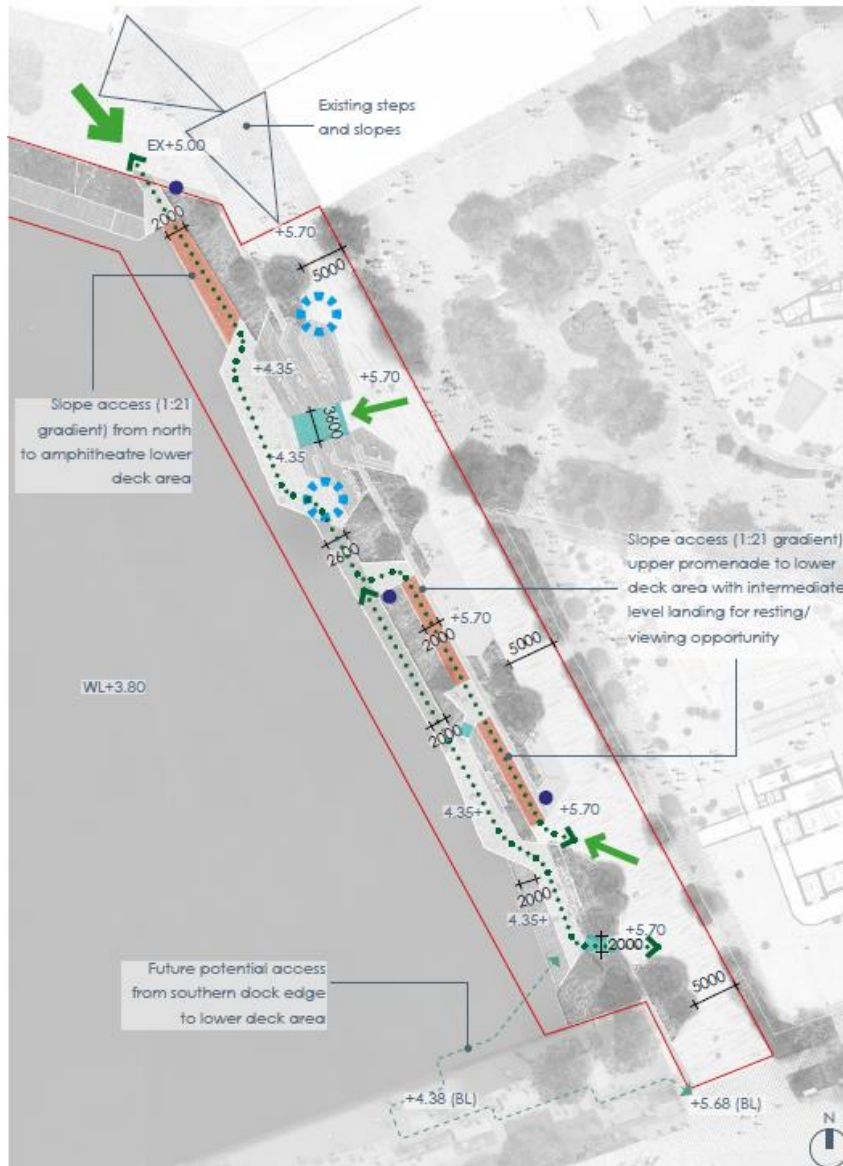
treatment, planting, street furniture and surface materials should be of good quality, fit for purpose, durable and sustainable and lighting should be carefully considered and well designed to minimise intrusive lighting infrastructure and reduce light pollution. Consideration should be given to creating a sense of place during different times of the day and night.

### Site layout, access and public realm

96. The existing primary access points to the eastern dock edge have been retained and the proposed upper and lower levels routes have been carefully considered to deliver inclusive access with clear width walkways and passing points, visible sight lines and shallow access gradients for wheelchair users. The plan below demonstrates the proposed circulation and access for the scheme, with associated measurements.

Key	
	Site boundary
	Site entrances
	Steps with nosing and corduroy tactile paving at top and bottom
	Slope
	Wheelchair space within tiered seating
	Continuous meandering route to connect the upper and lower promenade
	Potential access to southern dock edge
	Potential location for signage
+4.35	Proposed levels
EX+5.00	Existing levels
+4.38 (BL)	Proposed level by British Land's Southern Dock Edge





*Image above: Circulation and access diagram*

97. The furniture strategy comprises benches, bollards and litter bins; the location and details of which would be submitted through the recommended landscaping condition.

### Upper and lower deck areas

98. The upper deck maintains a level connection with the surrounding area and would act as the main thoroughfare through the scheme. The route is considered to be clear and well-defined, establishing legibility through the scheme and to the surrounding spaces. The landscaping is designed effectively to provide informal seating and play opportunities through raised planters as well as keeping the route free of clutter so as not to impede movement through the site. The upper deck would provide an attractive walking and cycling route

which would encourage and enable efficient movement throughout the site and wider Canada Water area.

99. The lower deck offers an alternative meandering route through the scheme, allowing for a different experience and a greater appreciation of the water. Landscaping has been carefully considered with planters cascading down towards the Dock and various points of seating are provided with good views over the Dock. The new dock edge structures and integrated planters would create vertical faces with the opportunity for signage and pattern. The detailed design and materiality should be secured via the recommended condition.
100. The levels are connected by a series of stairs and sloping sections. The sloped sections allow for a step-free route throughout the key points of the dock edge, including the amphitheatre, ensuring the scheme is accessible. Multiple resting points are also located along the step-free route, as well as throughout the scheme.
101. The proposed material palette is considered acceptable as the mix of harder paving and softer timber helps to demarcate between the different areas intended for movement or seating. It is noted that the materials are not the same as those proposed at the southern and western edge of the Dock, however the overall treatment is considered to be consistent. The use of timber is repeated throughout, bringing a consistency to the Dock as a whole and reflecting the local character and heritage. The different material palette will also help to establish a separate character for the eastern edge of the Dock and highlight the different public realm it offers.

### Amphitheatre

102. Amphitheatre type timber seating with landscaping is proposed to the north of the scheme, spanning both levels and providing an integral focal point for different activities and opportunities for socialising and relaxing. The central access stairs would comprise top, bottom and intermediate landings for rest and a unique natural stone paving pattern is proposed for the lower deck, which is considered to be acceptable.
103. The amphitheatre would also provide additional planting and spill out space for Waterfront Square, a key component of the public realm offered by the Canada Water Dockside Masterplan and intended to be a flexible area where public activities can be held. The space allows for wheelchair users to pull alongside the amphitheatre seating, thereby providing an inclusive environment.

### Young Anglers and wheelchair user area

104. The Young Anglers area would be separated from the public route and signage would demonstrate the use and operation of the space (for landing and netting activities only), which has been attached by condition and would also be subject to licence. The south eastern corner would have a low timber edge, to match

that of the southern edge of the Dock (consented planning permission under 21/AP/3794). The wheelchair user area would provide a flexible space for gathering and dwelling, with terraced timber seating and landscaping to enjoy views of the water.



*Image above: 1 Illustrative view of the Young Anglers and wheelchair user area*

105. The proposed development would enhance permeability and connectivity around the area, maintaining a clear route from Canada Water station and provide an attractive space for cyclists and pedestrians. The proposed site layout would also provide an opportunity to interact with the Dock in an accessible and inclusive manner.
106. Overall, the separation of spaces throughout the scheme is considered to be successful in realising the development's place making ambitions and the retention of the fishing pontoons, introduction of the young angler's area and various seating areas would encourage interactivity with the Dock. Subject to high quality execution and controlling detailed materials through conditions, the proposal is considered to deliver a high quality public realm and encourage a greater interaction with and enjoyment of the currently underutilised dock.

### Heritage considerations

107. Section 66 of the Planning (Listed Buildings and Conservation Areas) Act 1990 requires local planning authorities to consider the impacts of a development on a listed building or its setting and to pay special regard to the desirability of

preserving the building or its setting or any features of special architectural or historic interest which it possesses.

108. Chapter 16 of the NPPF contains national policy on the conservation of the historic environment. It explains that great weight should be given to the conservation of heritage assets. The more important the asset, the greater the weight should be (paragraph 199). Any harm to, or loss of significance of a designated heritage asset should require clear and convincing justification (paragraph 200). Paragraph 202 explains that where a development would give rise to less than substantial harm to a designated heritage asset, the harm should be weighed against the public benefits of the scheme. Paragraph 203 deals with non-designated heritage assets and explains that the effect of development on such assets should be taking into account, and a balanced judgment should be formed having regard to the scale of any harm or loss and the significance of the asset. Working through the relevant paragraphs of the NPPF will ensure that a decision-maker has complied with its statutory duty in relation to Conservation Areas and Listed Buildings.
109. Whilst Canada Water Dock is not located within a Conservation Area or within the curtilage of a Listed Building, the site has an important industrial, maritime and dockland heritage. P21 of the Southwark Plan 2022 and Policy HC1, Heritage conservation and growth of the London Plan 2021 emphasises the importance of considering heritage and historic character as they help provide a sense of place and enrich the townscape.
110. Canada Dock was constructed on the site of two former timber ponds which is considered to be sensitively reflected within the proposed development, with the repeated use of timber throughout the scheme, in reference to local heritage. The proposed development is not considered to cause harm to or have any negative impacts on local heritage assets and the proposed form, landscaping and design is considered to demonstrate an understanding of local character and history.

### Archaeology

111. P23 of the Southwark Plan 2022 focuses on archaeology and states that development must conserve the archaeology resources commensurate to its significance. Canada Dock is not located within an Archaeological Priority Zone.
112. The Council's Archaeologist was consulted in the application and stated that construction work within the Dock, due to the historic excavation of the Dock, will have no impact upon buried archaeological remains. Works outside the Dock will have a limited impact upon the deeply buried archaeological remains on site.

### Designing-out crime

113. Policy P16 of the Southwark Plan 2022 focuses on designing out crime and states that pedestrian routes, footpaths and cycle paths should be easy to navigate with permeable, direct routes that provide good visibility and avoid sharp or blind corners and have clear and uniform signage that helps people move around, making the public realm safer and more attractive for people to use. Development must be designed with effective street lighting that illuminates the public realm, enabling natural surveillance and avoiding the creation of dark, shadowed areas.
114. The Metropolitan Police stated that the proposal does not seem suitable to achieve SBD accreditation, however provided comments which were shared with the applicant, such as ensuring planted areas are well lit and not too dense and ensuring there are no areas of concealment, due to changes in site level that could be used by offenders. In addition, the Metropolitan Police stated that the bottom of tree canopies should be maintained to be no lower than 2 metres and ground planting should not be allowed to grow any higher than 1 metre, to ensure that good lines of sight across the public realm are maintained. The applicant stated that the proposed tree planting at the upper level would comprise a 2 metre canopy height to provide clear visibility across the public realm and the under planting would include herbaceous perennials and grasses with a degree of permeability, with dense tall shrub planting avoided.
115. In addition, the Metropolitan Police provided comments on lighting, public seating, management plans and CCTV. The applicant confirmed that the Landscape Management Plan will include a series of key maintenance principles to ensure that clear sightlines and a sense of direction for pedestrians, trees and planting areas are of high quality and that hardscape and site furnishings are kept tidy, clean and in good condition. An informative has been added to remind the applicant of the advice from the Metropolitan Police. It is appropriate to secure a detailed lighting strategy via the recommended condition as discussed earlier in this report.

### Site safety

116. P58 of the Southwark Plan 2022 states that development that affects designated open water spaces must ensure that all safety and navigational impacts are minimised. Policy D11 of the London Plan 2021 states that development proposals should minimise potential physical risks and consider measures at the start of the design process to ensure they are inclusive and aesthetically integrated into the development and wider area.
117. The Design and Access Statement comprises a Safety Strategy which maintains the majority of the existing 1.1 metre high balustrades along the eastern dock edge, with new metal balustrades with a timber top at the upper deck and metal wire balustrades at the lower deck, apart from the Young



Anglers and wheelchair user area where the timber edge is proposed for netting/landing for junior anglers. A secondary lightweight metal balustrade is proposed behind this area to ensure visitors safety. The applicant has confirmed that the details of the safety elements are to be developed during Stage 3, to ensure consistency with both the Canada Water Dockside Masterplan and British Land’s consented southern and western dock edge scheme.

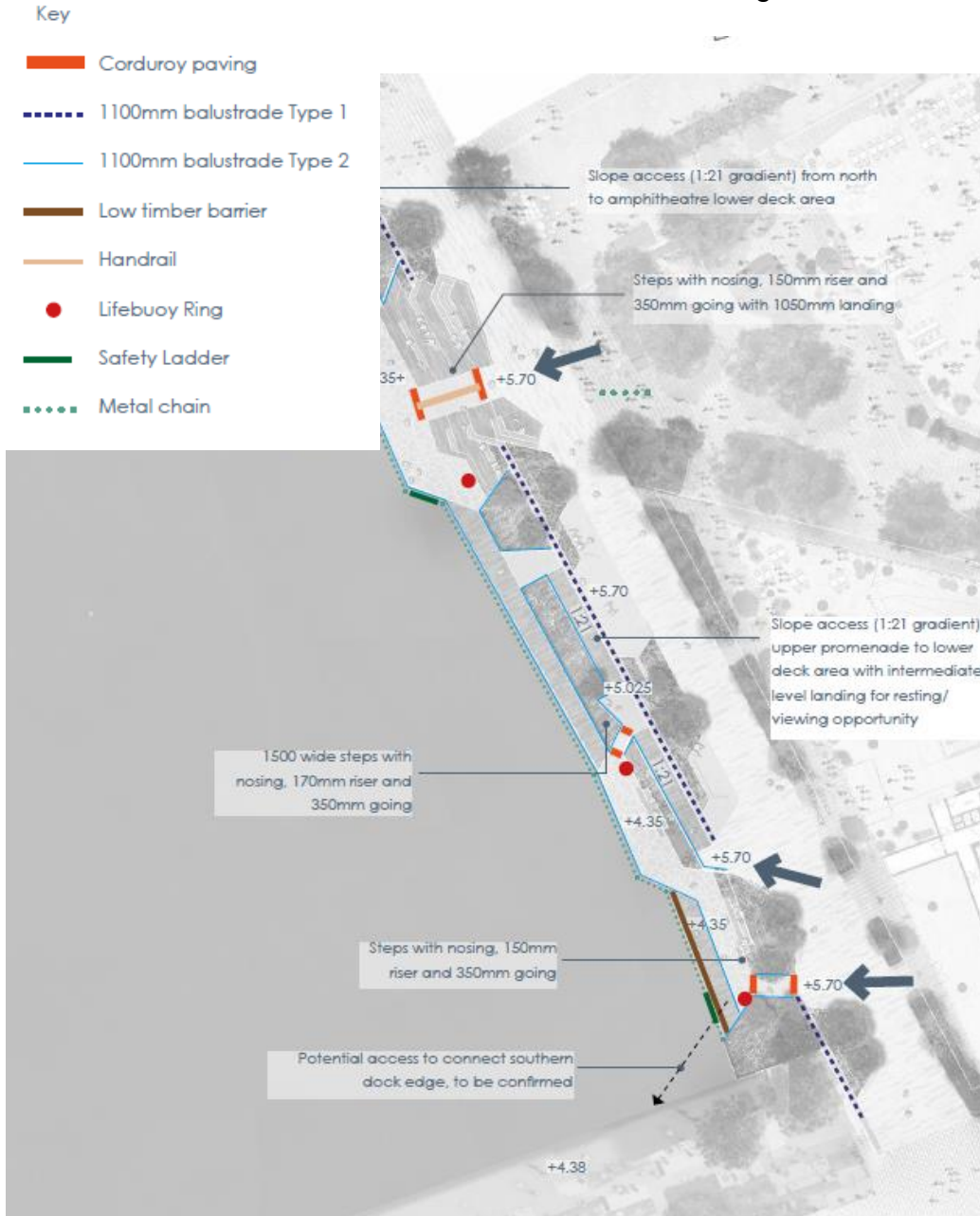


image above: Accessibility and circulation diagram

118. In addition, the applicant submitted a Water Safety Audit prepared by the Royal Society of the Prevention of Accidents (RoSPA). The Audit concluded that the proposed development has a water safety risk ratings of lower and medium risk levels.
119. The Audit recommends there are 'in case of emergency' instructions at the life ring points, egress ladders be painted to allow for high visibility during low light levels and signage for pedestrians to be aware for anglers casting and for anglers to give consideration for pedestrians. The Young Anglers area would be used for netting or landing opportunities only and would be separated from the continuous public route. The applicant has agreed for the signage condition to include the specific exclusion that the area is for netting and landing use only and the space will also be subject to a licence and management plan which would also include said exclusions
120. In addition, the Audit recommends that barriers be maintained and inspected regularly with repairs quickly carried out, provision of locked gate access to floating planting areas, and consideration to the possible issue of differing standards for access and railings. Risk assessments and method statements should be in place for maintenance worker safety and visitors should be communicated with in regards to water safety and water safety education should be encouraged in local primary schools. The Audit also recommends for annual monitoring and evaluation of the risk assessment and controls, particularly for the use of the basin during out of school times.
121. The applicant provided responses to each recommendation from the Water Safety Audit which are considered to be acceptable. The site wide safety strategy for Canada Dock is proposed to be incorporated with British Land's Masterplan, life rings will be provided and appropriate signage along the dock edge and fishing facilities will be constructed. The applicant stated that a Landscape Management and Maintenance Plan will be produced in conjunction with British Land to ensure edge treatments will be maintained to a high standard and the design details will continue to develop to provide a consistent design language and materiality for the dock edge with the British Land scheme during the detailed design stages. The Landscape Management Plan should be secured via the recommended condition






### **Wind microclimate**

122. P56 of the Southwark Plan 2022 considers impacts from wind as an amenity consideration to be taken into account to ensure a pleasant environment for present or future users. The applicant submitted a Pedestrian Level Wind Microclimate Assessment produced by RWDI to support the application. The assessment uses the 'Lawson Comfort Criteria' which seeks to define the reaction of an average person to the wind and if the measured wind condition exceeds the threshold wind speed for more than 5% of the time, then they are unacceptable for the stated pedestrian activity and the expectation is that there



may be complaints of nuisance or people will not use the area for its intended purpose. The four categories set out in the criteria include sitting, standing, strolling and walking, with a fifth category for conditions that are uncomfortable for all users. The report confirms that no occurrences of strong wind exceedances would occur on the eastern dock edge.

Table below: Lawson Comfort Criteria

Key	Comfort Category	Threshold	Description
	Sitting	0 – 14.4 km/h	Light breezes desired for outdoor restaurants and seating areas where one can read a paper or comfortably sit for long periods
	Standing	14.4 – 21.6 km/h	Gentle breezes acceptable for main building entrances, pick-up/drop-off points and bus stops
	Strolling	21.6 – 28.8 km/h	Moderate breezes that would be appropriate for strolling along a city/town street, plaza or park
	Walking	28.8 - 36 km/h	Relatively high speeds that can be tolerated if one's objective is to walk, run or cycle without lingering
	Uncomfortable	>36 km/h	Winds of this magnitude are considered a nuisance for most activities, and wind mitigation is typically recommended

123. Two iterations of the proposed development were initially assessed, one for the dock edge scheme in March 2023 referred to as the 'superseded scheme' and one for the 'proposed development' which subsequently underwent a small number of amendments from the superseded scheme prior to formal planning application submission. Three scenarios have been presented in the Pedestrian Level Wind Microclimate Assessment including:

- Configuration 1: Superseded Scheme with Existing Surrounding Buildings (including Canada Water Masterplan buildings A1 and A2) and Proposed Landscaping;
- Configuration 2: Superseded Scheme with Cumulative Surrounding Buildings (including Canada Water Masterplan buildings A1 and A2) and Proposed Landscaping; and
- Configuration 3: Proposed Development (May 2023) with the Existing Surrounding Buildings (including Canada Water Masterplan buildings A1 and A2) and Proposed Landscaping.

124. The applicant confirmed that as the eastern dock edge proposal would come forward alongside Building A1 of the Canada Water Dockside Masterplan, Buildings A1 and A2 are considered within the existing surrounding context. The meteorological data for the site indicate that the prevailing wind direction throughout the year is from the south-west, with secondary winds from the north-east, especially during the spring.

### Configuration 1

125. Configuration 1 demonstrates that wind conditions would be suitable for sitting and standing with one location suitable for strolling. No strong wind exceedances would occur at the site. To the north of Building A1 there would be two areas with standing use conditions in the summer season (at measurement locations 194 and 202). During the windiest season, wind conditions would generally be one category windier in comparison to the summer season with seating locations 194 and 202 having strolling conditions during the windiest season. The Technical Memorandum confirms that in terms of pedestrian safety, there would be no exceedance of the safety threshold, therefore throughout the year wind conditions would be safe for pedestrians along the dock edge in all three assessed configurations.



*Image above: Configuration 1 Wind Comfort Results*

### Configuration 2

126. Configuration 2 is considered to be the most important configuration as it's the most likely scenario that will exist in the future given that it includes the proposed CWD Buildings A1 and A2 as well as the British Land scheme which is highly likely to be built out. As in Configuration 1, there would be two isolated informal seating areas north of Building A1 (locations 194 and 202) that would have standing use conditions. No strong wind exceedances would occur on the Eastern Dock Edge.

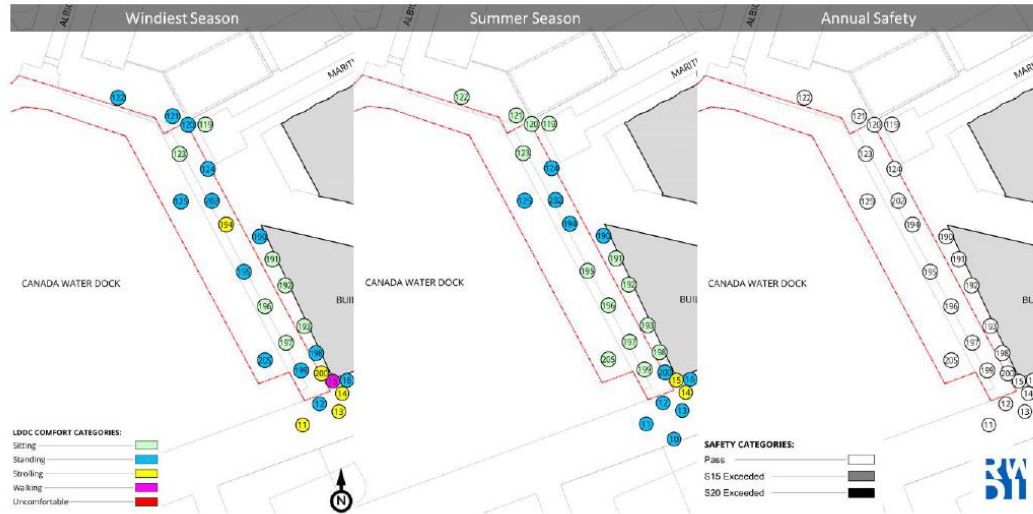


Image above: Configuration 2 Wind Comfort Results

### Configuration 3

127. Wind conditions would be similar to those in Configuration 1 with wind conditions at the south western corner of Building A2 Maritime Street changing slightly. The informal stepped seating areas with standing use conditions during the summer season situated north of Building A1 (locations 194 and 202). No strong wind exceedances would occur at the site.



Image above: Configuration 3 Wind Comfort Results

128. All configurations demonstrated that the seating locations 194 and 202 would only be suitable for standing use conditions in the summer season and consequently further design changes were undertaken to incorporate additional landscaping to improve comfort conditions. A further qualitative wind

assessment was submitted to demonstrate that all areas along the dock edge would be suitable for their intended use, specially seating areas during the summer season, as demonstrated in the image below.



*Image above: Configuration 4 (Proposed development with existing surrounding buildings and updated proposed landscaping) wind results*

129. It is considered necessary to secure full wind modelling as a pre commencement condition to demonstrate that the seating areas are all fit for purpose. As such a condition is recommended. Furthermore, the landscaping condition will ensure that the seating areas will be built in accordance with the plans submitted.
130. There is a further scenario that may arise whereby the Canada Water Dockside Masterplan would be constructed along with the dock edge works but the British Land Zone D development is not yet complete. This scenario would require additional temporary wind mitigation measures. The applicant has submitted a Landscape Plan for Temporary Wind Mitigation and prepared a strategy which proposes 3 planter and tree types (with specified product size, species, height girth and rootball size) and confirmed that all trees and shrubs would be evergreen. It is considered necessary for the applicant to undertake fully detailed modelling to demonstrate that the proposals are sufficient and as such a temporary wind mitigation pre commencement condition is recommended to ensure that satisfactory wind conditions can be achieved within the public realm.

## Sustainability

131. The presumption in favour of sustainable development championed by the NPPF is reinforced by P69 of the Southwark Plan 2022. P14 of the Southwark Plan 2022 states that development must provide a positive response to the context using durable, quality materials and spaces should be designed sustainably to adapt to the impacts of climate change. Policy SI 7 of the London

Plan 2021 promotes a more circular economy and encourages waste minimisation and waste prevention through the reuse of materials and using fewer resources in the production and distribution of products.

132. Given the nature and scale of the proposal it is not necessary or appropriate for the submission to address the full range of sustainability and energy considerations as set out in London and Southwark Plan Policies. There are no buildings proposed. However, it should be demonstrated that all reasonable measures have been taken to address climate change policies as far as possible. The existing structural concrete upper deck would be retained as part of the proposed development as well as the relocated fishing pontoons, however the reuse of the pontoon structure would be subject to RIBA Stages 3+. The applicant has confirmed the use of FSC hardwood and certified and sustainable source for all timber elements of the scheme and for the paving selection to match the wider Canada Water Dockside Masterplan where possible, to minimise future long term maintenance and replacement of different materials. In regard to biodiversity, the proposed planting across the scheme would be selected for their diversity, ecological benefits and biodiversity value.
133. The proposal is considered to provide a sustainable new piece of public realm, promote opportunities for walking and enhance soft landscaping adjacent to the SINC. As such the proposals addresses the principles of sustainability as far as possible.

### **Impact of proposed development on amenity of adjoining occupiers and surrounding area**

134. The importance of protecting neighbouring amenity is outlined in P56 of the Southwark Plan 2022 which states that development should not be permitted when it causes an unacceptable loss of amenity to present or future occupiers or users. Amenity considerations to be taken into account include the privacy and outlook of occupiers in both existing and proposed homes, actual or sense of overlooking or enclosure, impacts of smell, noise, vibration, lighting or other nuisances, daylight, sunlight and impacts from wind and on microclimate. Chapter 12 of the NPPF corroborates P56 and states that development should create places with a high standard of amenity for existing and future users.
135. The proposed development of the eastern dock edge of Canada Water Dock has been designed as part of the adjacent Canada Water Dockside Masterplan, to provide a public amenity space for local residents, workers and visitors to the area. The proposed development aims to enhance the accessibility, quality and biodiversity and ecological value of the eastern side of the Dock. The upgraded eastern dock edge is considered to bring a community benefit to the wider Canada Water Dockside Masterplan and significantly improve the existing condition in terms of design and landscaping and therefore would improve the amenity of adjoining occupiers and the surrounding area.

### Outlook, privacy, overlooking and enclosure

136. Given the proposed nature of the development and the location of this area of public realm in the context of both existing and proposed surrounding buildings, it is considered that the design and landscaping of the space would improve the outlook of surrounding occupiers and as no structures are proposed, there would be no negative sense of enclosure, privacy or overlooking impacts.

### Daylight, sunlight and impacts from wind and on the microclimate

137. The proposed development would not result in any negative daylight/sunlight impacts for surrounding occupiers as no structures are proposed. The applicant submitted a Wind Microclimate Assessment to support the application, and wind is assessed in its own section in this report.

### Noise, smell, vibration and lighting

138. P66 of the Southwark Plan 2022 states that development must mitigate any adverse impacts caused by noise on health and quality of life and avoid significant adverse impacts on health and quality of life. New spaces proposed as part of a major development should also assess the potential to enhance a place's character and identity through the acoustic environment and positive public soundscape. Policy D14 of the London Plan 2021 corroborates P66 and states that development proposals should manage noise by avoiding significant noise impacts on health and quality of life and improve and enhance the acoustic environment and promote appropriate soundscapes.
139. The proposed development of the eastern dock edge is an important piece of public realm for both the Canada Water Dockside Masterplan and the wider redevelopment of Canada Water. It is anticipated that there will be activity on the dock edge and amphitheatre during the day and evening and it is inevitable that there would be a degree of noise arising from the successful use of the space that would be typical of a town centre location. It is not unusual for public spaces such as this to be located in town centre locations alongside residential properties and those residents choosing to live in a town centre should anticipate a mix of uses taking place. Any excessive noise, disturbance or anti-social behaviour would be managed initially by the applicant as part of the management plan and if necessary the statutory powers that exist within the environmental health legislative framework if excessive noise should occur. It is not anticipated that the proposal would give rise to a level of noise or disturbance above that which might be experienced in the existing Deal Porter Square or proposed southern dock edge.
140. Short term, temporary noise and vibration effects may be generated during the construction of the proposed development. A Construction Environmental



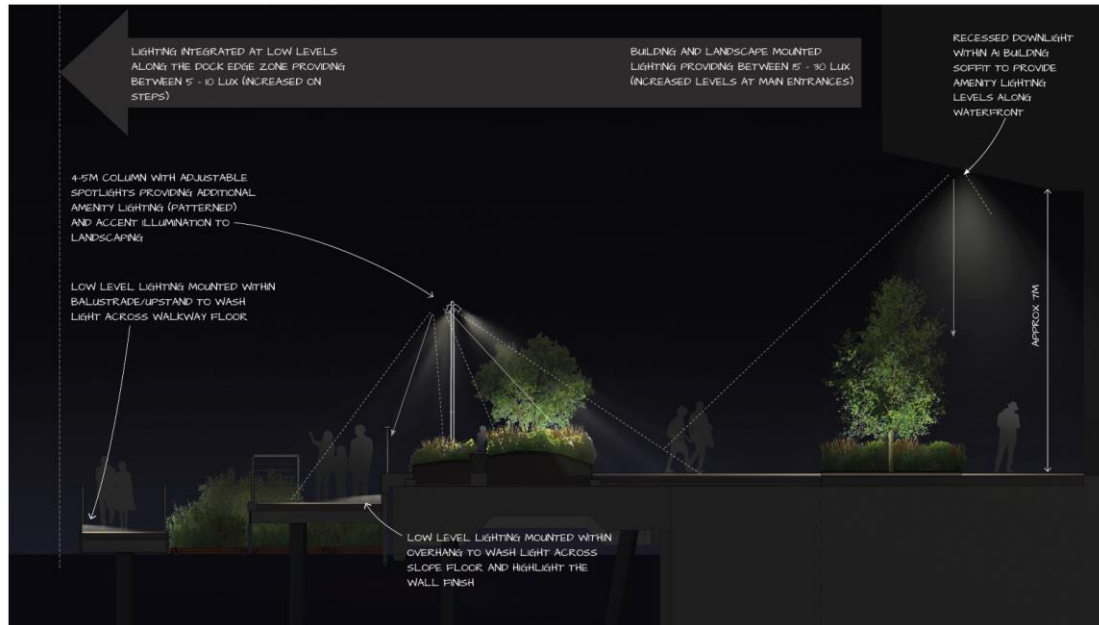
Management Plan pre approval condition is recommended to ensure that occupiers of neighbouring premises and the wider environment do not suffer a loss of amenity by reason of pollution and nuisance.

141. Lighting related impacts are assessed in its own section in the report and it is not anticipated that the proposed development would result in any negative smell impacts.

### **Light pollution**

142. Lighting of the public realm must appropriately address safety and security issues and make access routes welcoming and safe, whilst minimising light pollution. P16 of the Southwark Plan 2022 states that development must be designed with effective street lighting that illuminates the public realm, enables natural surveillance and avoids the creation of dark, shadowed areas; this is corroborated by Policy D8 of the London Plan 2021. The impact of lighting is also an amenity consideration, as cited by P56 of the Southwark Plan. Policy G6 of the London Plan states that development proposals adjacent to SINCs should consider the potential impact of indirect lighting effects to the site.
143. As Canada Water Dock is a sensitive receptor, details of lux levels and light spill were requested. The submitted Light Pollution Study details the proposed layered lighting approach; amenity lighting, accent lighting and feature lighting, and provides details on lighting equipment, lighting control and operation and maintenance. The lighting control system would allow for all feature and accent lighting to be switched off at an agreed curfew time and allow other amenity lighting around the dock edge to be dimmed as low as possible. Low level lighting will be utilised to illuminate ramps, steps and walkways for wayfinding whilst minimising light spill and pollution. Along the water edge, lighting levels are reduced for all lighting to be at low level and aimed downwards and away from the water to reduce risk of light spill into the Dock. The below image outlines the overall strategy for proposed lighting, with increased lighting levels on the upper deck and main entrances.





*Image above: Overall lighting strategy*

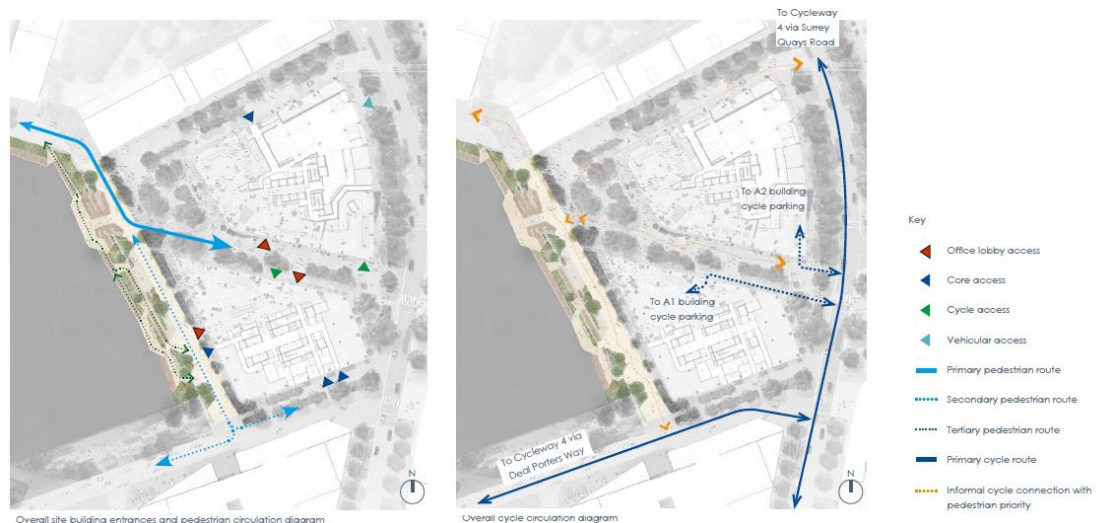
144. The Metropolitan Police stated that planted areas should be well lit and lighting should aim to achieve lighting standard BS 5489:2020. This lighting standard is particularly important in regard to preventing offences of violence against women and girls. Good levels of light are known to assist in preventing this type of offending as well as giving confidence to persons using the spaces in the hours of darkness. Due to the location of the proposed development, there is a recognised need to balance ecological considerations and site safety, which the Metropolitan Police recognised and stated that the BS standard should be the aim, with acceptable deviations to meet other requirements.
145. The Ecology Team confirmed that the lighting proposed meets best practice for bats and accepted the proposed lighting scheme. EPT recommended their external lighting standard condition to be added to ensure that the Council may be satisfied as the details of the development in the interest of visual amenity of the area, the amenity and privacy of adjoining occupiers and their protection from light nuisance.

## Transport and highways

146. P13 of the Southwark Plan 2022 states that development must ensure a high quality public realm that encourages walking and cycling and is safe, legible and attractive and eases the movement of pedestrians, cyclists, pushchairs, wheelchairs and mobility scooters; street clutter should be avoided. P51 states that development must enhance the borough's walking networks by providing footways, routes and public realm that enable access through development sites and adjoining areas. Routes and access must be safe and designed to be

inclusive, with particular emphasis on disabled people and the mobility impaired.

147. Policy D8 of the London Plan 2021 states that development must maximise the contribution that the public realm makes to active travel, ensure both the movement function of the public realm and its function as a place are provided for and priority modes of travel should be taken into account. Policy T2 Healthy Streets details qualities required to achieve improved walking networks so space is used more efficiently, is greener and more pleasant.
148. The proposed development of the eastern dock edge would integrate with the wider Canada Water Dockside Masterplan and provide a shared pedestrian and cycle route connecting the development with Canada Water Station, the neighbouring British Land Masterplan and the wider area. A tertiary route is proposed at the lower level next to the water, to promote opportunities to dwell and relax.



*Image above: Overall site building entrances and pedestrian circulation diagram [right] and overall cycle circulation diagram [left]*

149. It is considered that the proposed development would significantly improve the hard landscaping which currently exists and would encourage walking and cycling and help create a safe, legible and attractive piece of public realm.

## Highways

150. The dock edge is currently a signed cycle route forming part of the London Cycle Network (LCN) which would need to be maintained as part of the proposed development. The Council's Highways Team confirmed that they are satisfied with the proposed 5 metre width of the upper deck, as a shared pedestrian and cycle route, and the principle of the materials.

151. The Council's Highways Team stated that signage will be required on the upper promenade to warn both cyclists and pedestrians of the shared nature of the route and reminded the applicant that highway areas to have planting shall remain adopted. In addition, the Highways Team confirmed that the proposal for the developer to maintain both the hardscape and the soft landscape within the public realm and the adopted footway is acceptable subject to a maintenance agreement being reached. An informative has been attached to advise the applicant that the structural design of the proposed pillars/columns, middle and lower level decks require an approval in principle (AIP) in accordance with BD 2/12 'Technical Approval of Highway Structures'.
152. The applicant has been informed that they must enter into a s278 agreement to undertake works including paving the adopted highway using 750 mm x 600mm silver grey natural granite stone slabs including 300mm wide silver grey natural granite stone kerbs. Any material departures proposed have to be through the SSDM departure application process and could be subject to commuted sums if granted. In addition, gradients connecting areas of different levels must comply with the Equality Act and planting or rain gardens adjacent to the highway must be in accordance with SSDM DS 502. The applicant has been informed to upgrade street lighting to current Council standards. Surface water from private areas should not flow onto public highway in accordance with Section 163 of the Highways Act 1980. Detailed drawings must be submitted as part of the s278 application confirming this requirement. A condition to remind the applicant to enter into a s278 agreement is recommended and the applicant has been sent relevant contact details in advance.
153. An Outline Construction Environmental and Management Plan has been submitted to establish the principles for this application, however the applicant has been informed that a detailed Construction Management Plan should be approved by the Council prior to the implementation of the development, and a Pro Forma has been shared with the applicant notifying them of what to include for the Council's Network Management Team. As discussed elsewhere in this report a condition is recommended to secure a CEMP.

### Cycle parking and cycling facilities

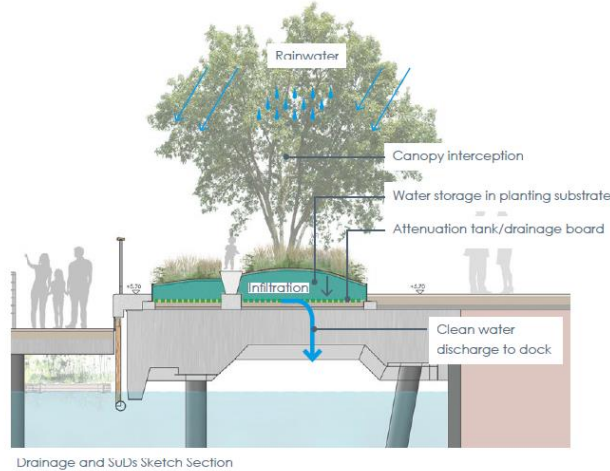
154. Whilst no cycle parking has been proposed as part of this application, the Canada Water Dockside Masterplan comprises extensive cycle parking, with cycle parking strategically located in areas of the masterplan served by access routes and short stay spaces located in close proximity to the dock edge, for visitor use. The signage and wayfinding condition would include signposting to nearby cycle parking facilities.

### Refuse storage arrangements

155. The Design and Access Statement comprises a Furniture Strategy which details indicative locations of two litter bins along the eastern dock edge. Further details will be submitted as part of the recommended detailed landscape condition.

## Water resources, flood risk and SUDs

156. P68 of the Southwark Plan 2022 states that development must not increase flood risk on or off site and should be designed to be safe and resilient to flooding. Policy SI 12 of the London Plan states that development proposals should ensure that flood risk is minimised and mitigated and that residual risk is addressed. Natural flood management methods should be employed in development proposals due to their multiple benefits including increasing flood storage and creating recreational areas and habitat. Policy SI 13 sets out sustainable drainage measures and details the drainage hierarchy, with the preference for green over grey features.
157. The site is in Flood Zone 2 and 3 and in an area benefiting from flood defences. The proposed development would comprise of level changes throughout the site with the existing concrete deck structure retained below the paving and the existing drainage outlet points. The Design and Access Statement includes the proposed drainage and SUDs drawings demonstrating that the upper deck would discharge to Canada Water Dock.



Key

- Site Boundary
- Planting area/reduced run-off
- Discharge to Canada Water Dock
- ← Outfall

*Image above: Proposed SUDs diagram*

158. The Flood Risk Management Team accept the proposed drainage strategy for the dock edge and stated that measures have been put in place which will help to manage and store surface water. The Flood Risk Management Team recommend a SUDs drainage condition to minimise the potential for the site to contribute to surface water flooding.
159. The target dock water level is 3.8 metres, which is consistent with the information presented in the adjacent British Land scheme 21/AP/3794. The Ecology Team have confirmed that the planting and tree species can tolerate periodic inundation with water.

### **Future dock management**

160. British Land is the long leaseholder of Canada Water Dock, with Southwark Council as the freehold owner. The maintenance and management of the proposed development is paramount for its success as an area of public realm adjacent to a SINC. As an important piece of public realm connected to the wider masterplan the future maintenance and management is secured in the s106 agreement attached to 21/AP/2655. This obligates the Applicant to maintain the dock edge works. In practice the Applicant has stated that British Land are likely to manage the eastern dock edge scheme following practical completion. It remains open for the Applicant to engage a third party (which could be British Land) to maintain the dock on their behalf but they will remain obligated to ensure that maintenance is and management is undertaken to a standard that is acceptable to the Council. Any third party agreement would sit outside of the planning application process.

### **Fire safety**

161. Policy D12 of the London Plan 2021 expects all development proposals to achieve the highest standards of fire safety and to this end requires applications to be supported by an independent Fire Strategy, produced by a third party suitably qualified assessor. Policy D12 (A) of the London Plan (2021) requires that all development must submit a planning fire safety strategy. The fire safety strategy should address criteria outlined in Policy D12 (A).
162. A Reasonable Exception Statement has been submitted to support the application. No buildings or enclosed spaces are proposed as part of the development. The area within the application boundary would be maintained as open, public realm with accessible routes and clear lines of sight. The proposed development would be situated adjacent to open water. No fire tender routes are required within the dock edge boundary.
163. Given the nature of the development, the Reasonable Exception Statement is accepted. The duty to identify fire risks and hazards in premises and to take

appropriate action lies solely with the developer. This is in no way a professional technical assessment of the fire risks presented by the development.

### **Planning obligations**

164. London Plan Policy DF1 and Southwark Plan Policy IP3 advise that planning obligations can be secured to overcome the negative impacts of a generally acceptable proposal. These policies are reinforced by the Section 106 Planning Obligations 2015 SPD, which sets out in detail the type of development that qualifies for planning obligations. The NPPF echoes the Community Infrastructure Levy Regulation 122 which requires obligations to be:

- necessary to make the development acceptable in planning terms;
- directly related to the development; and
- fairly and reasonably related in scale and kind to the development

165. This application is bound by the s106 obligations secured in the legal agreement attached to 21/AP/2655. For this particular application there is no requirement for additional mitigation beyond that secured at Outline stage.

### **Mayoral and Borough Community Infrastructure Levies**

166. Section 143 of the Localism Act states that any financial contribution received as community infrastructure levy (CIL) is a material "local financial consideration" in planning decisions. The requirement for payment of the Mayoral or Borough CIL is therefore a material consideration. However, the weight attached is determined by the decision maker. The Mayoral CIL is required to contribute towards strategic transport investments in London as a whole, while the Borough CIL will provide for infrastructure that supports growth in Southwark.

167. The site is located within Southwark CIL Zone 2, and MCIL2 Band 2 Zone. Based on the information in the design and access statement, the gross amount of CIL is approximately £0. It should be noted that this is an estimate, floor areas will be checked after planning approval has been secured.

### **Community involvement and engagement**

168. This application was accompanied by a Statement of Community Involvement and a Development Consultation Charter which comprises details of the stakeholder analysis undertaken, heritage and site layout, accessibility and movement, climate change and sustainability, local economy and community infrastructure. The document also includes a summary of engagement activities which have taken place, stakeholder views and vision for the site and a 'You Said, We Did' feedback summary for the proposed development. A Social Value Statement and monitoring data is also provided.

169. Public engagement for the proposed development was undertaken in parallel with the wider Canada Water Dockside masterplan consultation. Engagement activities for the dock edge included a public forum, public consultation events, drop in sessions and stakeholder meetings between October 2022 and February 2023. The Development Consultation Charter details the date of the engagement activities, number of attendees and the format for how feedback was received, including via feedback forms, online survey and verbally.
170. A total of 8 advertised engagement events took place, excluding one-to-one stakeholder meetings and door-to-door canvassing sessions. The applicant confirmed the following methods of engagement were undertaken:
- 10 local stakeholders were sent letters and e-newsletter updates in July, September and October 2022 and January 2023
  - 350-385 subscribers to the mailing list were updated via 6 e-newsletters sent in July and October 2022 and January 2023
  - 12,813 surrounding addresses received 3 printed flyers, distributed in July 2022, October 2022 and January 2023
  - A total of 218 people attended the drop in events and public forum
  - 58 responses were submitted in total (29 paper feedback forms and 29 responses to the online survey)
  - Engagement events were advertised via:
    - The dedicated consultation website
    - Twitter social media posts
    - 3 flyers sent to surrounding residents and businesses
    - 3 e-newsletters sent to the mailing list
171. The Development Consultation Charter provides a summary of the engaged stakeholder views and vision for the site including elements of the scheme that were liked by stakeholders including support for design elements and the public realm, added greenery and open space. Elements of the scheme that stakeholders disliked include the potential for increased crime and antisocial behaviour occurring throughout the public spaces in the site and maintenance and management of the dock edge were questioned. In terms of what stakeholders wanted to see as part of the development, the feedback included the importance of maintaining local biodiversity within the development as well as implementing sustainable measures in the dock edge. The delivery of public facing activities and amenities was also identified as a priority.
172. Alongside digital engagement methods, the application provided a phone line and postal address for the public to use. The Statement of Community Involvement states that upon request, a call back from the project team could be requested by local residents. The level of pre application consultation undertaken by the application is considered to be an acceptable effort to engage with those affected by the proposals.



173. As part of its statutory requirements, the Council sent letters to all residents and local businesses within 100 metre radius of the site, issued a press notice publicising the planning application, posted 4 site notices in the vicinity of the site and advertised the application on the website. Adequate efforts have therefore been made to ensure the community has been given the opportunity to participate in the planning process.
174. Full details of consultation undertaken by the Local Planning Authority in respect of this application are set out in Appendix 4 The responses received are summarised earlier in this report.

### **Consultation responses from external and statutory consultees**

175. Environment Agency:
- The Environment Agency have no objection to the proposed development subject to the inclusion of an unexpected contamination condition and piling condition. The Environment Agency also provided advice relating to flood risk.
176. London Borough of Lewisham:
- No comments received.
177. London Underground:
- London Underground/DLR Infrastructure Protection confirmed they have no comment to make on this planning application.
178. Natural England:
- No comments received.
179. Metropolitan Police Service (Designing out crime):
- The Metropolitan Police stated that the proposal does not seem suitable to achieve SBD accreditation, however provided observations and advice on planting areas and maintenance, changes in site level, lighting standards, public seating, management plan and CCTV. The comments were shared with the applicant who provided a detailed response which was considered to be acceptable by Officers.
180. Friends of Russia Dock Woodland:
- No comments received.
181. Southwark Park Association 1869:
- No comments received.
182. Surrey Docks Angling Club:
- No comments received.

## 183. Transport for London:

- TfL Spatial Planning stated that the proposal would not affect TfL assets and services, however stated that the dock edge is currently a signed cycle route forming part of the London Cycle Network (LCN) and clearly will need to be maintained as such. The Council should therefore be satisfied that the proposed surfacing (in terms of skid resistance, avoiding paving joints that could deflect wheels and maintenance regime) and residual widths (suitable for a shared pedestrian/cycle route), taking into account the permitted Canada Water Dockside development directly adjacent, are appropriate. Both Southwark Highways Team and Transport Policy Team confirmed they are satisfied with the 5m width and the principle of the materials.

## 184. Thames Water:

- Thames Water confirmed they have no comments to make at this time.

### **Consultation responses from internal consultees**

## 185. Arboricultural Services Team:

- No comments received.

## 186. Archaeology Team:

- The Archaeology Team stated that construction work within the Dock, due to the historic excavation of the Dock, will have no impact upon buried archaeological remains. Works outside the Dock will have a limited impact upon the deeply buried archaeological remains on site. Therefore no archaeological response is necessary for this application.

## 187. Community Infrastructure Team:

- The Community Infrastructure Team stated that the site is located within Southwark CIL Zone 2, and MCIL2 Band 2 Zone. Based on the information in the design and access statement, the gross amount of CIL is approximately £0. It should be noted that this is an estimate, floor areas will be checked after planning approval has been secured.

## 188. Ecology Team:

- The Ecology Team stated that the palette of proposed species is fine and the planting and tree species can tolerate periodic inundation with water. The Ecology Team confirmed acceptance of the BNG Report and stated that the BNG increase exceeds the minimum requirement. In regards to the Environmental Impact Report, the Ecology Team confirmed they are happy with the report's conclusions and accepted that an ecological clerk of works would be present to monitor the impacts while works are in progress. The Ecology Team recommend assessing whether more habitat features/bricks could be attached to the supports once in place.

The team confirmed that the proposed lighting meets best practice for bats.

189. Environmental Protection Team:

- The Environmental Protection confirmed they have no objections to the proposed development but recommend that a Construction Environmental Management Plan (pre-approval) condition, Contamination watching brief condition and standard External Lighting condition be attached to the permission.

190. Highways Development Management Team:

- The Highways Development Management Team confirmed that the proposal to have the upper promenade as a shared surface for pedestrians and cyclists with a minimum 5 metre width is acceptable, signing will be required to warn both types of users of the shared nature of the route. Highway areas to have planting shall remain adopted. The Highways Team confirmed that the proposal for the developer to maintain both the hardscape and soft landscape within the public realm and adopted footway is acceptable subject to a maintenance agreement being reached. The structural design of the proposed pillars/columns, middle and lower level decks require an approval in principle (AIP) in accordance with BD 2/12 'Technical Approval of Highway Structures'. This should be submitted and approved by the Highway Authority. In addition, the developer must enter into a s278 agreement to undertake a range of works, with details shared with the applicant and attached by condition. A Construction Management Plan should be approved by the Council prior to the implementation of the development.

191. Harbour Master:

- No comments received.

192. Parks and Cemeteries Team:

- No comments received.

193. S106 Team:

- No comments received.

194. Flood Risk Management and Urban Drainage Team:

- The Flood Risk Management and Urban Drainage Team accept the proposed drainage strategy for the dock edge and state that measures have been put in place which will help to manage/ store surface water. The FRM Team confirmed that they considered the application as a minor application, with the site areas of 23/AP/0562, 23/AP/0564, 23/AP/0565 being regarded as major applications where further drainage measure will be installed.

## 195. Transport Policy Team:

- The Transport Policy Team stated that the minimum width of 5 metres on the upper promenade is sufficient for pedestrians and cyclists. The Transport Policy Team recommend signage directing visitors to nearby cycle parking, this has been attached via condition.

## 196. Urban Forrester:

- The Urban Forrester stated that the inclusion of suitable species from the palette previously proposed is welcome, as is the incorporation of an innovative wicking membrane recommended to aid irrigation. This will ensure the proposed landscape design is provided to an acceptable quality, as specified during detailed pre-application discussions. The Urban Forrester recommended relevant landscaping conditions.

## 197. Waste Management Team:

- No comments received.

### **Community impact and equalities assessment**

198. The council must not act in a way which is incompatible with rights contained within the European Convention of Human Rights.

199. The council has given due regard to the above needs and rights where relevant or engaged throughout the course of determining this application.

200. The Public Sector Equality Duty (PSED) contained in Section 149 (1) of the Equality Act 2010 imposes a duty on public authorities to have, in the exercise of their functions, due regard to three "needs" which are central to the aims of the Act:

1. The need to eliminate discrimination, harassment, victimisation and any other conduct prohibited by the Act
2. The need to advance equality of opportunity between persons sharing a relevant protected characteristic and persons who do not share it. This involves having due regard to the need to:
  - Remove or minimise disadvantages suffered by persons who share a relevant protected characteristic that are connected to that characteristic
  - Take steps to meet the needs of persons who share a relevant protected characteristic that are different from the needs of persons who do not share it
  - Encourage persons who share a relevant protected characteristic to participate in public life or in any other activity in which participation by such persons is disproportionately low
3. The need to foster good relations between persons who share a relevant protected characteristic and those who do not share it. This involves

having due regard, in particular, to the need to tackle prejudice and promote understanding.

201. The protected characteristics are: race, age, gender reassignment, pregnancy and maternity, disability, sexual orientation, religion or belief, sex, marriage and civil partnership.
202. This application was accompanied by an Equalities Impact Assessment to assist the Council with considering the impact of the development in this respect. The study area considered for the assessment is focused on the local area (i.e. Rotherhithe Ward) and at a borough level (i.e. London Borough of Southwark) although data for the Greater London area has also been provided for wider context. This is considered to be appropriate to form a baseline for the assessment.
203. The document confirms that the applicant has conducted public consultation exercises to develop an understanding of the needs and aspirations of local residents and key stakeholders. This was discussed in detail earlier in the committee report and is considered to be proportionate and appropriate for the scale of the development.
204. The assessment identifies a range of potential impacts on each protected characteristic for key environmental disciplines during both construction and once in operation, including health, traffic and transport, air quality and noise and vibration. These aspects have been grouped into seven key considerations, informed by the baseline analysis and carried through to the assessment of potential equality related impacts for each protected characteristic including open space and green infrastructure, environmental amenity, crime and antisocial behaviour, community uses, accessibility, inclusivity and active travel, employment creation and health. These potential impacts have been discussed in the relevant sections of this report and any necessary mitigation to limit adverse impacts has been secured through planning conditions (for example crime and antisocial behaviour will be minimised through the use of an external lighting condition).
205. The application would deliver improved access and connectivity through the site and significant planting and green infrastructure which is considered beneficial to the wellbeing and experience of visitors to the area. The positive impacts arising from the development would benefit those groups with protected characteristics as well as the wider community.
206. The proposed development has been designed to ensure an inclusive environment for everyone. Street furniture, such as benches, would be located no more than 50 metres apart and would include arm and back rests. The proposed amphitheatre seating would ensure ease of use and incorporate spaces dedicated for wheelchairs to meet standards of accessibility and

inclusivity. In addition, the development would include both stepped and sloped routes and the sloped route down to the lower deck from the upper promenade would have a gradient no steeper than 1:21. Walkways would be of an accessible width and would include appropriate surfaces for movement, as well as resting places.

207. The eastern dock edge would be bound by 1.1 metre high balustrades, except the Young Anglers' and Wheelchair Users' area where a timber edge is proposed to ensure the visitors' safety. The Design and Access Statement contains a Water Safety Review authored by the Royal Society for the Prevention of Accidents with recommendations to mitigate risks as far as reasonably practical.
208. The proposed development is considered to result in a heightened sense of security as adequate lighting, wayfinding, signage and clear sight lines would enhance natural surveillance. The effects of construction impacts on environmental amenity created by the development would be mitigated by measures incorporated within the outline CEMP submitted with this application and a detailed CEMP is attached by condition.

### **Human rights implications**

209. This planning application engages certain human rights under the Human Rights Act 2008 (the HRA). The HRA prohibits unlawful interference by public bodies with conventions rights. The term 'engage' simply means that human rights may be affected or relevant.
210. This application has the legitimate aim of enhancing a protected SINC and providing a high quality area of public realm with the creation of a public pedestrian promenade at ground level and lower level boardwalk with associated amenity space, soft landscaping, tree planting and the relocation and refurbishment of the existing fishing pontoon and the creation of a new fishing pontoon. The creation and enhancement of public realm would encourage interaction between groups and individuals with different characteristics and would provide inclusive access routes, ensuring compliant access can be achieved. The rights potentially engaged by this application, including the right to a fair trial and the right to respect for private and family life are not considered to be unlawfully interfered with by this proposal.

### **Positive and proactive statement**

211. The Council has published its development plan and Core Strategy on its website together with advice about how applications are considered and the information that needs to be submitted to ensure timely consideration of an application. Applicants are advised that planning law requires applications to be

determined in accordance with the development plan unless material considerations indicate otherwise.

212. The Council provides a pre-application advice service that is available to all applicants in order to assist applicants in formulating proposals that are in accordance with the development plan and core strategy and submissions that are in accordance with the application requirements.

<b>Positive and proactive engagement: summary table</b>	
Was the pre-application service used for this application?	YES
If the pre-application service was used for this application, was the advice given followed?	YES
Was the application validated promptly?	YES
If necessary/appropriate, did the case officer seek amendments to the scheme to improve its prospects of achieving approval?	YES

## **CONCLUSION**

213. This proposed development conforms with the ambitions of the Canada Water Dockside Masterplan and forms an important part of the development, creating a high quality piece of public realm with significantly improved landscaping, access routes and biodiversity net gain. Careful consideration has been given to public safety, delivering environmental quality and the re-provision of fishing pontoons to benefit the local community, workers and visitors to the area.
214. Whilst the priority is for the proposed development to be delivered, there is a fall-back position whereby a payment in lieu will be made to the Council for public realm enhancements if the Developer is unable to obtain all necessary consents to implement the proposal.
215. The proposal would comply with the principles of sustainable development and would not harm residential amenity. It is therefore recommended that this application be recommended for approval, subject to the inclusion of relevant conditions.



## BACKGROUND INFORMATION

### BACKGROUND DOCUMENTS

Background Papers	Held At	Contact
Site history file Application file Southwark Local Development Framework and Development Plan Documents	Environment, Neighbourhoods & Growth Department 160 Tooley Street London SE1 2QH	Planning enquiries telephone: 020 7525 5403 Planning enquiries email: planning.enquiries@southwark.gov.uk Case officer telephone: 0207 525 0254 Council website: www.southwark.gov.uk

## APPENDICES

No.	Title
Appendix 1	Recommendation (draft decision notice)
Appendix 2	List of Planning Policy
Appendix 3	Consultation undertaken
Appendix 4	Consultation responses received

## AUDIT TRAIL

<b>Lead Officer</b>	Stephen Platts, Director of Planning and Growth	
<b>Report Author</b>	Rose Sharkey, Graduate Planner	
<b>Version</b>	Final	
<b>Dated</b>	24 August 2023	
<b>Key Decision</b>	No	
<b>CONSULTATION WITH OTHER OFFICERS / DIRECTORATES / CABINET MEMBER</b>		
<b>Officer Title</b>	<b>Comments Sought</b>	<b>Comments included</b>
Strategic Director of Finance and Governance	No	No
Strategic Director of Environment and Leisure	No	No
Strategic Director of Housing and Modernisation	No	No
<b>Date final report sent to Constitutional Team</b>		29 August 2023

## Appendix 1: Recommendation

This document shows the case officer's recommended decision for the application referred to below.

This document is not a decision notice for this application.

**Applicant** On behalf of AIRE UK Canada Water GP PropCos Ltd **Reg. Number** 23/AP/0798

**Application Type** Minor application

**Recommendation** GRANT permission

**Case Number** 468-C

### Draft of Decision Notice

**planning permission is GRANTED for the following development:**

Works to improve and enhance the eastern edge of Canada Dock, including the creation of a public pedestrian promenade at ground level and lower level boardwalk with associated amenity space, soft landscaping, tree planting and associated works together with the relocation and refurbishment of the existing fishing pontoon and the creation of a new fishing pontoon.

Eastern Edge Of Canada Dock Adjacent To Units 1 And 4 Canada Water Retail Park London Southwark SE16

**In accordance with application received on 21 March 2023 and Applicant's Drawing Nos.:**

Location Plan Site Location Plan CWD-ASD-DE-XX-DR-L-0010-XX P03 received 12/05/2023

Existing Plans

Proposed Plans

Plans - Proposed Landscape Cross Sections 1 of 2 CWD-ASD-DE-XX-DR-L-0120-XX P04 received 12/05/2023

Plans - Proposed Demolition Plan CWD-ASD-DE-XX-DR-L-0012-XX P02 received 12/05/2023

Plans - Proposed Landscape Cross Sections 2 of 2 CWD-ASD-DE-XX-DR-L-0121-XX P04`  
received 15/05/2023

Plans - Proposed Landscape Plan for Temporary Wind Mitigation CWD-ASD-DE-XX-DR-L-0701-  
XX P01 received 17/07/2023

Plans - Proposed Proposed Levels Plan CWD-ASD-DE-XX-DR-L-0101-XX P05 received  
23/08/2023

Plans - Proposed Landscape GA Plan CWD-ASD-DE-XX-DR-L-0100-XX P05 received 23/08/2023

Plans - Proposed Landscape Elevation CWD-ASD-DE-XX-DR-L-0110-XX P03 received  
23/08/2023

Plans - Proposed Dock Edge Temporary Wind Mitigation CWD-ASD-DE-XX-DR-L-0700-XX P01  
received 17/07/2023

Plans - Proposed Planting Plan CWD-ASD-DE-XX-DR-L-0102-XX P03 received 23/08/2023

Plans - Proposed Tree Planting Plan CWD-ASD-DE-XX-DR-L-0103-XX P03 received 23/08/2023

#### Other Documents

Document Biodiversity Metric 3.1 received 12/05/2023

Document Biodiversity Net Gain Report CWD-ASS-DE-XX-RP-Y-0001-XX P00 received  
12/05/2023

Fire Statement Reasonable Exception Statement received 22/05/2023

Construction Method Statement Outline Construction Environmental and Management Plan CWD-  
GTM-DE-XX-RP-Y-0001-XX P00 received 12/05/2023

Design and access statement Design and Access Statement CWD-ASD-DE-XX-RP-A-0001-XX  
P00 received 12/05/2023

Document Development Consultation Charter CWD-KAN-DE-XX-RP-Y-0001-XX P00 received  
12/05/2023

Document Statement of Community Involvement received 05/06/2023

Environmental Statement Canada Water Dock Environmental Impact Report V5 received  
10/07/2023

Document Equalities Impact Assessment CWD-TRI-DE-XX-RP-Y-0001-XX P00 received  
22/05/2023

Document Wind Report C W D -RW D -DE-X X-R P -Y-000 1-X X P01 received 12/05/2023

Document Dock Edge RIBA Stage 2 Cost Plan CWD-GTQ-DE-XX-RP-Q-0001-XX P00 received 12/05/2023

Document Drainage details C W D -RUK-DE-X X-R P -C-000 2-X X P01 received 12/05/2023

Document Geo Environmental Desk Study CWD-RUK-DE-XX-RP-C-0002-XX P00 received 12/05/2023

Document Technical Memorandum 2202096 received 11/08/2023

Document RESPONSE TO REPRESENTATIONS RECEIVED FROM MET POLICE ON THE EASTERN EDGE OF CANADA DOCK received 12/07/2023

Design and access statement Light Pollution Study received 10/07/2023

Document Technical Memorandum 2202096 received 23/08/2023

Design and access statement Sustainability Strategy received 12/07/2023

Design and access statement PLANNING ADDENDUM CWD-ASD-DE-XX-RP-L-0803-XX V2 received 23/08/2023

### **Time limit for implementing this permission and the approved**

2. The development hereby permitted shall be begun before the end of three years from the date of this permission.

Reason:

As required by Section 91 of the Town and Country Planning Act 1990 as amended.

### **Permission is subject to the following Pre-Commencements**

3.
  - a) Not to commence the development hereby approved until the developer has submitted details of the temporary wind mitigation measures shown on the plan hereby approved (CWD-ASD-DE-XX-DR-L-0701-XX-P01\_LANDSCAPE PLAN FOR TEMPORARY WIND MITIGATION) to the Council and the Council has approved the mitigation measures. The details shall include the location, design, material, dimension and height of all planters, the species of planting and size, and a wind mitigation assessment demonstrating the mitigation measures achieve the relevant comfort category.

- b) Not to commence above ground works of the development hereby approved until the developer has confirmed to the Council that the necessary consents relating to the delivery of the temporary wind mitigation approved under part a) have been obtained. If the necessary consents cannot be obtained, the developer shall submit details of alternative temporary wind mitigation to the Council for approval.
- c) The wind mitigation assessment submitted under Part a) or Part b) shall identify the full impacts of wind within the public realm to the East and Southern edges of The Dock and the public realm to the South and West of Canada Water Dockside Plot A1 (using the Lawson criteria) to match the scope of the approved Wind Report (CWD-RWD-DE-XX-RP-Y-0001-X X-P01) prepared by RWDI and to detail the circumstances in which the mitigation measures would no longer be required.
- d) Not to occupy the development hereby approved until the mitigation measures approved by the Council pursuant to Part a) or Part b) above have been provided and, unless otherwise agreed by the Council, not to remove or permit the removal of such mitigation measures until practical completion of the development in Development Zone D of the adjacent Canada Water Masterplan development site (allocated local planning authority (18/AP/1604).
- e) Not to occupy the development hereby approved until the developer has submitted an updated wind mitigation assessment in respect of the proposed amphitheatre seating as shown on the plans hereby approved.
- f) The wind mitigation assessment submitted under Part d) shall identify the full impacts of wind within the public realm in this location (using the Lawson criteria) and must demonstrate that the seating area will achieve the relevant comfort category.
- g) Not to occupy the development hereby approved until the mitigation measures approved by the Council pursuant to Part e) above have been installed and, unless otherwise agreed by the Council, not to remove or permit the removal of such mitigation measures for the duration that the development or any part of the development remains occupied.

Reason: To ensure that satisfactory wind conditions can be achieved within the public realm in accordance with the strategy proposed in the plans and documents submitted as part of this application and to accord with policies D8 Public realm of the London Plan (2021) and P13 Design of places and P56 Protection of amenity of the Southwark Plan (2022).

#### 4. Construction Environmental Management Plan (CEMP) - pre approval

No development shall take place, including any works of demolition, until a written CEMP has been submitted to and approved in writing by the Local Planning Authority. The CEMP shall oblige the applicant, developer and contractors to commit to current best practice with regard to construction site management and to use all best endeavours to minimise off-site impacts, and will include the following information:

- A detailed specification of demolition and construction works at each phase of development including consideration of all environmental and ecological impacts and the identified remedial measures;
- Confirmation of the appointed 'Ecological Clerk of Works' responsible for overseeing the construction together with full details of their relevant qualifications and responsibilities as well as an outline of the anticipated effects that could arise and any suggested mitigation to minimise harm to the ecology of the SINC
- Site perimeter continuous automated noise, dust and vibration monitoring;
- Engineering measures to eliminate or mitigate identified environmental impacts e.g. hoarding height and density, acoustic screening, sound insulation, dust control measures, emission reduction measures, location of specific activities on site, etc.;
- Arrangements for a direct and responsive site management contact for nearby occupiers during demolition and/or construction (signage on hoardings, newsletters, residents liaison meetings, etc.)
- A commitment to adopt and implement of the ICE Demolition Protocol and Considerate Contractor Scheme; Site traffic - Routing of in-bound and outbound site traffic, one-way site traffic arrangements on site, location of lay off areas, etc.;
- Site waste Management - Accurate waste stream identification, separation, storage, registered waste carriers for transportation and disposal at appropriate destinations.
- A commitment that all NRMM equipment (37 kW and 560 kW) shall be registered on the NRMM register and meets the standard as stipulated by the Mayor of London

To follow current best construction practice, including the following:-

- Southwark Council's Technical Guide for Demolition & Construction at <http://www.southwark.gov.uk/construction>
- Section 61 of Control of Pollution Act 1974,
- The London Mayors Supplementary Planning Guidance 'The Control of Dust and Emissions During Construction and Demolition',
- The Institute of Air Quality Management's 'Guidance on the Assessment of Dust from Demolition and Construction' and 'Guidance on Air Quality Monitoring in the Vicinity of Demolition and Construction Sites',
- BS 5228-1:2009+A1:2014 'Code of practice for noise and vibration control on construction and open sites. Noise',
- BS 5228-2:2009+A1:2014 'Code of practice for noise and vibration control on construction and open sites. Vibration'
- BS 7385-2:1993 Evaluation and measurement for vibration in buildings. Guide to damage levels from ground-borne vibration,
- BS 6472-1:2008 'Guide to evaluation of human exposure to vibration in buildings - vibration sources other than blasting,
- Relevant Stage emission standards to comply with Non-Road Mobile Machinery (Emission of Gaseous and Particulate Pollutants) Regulations 1999 as amended & NRMM London emission standards <http://nrmm.london/>

All demolition and construction work shall be undertaken in strict accordance with the approved CEMP and other relevant codes of practice, unless otherwise agreed in writing by the Local Planning Authority.

#### Reason

To ensure that occupiers of neighbouring premises and the wider environment do not suffer a loss of amenity by reason of pollution and nuisance, and to protect the ecological status of the SINC in accordance with the London Plan 2021 Policies G6 (Biodiversity and access to nature) and SI 17 (Protecting and enhancing London's waterways), Southwark Plan 2022 Policies P56 (Protection of amenity), P60 (Biodiversity) and P66 (Reducing noise pollution and enhancing soundscapes) and the National Planning Policy Framework 2021.

#### 5. Hard and soft landscaping



Before any Above Grade works hereby authorised begin, detailed drawings of a hard and soft landscaping scheme shall be submitted to the Local Planning Authority for approval in writing.

Details must include cross sections and plans, detailed planting specifications, signage and wayfinding (with details of warning both pedestrians and cyclists of the shared nature of the upper promenade route, signposting nearby cycle parking facilities and signage with specific exclusion for the Young Angler's area denoting that the area is for netting and landing activities only), surfacing materials, street furniture, access or pathways layouts, materials, details of sustainable drainage or other water features, details of the treatment of any edges/boundaries or other means of enclosure and any necessary wind mitigation features. Details shall be provided of the intended maintenance regime for all areas of landscaping.

The landscaping shall not be carried out otherwise than in accordance with any such approval given and shall be retained and maintained for the duration of the use.

The planting, seeding and/or turfing shall be carried out in the first planting season following completion of building works and any trees or shrubs that are found to be dead, dying, severely damaged or diseased within five years of the completion of the building works OR five years of the carrying out of the landscaping scheme (whichever is later), shall be replaced in the next planting season by specimens of the same size and species in the first suitable planting season. Planting shall comply to BS: 4428 Code of practice for general landscaping operations, BS: 5837 (2012) Trees in relation to demolition, design and construction and BS 7370-4:1993 Grounds maintenance Recommendations for maintenance of soft landscape (other than amenity turf).

Reason:

So that the Local Planning Authority may be satisfied with the details of the landscaping scheme in accordance with Southwark Plan 2022 Policy P56 (Protection of amenity), P13 (Design of places), P14 (Design quality), P50 (Highways impacts), P51 (Walking), P53 (Cycling), P58 (Open water space), P59 (Green infrastructure), P60 (Biodiversity), P61 (Trees) and the National Planning Policy Framework 2021.

6. Piling or any other foundation designs using penetrative methods shall not be permitted other than with the express written consent of the local planning authority, which may be given for those parts of the site where it has been demonstrated that there is no resultant unacceptable risk to groundwater. The development shall be carried out in accordance with the approved details.

Reason

This is to ensure piling works taking place in made ground or contaminated areas do not present a risk to groundwater within the underlying principal and secondary aquifers or to surface waters.

**Permission is subject to the following Pre-Occupation Condition(s)**

7. Prior to the first opening of the development to the public, a landscape management plan, including long term design objectives, management responsibilities and maintenance schedules for all landscape areas shall be submitted to and approved by the Local Planning Authority.

The landscape management plan shall be carried out as approved and any subsequent variations shall be agreed in writing by the local planning authority. If within a period of five years from the date of the planting of any tree that tree, or any tree planted in replacement for it, is removed, uprooted or destroyed or dies, or becomes, in the opinion of the local planning authority, seriously damaged or defective, another tree of the same species and size as that originally planted shall be planted at the same place in the first suitable planting season, unless the local planning authority gives its written consent to any variation.

Works shall comply to BS: 4428 Code of practice for general landscaping operations, BS: 8545 (2014) Trees: from nursery to independence in the landscape; BS3998: (2010) Tree work - recommendations; BS 7370-4:1993 Grounds maintenance Recommendations for maintenance of soft landscape (other than amenity turf); EAS 01:2021 (EN) -Tree Pruning Standard; EAS 03:2022 (EN) -Tree Planting Standard.

Reason:

This condition is necessary to ensure the protection of wildlife and supporting habitat and secure opportunities for the enhancement of the nature conservation value of the site. This is an

mandatory criteria of BREEAM (LE5) to monitor long term impact on biodiversity a requirement is to produce a Landscape and Habitat Management Plan.

8. Prior to the first opening of the development to the public, the developer shall enter into a section 278 agreement to cover the following works:

- Repave the adopted highway using 750mm x 600mm silver grey natural granite stone slabs including 300mm wide silver grey natural granite stone kerbs.
- Any material departures proposed have to be through the SSDM departure application process and could be subject to commuted sums if granted.
- Gradients connecting areas of different levels must comply with the Equality Act.
- Planting or rain gardens adjacent to the highway must be in accordance with SSDM DS 502
- Details of tree planting and species
- Upgrade street lighting to current LBS standards.

Any such measures shall be retained and maintained for the duration of the consented use unless otherwise agreed in writing with the Local Planning Authority.

Reason:

In order that the immediate pedestrian environment is of sufficient quality to accommodate the additional footfall that the development will generate in accordance with the National Planning Policy Framework 2021 and Southwark Plan (2022) Policies P50 Highways impacts, P51 Walking and P53 Cycling

### **Permission is subject to the following Compliance**

9. Contaminated land - further findings

If, during development, contamination not previously identified is found to be present at the site then no further development (unless otherwise agreed in writing with the Local Planning Authority [LPA]) shall be carried out until the developer has submitted, and obtained written approval from the Local Planning Authority for, a remediation strategy detailing how this unsuspected

contamination shall be dealt with. The remediation strategy shall be implemented as approved, verified and reported to the satisfaction of the Local Planning Authority.

Reason:

There is always the potential for unexpected contamination to be identified during development ground works. The Environment Agency and the Environmental Protection Team should be consulted should any contamination be identified, in accordance with the Southwark Plan 2022 Policy P56 (Protection of amenity); Policy P64 (Contaminated land and hazardous substances), and the National Planning Policy Framework 2021.

10. Any external lighting system installed at the development shall comply with Institute of Lighting Professionals Guidance Note 01/21 'Guidance notes for the reduction of obtrusive light'.

Reason

In order that the Council may be satisfied as to the details of the development in the interest of the visual amenity of the area, the amenity and privacy of adjoining occupiers, and their protection from light nuisance, in accordance with the Southwark Plan 2022 Policy P16 (Designing out crime); Policy P56 (Protection of amenity), and the National Planning Policy Framework 2021.

11. Prior to the occupation of any buildings on site, evidence (photographs and installation contracts) should be submitted to demonstrate that the development has been constructed in full accordance with the drainage strategy hereby approved (ref: A1 & A2 and Maritime Street Surface and Foul Water Drainage Strategy Report CWD-RUK-DE -XX-RP-C-0002-XX-P01, dated 3 May 2023), prepared by Ramboll.

The Applicant is reminded that in order to discharge into the Dock, consent will be required from all relevant landowners. The Applicant must ensure that any water proposed to be discharged into the Dock meets CIRIA SuDS Standards.

Reason: To minimise the potential for the site to contribute to surface water flooding in accordance with Southwark's Strategic Flood Risk Assessment (2017) and Policy SI 13 of the London Plan (2021) and Policy P68 of the Southwark Plan (2022) and to ensure that water discharged into the dock would not adversely affect ecology.

## Informatives

1 The applicant is advised that planted areas should be well lit and not planted too densely as weapons, stolen property and drugs are often stashed in dense planting by offenders. The bottom of tree canopies should be maintained to be no lower than 2m and ground planting should not be allowed to grow any higher than 1m to ensure good lines of sight across the public realm are maintained. The changes in level on this site may make lines of sight difficult to maintain so care should be taken to ensure there are not areas of concealment that could be utilised by offenders.

The Metropolitan Police advises that lighting should aim to achieve lighting standard BS 5489:2020. This lighting standard is particularly important in regard to preventing offences of violence against women and girls. Good levels of light are known to assist in preventing this type of offending as well as giving confidence to persons using the spaces in the hours of darkness. It is understood that there are likely to be ecological considerations when planning this lighting scheme, the above standard should be the aim, with acceptable deviations to meet other requirements.

Consideration should be given to the types and position of public seating. This is essential to encourage legitimate use of the space but can also be used for rough sleeping and street drinking ' the use of handrails or slightly sloping surfaces can assist in preventing these incidents. Public seating should not be provided under areas of cover.

The applicant is advised that the management plan should detail how ASB in the form of street drinking, rough sleeping, skateboarding etc. will be dealt with. This, ideally, should be by the management team on site so that there is not an increase in calls to service to the police for this type of incident.

The applicant is advised that CCTV is an effective tool in reducing or preventing crime if the cameras are monitored and staff are able to respond to incidents or report these immediately to the appropriate authority. If the cameras are not monitored they can be useful in identification of offenders. The Metropolitan Police recommend some coverage of the public realm space that, as a minimum, provides a monitoring level of coverage.

2 The applicant is advised that the dock edge is currently a signed cycle route forming part of the London Cycle Network (LCN) and clearly will need to be maintained as such.

3 The applicant is advised that it is illegal to disturb nesting birds. Even with the provision of supervision, there will be a residual risk of killing and injuring breeding birds.

The applicant must therefore avoid clearance works during the bird nesting season. If clearance works are carried out between the months of September to February there will be no impact to

breeding birds in relation to death and/or injury. The Ecological Clerk of Works (ECoW) should be secured as part of the detailed Construction Environmental Management Plan.

4 The applicant is advised that in order to limit the potential adverse impact to swans during clearance and construction works the CEMP submitted must include a water fowl management plan to be followed for the duration of the works, which shall include:

1. The harvesting and storage of areas of reedbed and marginal vegetation and provision of fixed or floating platforms, at least one of which is specifically designed to provide a nest site for swans, to provide retained foraging and potential nesting opportunities whilst works are undertaken.
2. An arrangement with a suitable wildlife rescue organisation, such as the Swan Sanctuary, to respond to any problems or issues that might arise.

5 The applicant is advised that highway areas to have planting shall remain adopted. The proposal for the developer to maintain both the hardscape and soft landscape within the public realm and the adopted footway is acceptable subject to a maintenance agreement being reached. The structural design of the proposed pillars/columns, middle and lower level decks require an approval in principle (AIP) in accordance with BD 2/12 'Technical Approval of Highway Structures'. This should be submitted and approved by the Highway Authority.

The applicant is advised that the application site falls within 'Town Centre' designation and the adopted footways should be paved with silver grey natural granite stone slabs and 300mm wide silver grey granite natural stone kerbs. Section 106 and 278 agreements are to secure this.

The applicant is advised that surface water from private areas should not flow onto public highway in accordance with Section 163 of the Highways Act 1980. Detailed drawings must be submitted as part of the s278 application confirming this requirement.

The applicant is advised to note the following:

The Highway Authority requires works to all existing and any proposed new streets and spaces (given for adoption or not) to be designed and constructed to adoptable standards.

Southwark Council's published adoptable standards as Highway Authority are contained in the Southwark Streetscape Design Manual (SSDM), <https://www.southwark.gov.uk/transport-and-roads/traffic-orders-licensing-strategies-and-regulation/southwark-streetscape-design-manual-ssdm>

Applicants will be required to enter into an s278 agreement under the Highways Act 1980 for any works to existing adopted Highways.



## Appendix 2: Relevant Policies

### National Planning Policy Framework

The revised National Planning Policy Framework ('NPPF') was published on 20 July 2021 which sets out the national planning policy and how this needs to be applied. The NPPF focuses on sustainable development with three key objectives: economic, social and environmental. Paragraph 218 states that the policies in the Framework are material considerations, which should be taken into account in dealing with applications.

The following chapters are relevant:

Chapter 2 Achieving sustainable development

Chapter 6 Building a strong, competitive economy

Chapter 7 Ensuring the vitality of town centres

Chapter 8 Promoting healthy and safe communities

Chapter 9 Promoting sustainable transport

Chapter 11 Making effective use of land

Chapter 12 Achieving well-designed places

Chapter 14 Meeting the challenge of climate change, flooding and coastal change

Chapter 16 Conserving and enhancing the historic environment

### New London Plan 2021 Policies

On 2 March 2021, the Mayor of London published the London Plan 2021. The spatial development strategy sets a strategic framework for planning in Greater London and forms part of the statutory Development Plan for Greater London.

The relevant policies are:

GG1 Building strong and inclusive communities

GG2 Making the best use of land

GG3 Creating a healthy city

Policy SD1 Opportunity Areas

Policy SD6 Town centres and high streets

Policy SD7 Town centres: development principles and Development Plan Documents

Policy SD8 Town centre network

Policy SD10 Strategic and local regeneration

Policy D1 London's form, character and capacity for growth

Policy D3 Optimising site capacity through the design-led approach

Policy D4 Delivering good design

Policy D5 Inclusive design

Policy D8 Public realm

Policy D12 Fire safety

Policy D14 Noise

Policy S4 Play and informal recreation

Policy HC1 Heritage conservation and growth

Policy G1 Green infrastructure

Policy G4 Open space

Policy G5 Urban greening  
 Policy G6 Biodiversity and access to nature  
 Policy G7 Trees and woodlands  
 Policy SI 1 Improving air quality  
 Policy SI 7 Reducing waste and supporting the circular economy  
 Policy SI 8 Waste capacity and net waste self-sufficiency  
 Policy SI 12 Flood risk management  
 Policy SI 13 Sustainable drainage  
 Policy SI 16 Waterways – use and enjoyment  
 Policy SI 17 Protecting and enhancing London’s waterways  
 Policy T1 Strategic approach to transport  
 Policy T2 Healthy Streets  
 Policy T5 Cycling  
 Policy T7 Deliveries, servicing and construction

### Southwark Plan 2022

ST1 Southwark’s Development targets  
 ST2 Southwark’s Places  
 SP2 Southwark Together  
 SP3 Great start in life  
 SP5 Thriving neighbourhoods and tackling health equalities  
 SP6 Climate Emergency  
 AV.15 Rotherhithe Area Vision  
 P13 Design of places  
 P14 Design quality  
 P16 Designing out crime  
 P18 Efficient use of land  
 P21 Conservation of the historic environment and natural heritage  
 P23 Archaeology  
 P35 Town and local centres  
 P45 Healthy developments  
 P47 Community uses  
 P49 Public transport  
 P50 Highways impacts  
 P51 Walking  
 P53 Cycling  
 P56 Protection of amenity  
 P58 Open water space  
 P59 Green infrastructure  
 P60 Biodiversity  
 P61 Trees  
 P62 Reducing waste  
 P65 Improving air quality  
 P66 Reducing noise pollution and enhancing soundscapes  
 P68 Reducing flood risk  
 P69 Sustainability standards

NSP80 Decathlon Site and Mulberry Business Park  
Sustainable Transport (2009)

### **Appendix 3: Consultation undertaken**

**Site notice date:** 15/05/2023

**Press notice date:** 18/05/2023

**Case officer site visit date:** n/a

**Neighbour consultation letters sent:** 15/05/2023

#### **Internal services consulted**

Ecology

Archaeology

Community Infrastructure Levy Team

Design and Conservation Team [Formal]

Ecology

Environmental Protection

Highways Development and Management

Section 106 Team

Flood Risk Management & Urban Drainage

Transport Policy

Urban Forester

Waste Management

#### **Statutory and non-statutory organisations**

Environment Agency

London Underground

Natural England - London & South East Re  
Metropolitan Police Service (Designing O  
Transport for London  
Thames Water

**Neighbour and local groups consulted**

85 Montreal House Surrey Quays Road London  
70 Montreal House Surrey Quays Road London  
69 Montreal House Surrey Quays Road London  
67 Montreal House Surrey Quays Road London  
53 Montreal House Surrey Quays Road London  
50 Montreal House Surrey Quays Road London  
42 Montreal House Surrey Quays Road London  
40 Montreal House Surrey Quays Road London  
1 Montreal House Surrey Quays Road London  
9 Maritime Street London Southwark  
109 Montreal House Surrey Quays Road London  
72 Montreal House Surrey Quays Road London  
Flat 26 29 Surrey Quays Road London  
Flat 56 11 Maritime Street London  
167 Montreal House Surrey Quays Road London  
Flat 35 7 Maritime Street London  
Flat 23 5 Maritime Street London  
Flat 22 29 Surrey Quays Road London  
38 Montreal House Surrey Quays Road London

Flat 41 29 Surrey Quays Road London

Flat 52 7 Maritime Street London

Flat 64 29 Surrey Quays Road London

Flat 54 11 Maritime Street London

46 Montreal House Surrey Quays Road London

27 Montreal House Surrey Quays Road London

Unit B Montreal House Surrey Quays Road

153 Montreal House Surrey Quays Road London

145 Montreal House Surrey Quays Road London

137 Montreal House Surrey Quays Road London

111 Montreal House Surrey Quays Road London

76 Montreal House Surrey Quays Road London

65 Montreal House Surrey Quays Road London

Flat 71 29 Surrey Quays Road London

Flat 63 29 Surrey Quays Road London

Flat 21 29 Surrey Quays Road London

Flat 7 29 Surrey Quays Road London

Flat 68 11 Maritime Street London

Flat 64 11 Maritime Street London

Flat 61 11 Maritime Street London

Flat 55 11 Maritime Street London

Flat 47 11 Maritime Street London

Flat 24 11 Maritime Street London

Flat 17 7 Maritime Street London

Flat 11 7 Maritime Street London

Flat 8 7 Maritime Street London  
Flat 5 7 Maritime Street London  
Flat 2 7 Maritime Street London  
Flat 18 5 Maritime Street London  
Flat 17 5 Maritime Street London  
Flat 9 5 Maritime Street London  
25 Maritime Street London Southwark  
Unit C Montreal House Surrey Quays Road  
169 Montreal House Surrey Quays Road London  
160 Montreal House Surrey Quays Road London  
140 Montreal House Surrey Quays Road London  
135 Montreal House Surrey Quays Road London  
118 Montreal House Surrey Quays Road London  
91 Montreal House Surrey Quays Road London  
56 Montreal House Surrey Quays Road London  
37 Montreal House Surrey Quays Road London  
34 Montreal House Surrey Quays Road London  
14 Montreal House Surrey Quays Road London  
Construction Site Office Deal Porters Way London  
Top Hill Gelato Deal Porter Square London  
15 Maritime Street London Southwark  
Flat 42 7 Maritime Street London  
Flat 41 7 Maritime Street London  
101 Montreal House Surrey Quays Road London  
81 Montreal House Surrey Quays Road London



Flat 5 5 Maritime Street London

Flat 58 11 Maritime Street London

Flat 14 11 Maritime Street London

Flat 36 29 Surrey Quays Road London

Flat 31 7 Maritime Street London

Flat 7 7 Maritime Street London

Flat 22 5 Maritime Street London

Flat 4 11 Maritime Street London

Flat 19 29 Surrey Quays Road London

Flat 14 29 Surrey Quays Road London

Flat 8 29 Surrey Quays Road London

Flat 2 29 Surrey Quays Road London

Flat 69 11 Maritime Street London

30 Montreal House Surrey Quays Road London

158 Montreal House Surrey Quays Road London

139 Montreal House Surrey Quays Road London

119 Montreal House Surrey Quays Road London

15 Montreal House Surrey Quays Road London

Unit 3 Ticket Hall Canada Water Underground Station Surrey Quays Road

Flat 51 29 Surrey Quays Road London

Flat 43 29 Surrey Quays Road London

Flat 34 29 Surrey Quays Road London

Flat 13 29 Surrey Quays Road London

Flat 4 29 Surrey Quays Road London

Flat 1 29 Surrey Quays Road London

Flat 67 11 Maritime Street London

Flat 32 11 Maritime Street London

Flat 2 11 Maritime Street London

Flat 30 7 Maritime Street London

Flat 27 7 Maritime Street London

Flat 6 7 Maritime Street London

Flat 26 5 Maritime Street London

Flat 6 5 Maritime Street London

Stall 1 Deal Porter Square London

164 Montreal House Surrey Quays Road London

114 Montreal House Surrey Quays Road London

112 Montreal House Surrey Quays Road London

93 Montreal House Surrey Quays Road London

78 Montreal House Surrey Quays Road London

77 Montreal House Surrey Quays Road London

66 Montreal House Surrey Quays Road London

51 Montreal House Surrey Quays Road London

45 Montreal House Surrey Quays Road London

24 Montreal House Surrey Quays Road London

23 Montreal House Surrey Quays Road London

9 Montreal House Surrey Quays Road London

Unit 2 Ticket Hall Canada Water Underground Station Surrey Quays Road

Flat 30 11 Maritime Street London

127 Montreal House Surrey Quays Road London

39 Montreal House Surrey Quays Road London

Flat 16 7 Maritime Street London  
79 Montreal House Surrey Quays Road London  
89 Montreal House Surrey Quays Road London  
Flat 57 11 Maritime Street London  
Flat 59 29 Surrey Quays Road London  
Flat 30 29 Surrey Quays Road London  
Flat 13 7 Maritime Street London  
13 Maritime Street London Southwark  
Flat 13 11 Maritime Street London  
Flat 55 7 Maritime Street London  
Flat 67 29 Surrey Quays Road London  
Flat 63 11 Maritime Street London  
Flat 37 11 Maritime Street London  
49 Montreal House Surrey Quays Road London  
166 Montreal House Surrey Quays Road London  
129 Montreal House Surrey Quays Road London  
5 Montreal House Surrey Quays Road London  
73 Montreal House Surrey Quays Road London  
63 Montreal House Surrey Quays Road London  
Flat 52 29 Surrey Quays Road London  
Flat 45 29 Surrey Quays Road London  
Flat 42 29 Surrey Quays Road London  
Flat 65 29 Surrey Quays Road London  
Flat 58 29 Surrey Quays Road London  
21 Maritime Street London Southwark

Flat 38 29 Surrey Quays Road London

Flat 31 29 Surrey Quays Road London

Flat 25 29 Surrey Quays Road London

Flat 16 29 Surrey Quays Road London

Flat 48 11 Maritime Street London

Flat 42 11 Maritime Street London

Flat 41 11 Maritime Street London

Flat 21 11 Maritime Street London

Flat 11 11 Maritime Street London

Flat 9 11 Maritime Street London

Flat 56 7 Maritime Street London

Flat 47 7 Maritime Street London

Flat 26 7 Maritime Street London

Flat 24 7 Maritime Street London

Flat 27 5 Maritime Street London

Flat 14 5 Maritime Street London

Flat 11 5 Maritime Street London

Flat 2 5 Maritime Street London

1 Albatross Way London Southwark

149 Montreal House Surrey Quays Road London

141 Montreal House Surrey Quays Road London

86 Montreal House Surrey Quays Road London

83 Montreal House Surrey Quays Road London

80 Montreal House Surrey Quays Road London

36 Montreal House Surrey Quays Road London

32 Montreal House Surrey Quays Road London  
21 Montreal House Surrey Quays Road London  
20 Montreal House Surrey Quays Road London  
4 Montreal House Surrey Quays Road London  
Stompin Jago Canada Water Library 21 Surrey Quays Road  
Flat 12 7 Maritime Street London  
Flat 24 5 Maritime Street London  
138 Montreal House Surrey Quays Road London  
Flat 37 29 Surrey Quays Road London  
47 Montreal House Surrey Quays Road London  
12 Montreal House Surrey Quays Road London  
Flat 29 7 Maritime Street London  
146 Montreal House Surrey Quays Road London  
Flat 60 7 Maritime Street London  
19 Montreal House Surrey Quays Road London  
Flat 49 7 Maritime Street London  
Flat 68 29 Surrey Quays Road London  
33 Montreal House Surrey Quays Road London  
Flat 13 5 Maritime Street London  
Flat 47 29 Surrey Quays Road London  
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Flat 1 11 Maritime Street London  
Flat 70 29 Surrey Quays Road London

Flat 49 11 Maritime Street London  
Flat 43 11 Maritime Street London  
161 Montreal House Surrey Quays Road London  
155 Montreal House Surrey Quays Road London  
126 Montreal House Surrey Quays Road London  
113 Montreal House Surrey Quays Road London  
84 Montreal House Surrey Quays Road London  
7 Montreal House Surrey Quays Road London  
71 Montreal House Surrey Quays Road London  
Flat 55 29 Surrey Quays Road London  
Flat 46 29 Surrey Quays Road London  
17 Maritime Street London Southwark  
Flat 35 29 Surrey Quays Road London  
Flat 32 29 Surrey Quays Road London  
Flat 28 29 Surrey Quays Road London  
Flat 27 11 Maritime Street London  
Flat 17 11 Maritime Street London  
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Flat 12 11 Maritime Street London  
Flat 5 11 Maritime Street London  
Flat 57 7 Maritime Street London  
Flat 54 7 Maritime Street London  
Flat 9 7 Maritime Street London  
Flat 20 5 Maritime Street London  
Unit A Montreal House Surrey Quays Road

157 Montreal House Surrey Quays Road London  
120 Montreal House Surrey Quays Road London  
117 Montreal House Surrey Quays Road London  
105 Montreal House Surrey Quays Road London  
97 Montreal House Surrey Quays Road London  
64 Montreal House Surrey Quays Road London  
59 Montreal House Surrey Quays Road London  
55 Montreal House Surrey Quays Road London  
48 Montreal House Surrey Quays Road London  
11 Montreal House Surrey Quays Road London  
Canada Water Library 21 Surrey Quays Road London  
Canada Estate Tenants Hall Renforth Street London  
Canada Water Bus Station Surrey Quays Road London  
Flat 66 29 Surrey Quays Road London  
Flat 3 29 Surrey Quays Road London  
Flat 59 7 Maritime Street London  
Flat 18 11 Maritime Street London  
Flat 52 11 Maritime Street London  
Flat 1 5 Maritime Street London  
Flat 37 7 Maritime Street London  
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Flat 43 7 Maritime Street London  
Flat 17 29 Surrey Quays Road London

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Flat 25 11 Maritime Street London  
Flat 19 11 Maritime Street London  
52 Montreal House Surrey Quays Road London  
43 Montreal House Surrey Quays Road London  
25 Montreal House Surrey Quays Road London  
131 Montreal House Surrey Quays Road London  
98 Montreal House Surrey Quays Road London  
82 Montreal House Surrey Quays Road London  
10 Montreal House Surrey Quays Road London  
Flat 57 29 Surrey Quays Road London  
Flat 62 29 Surrey Quays Road London  
Flat 29 29 Surrey Quays Road London  
Flat 59 11 Maritime Street London  
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Flat 6 11 Maritime Street London  
Flat 3 11 Maritime Street London



Flat 44 7 Maritime Street London  
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Flat 21 5 Maritime Street London  
143 Montreal House Surrey Quays Road London  
136 Montreal House Surrey Quays Road London  
133 Montreal House Surrey Quays Road London  
128 Montreal House Surrey Quays Road London  
123 Montreal House Surrey Quays Road London  
107 Montreal House Surrey Quays Road London  
96 Montreal House Surrey Quays Road London  
74 Montreal House Surrey Quays Road London  
44 Montreal House Surrey Quays Road London  
28 Montreal House Surrey Quays Road London  
16 Montreal House Surrey Quays Road London  
Tinos Mobile Pizza Deal Porter Square London  
8 Montreal House Surrey Quays Road London  
6 Montreal House Surrey Quays Road London  
3 Montreal House Surrey Quays Road London  
Flat 8 5 Maritime Street London  
122 Montreal House Surrey Quays Road London  
58 Montreal House Surrey Quays Road London  
Flat 46 7 Maritime Street London

Flat 48 29 Surrey Quays Road London  
147 Montreal House Surrey Quays Road London  
Flat 44 29 Surrey Quays Road London  
Flat 56 29 Surrey Quays Road London  
Flat 10 7 Maritime Street London  
Flat 25 5 Maritime Street London  
Flat 60 11 Maritime Street London  
168 Montreal House Surrey Quays Road London  
124 Montreal House Surrey Quays Road London  
100 Montreal House Surrey Quays Road London  
87 Montreal House Surrey Quays Road London  
18 Montreal House Surrey Quays Road London  
Cafe Canada Water Library 21 Surrey Quays Road  
Flat 40 29 Surrey Quays Road London  
Flat 15 29 Surrey Quays Road London  
Flat 10 29 Surrey Quays Road London  
Flat 65 11 Maritime Street London  
Flat 51 11 Maritime Street London  
Flat 45 11 Maritime Street London  
Flat 29 11 Maritime Street London  
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Flat 12 5 Maritime Street London

151 Montreal House Surrey Quays Road London

148 Montreal House Surrey Quays Road London

110 Montreal House Surrey Quays Road London

99 Montreal House Surrey Quays Road London

88 Montreal House Surrey Quays Road London

62 Montreal House Surrey Quays Road London

31 Montreal House Surrey Quays Road London

26 Montreal House Surrey Quays Road London

Flat 7 Toronto House Surrey Quays Road

Flat 5 Toronto House Surrey Quays Road

Flat 31 Toronto House Surrey Quays Road

Flat 22 Toronto House Surrey Quays Road

Flat 21 Toronto House Surrey Quays Road

Flat 19 Toronto House Surrey Quays Road

Flat 9 Toronto House Surrey Quays Road

Rear Of Room Quays Unit A2 Toronto House Surrey Quays Road

Unit A2 Toronto House Surrey Quays Road

Flat 35 Toronto House Surrey Quays Road

Flat 4 Toronto House Surrey Quays Road

Flat 29 Toronto House Surrey Quays Road

Flat 16 Toronto House Surrey Quays Road

Flat 11 Toronto House Surrey Quays Road

Flat 8 Toronto House Surrey Quays Road

Unit 1 Toronto House Surrey Quays Road  
Flat 44 Toronto House Surrey Quays Road  
Flat 56 Toronto House Surrey Quays Road  
Flat 53 Toronto House Surrey Quays Road  
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Flat 39 Ontario Point 28 Surrey Quays Road

Site Office Surrey Quays Road London

Flat 74 Ontario Point 28 Surrey Quays Road

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Flat 144 Ontario Point 28 Surrey Quays Road

Flat 134 Ontario Point 28 Surrey Quays Road

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Flat 84 Ontario Point 28 Surrey Quays Road

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Flat 6 29 Surrey Quays Road London  
Flat 36 11 Maritime Street London  
61 Montreal House Surrey Quays Road London  
Flat 50 29 Surrey Quays Road London  
23 Maritime Street London Southwark  
Flat 53 29 Surrey Quays Road London  
Flat 39 29 Surrey Quays Road London  
Flat 33 29 Surrey Quays Road London  
Flat 25 7 Maritime Street London  
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54 Montreal House Surrey Quays Road London

35 Montreal House Surrey Quays Road London  
163 Montreal House Surrey Quays Road London  
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2 Montreal House Surrey Quays Road London  
106 Montreal House Surrey Quays Road London

90 Montreal House Surrey Quays Road London  
13 Montreal House Surrey Quays Road London  
3 Albatross Way London Southwark  
3 Maritime Street London Southwark  
Flat 18 29 Surrey Quays Road London  
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Flat 70 11 Maritime Street London  
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165 Montreal House Surrey Quays Road London  
159 Montreal House Surrey Quays Road London  
152 Montreal House Surrey Quays Road London  
144 Montreal House Surrey Quays Road London  
115 Montreal House Surrey Quays Road London  
104 Montreal House Surrey Quays Road London  
94 Montreal House Surrey Quays Road London  
75 Montreal House Surrey Quays Road London  
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142 Montreal House Surrey Quays Road London

108 Montreal House Surrey Quays Road London  
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103 Montreal House Surrey Quays Road London  
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Flat 40 11 Maritime Street London  
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57 Montreal House Surrey Quays Road London  
41 Montreal House Surrey Quays Road London  
22 Montreal House Surrey Quays Road London  
150 Montreal House Surrey Quays Road London  
134 Montreal House Surrey Quays Road London  
95 Montreal House Surrey Quays Road London  
92 Montreal House Surrey Quays Road London  
60 Montreal House Surrey Quays Road London  
Unit 1 Ticket Hall Canada Water Underground Station Surrey Quays Road  
Flat 54 29 Surrey Quays Road London

Flat 69 29 Surrey Quays Road London

Flat 60 29 Surrey Quays Road London

1 Maritime Street London Southwark

Flat 23 29 Surrey Quays Road London

Flat 9 29 Surrey Quays Road London

Flat 62 11 Maritime Street London

Flat 50 11 Maritime Street London

Flat 44 11 Maritime Street London

Flat 33 11 Maritime Street London

Flat 20 11 Maritime Street London

Flat 45 7 Maritime Street London

Flat 33 7 Maritime Street London

Flat 23 7 Maritime Street London

Flat 18 7 Maritime Street London

Flat 15 5 Maritime Street London

162 Montreal House Surrey Quays Road London

154 Montreal House Surrey Quays Road London

132 Montreal House Surrey Quays Road London

130 Montreal House Surrey Quays Road London

125 Montreal House Surrey Quays Road London

102 Montreal House Surrey Quays Road London

## **Re-consultation:**

## **Appendix 4: Consultation responses received**

### **Internal services**

Ecology

Archaeology

Community Infrastructure Levy Team

Design and Conservation Team [Formal]

Ecology

Environmental Protection

Highways Development and Management

Flood Risk Management & Urban Drainage

Transport Policy

Urban Forester

### **Statutory and non-statutory organisations**

Environment Agency

London Underground

Metropolitan Police Service (Designing O

Transport for London

Thames Water

### **Neighbour and local groups consulted:**

York House 45 Seymour St London



